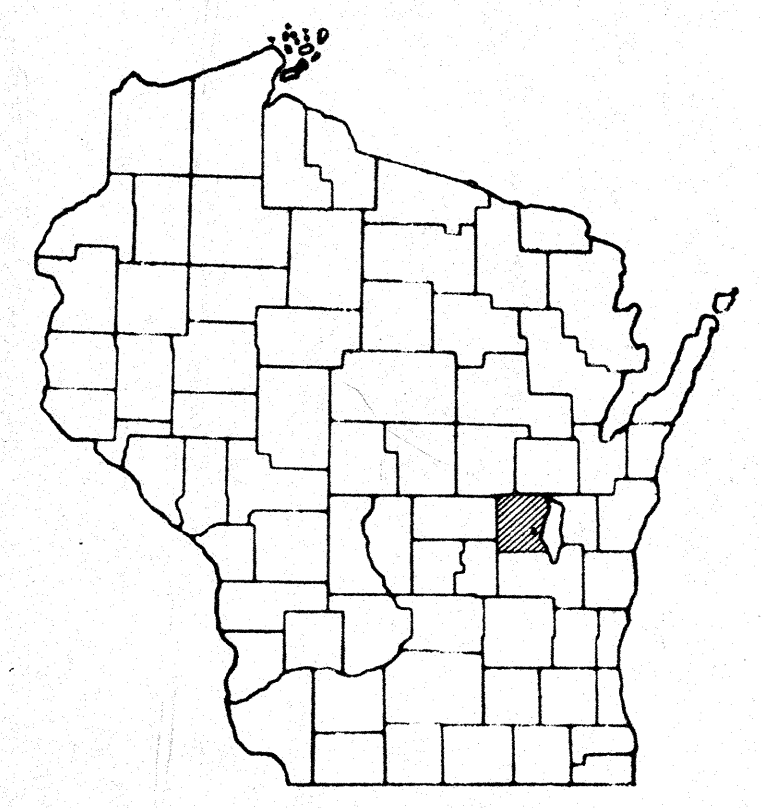


COUNTY AND HIGHWAY	ROUTE AND SECTION	CLASS AND AGREEMENT		FEDERAL DIVISION OFFICE	SHEET NUMBER	TOTAL SHEETS
		STATE	FEDERAL			
70.1	3.2		11.33	WIS. 4	1	61

INDEX OF SHEETS

- SHEET NO. 1 TITLE
- SHEET NO. 2-2.1 TYPICAL CROSS SECTIONS
- SHEET NO. 3 ESTIMATE OF QUANTITIES
- SHEET NO. 2 MISCELLANEOUS QUANTITIES
- SHEET NO. 4 RIGHT OF WAY PLAT
- SHEET NO. 5-10 PLAN AND PROFILE STA. 640+87.3 TO STA. 686+50
- SHEET NO. 11-19 STANDARD DETAILS
- SHEET NO. 20-50 DRAINAGE STRUCTURES
- SHEET NO. 51-61 CROSS SECTIONS



STATE OF WISCONSIN
STATE HIGHWAY COMMISSION OF WISCONSIN

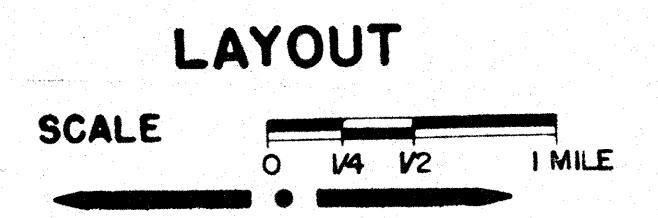
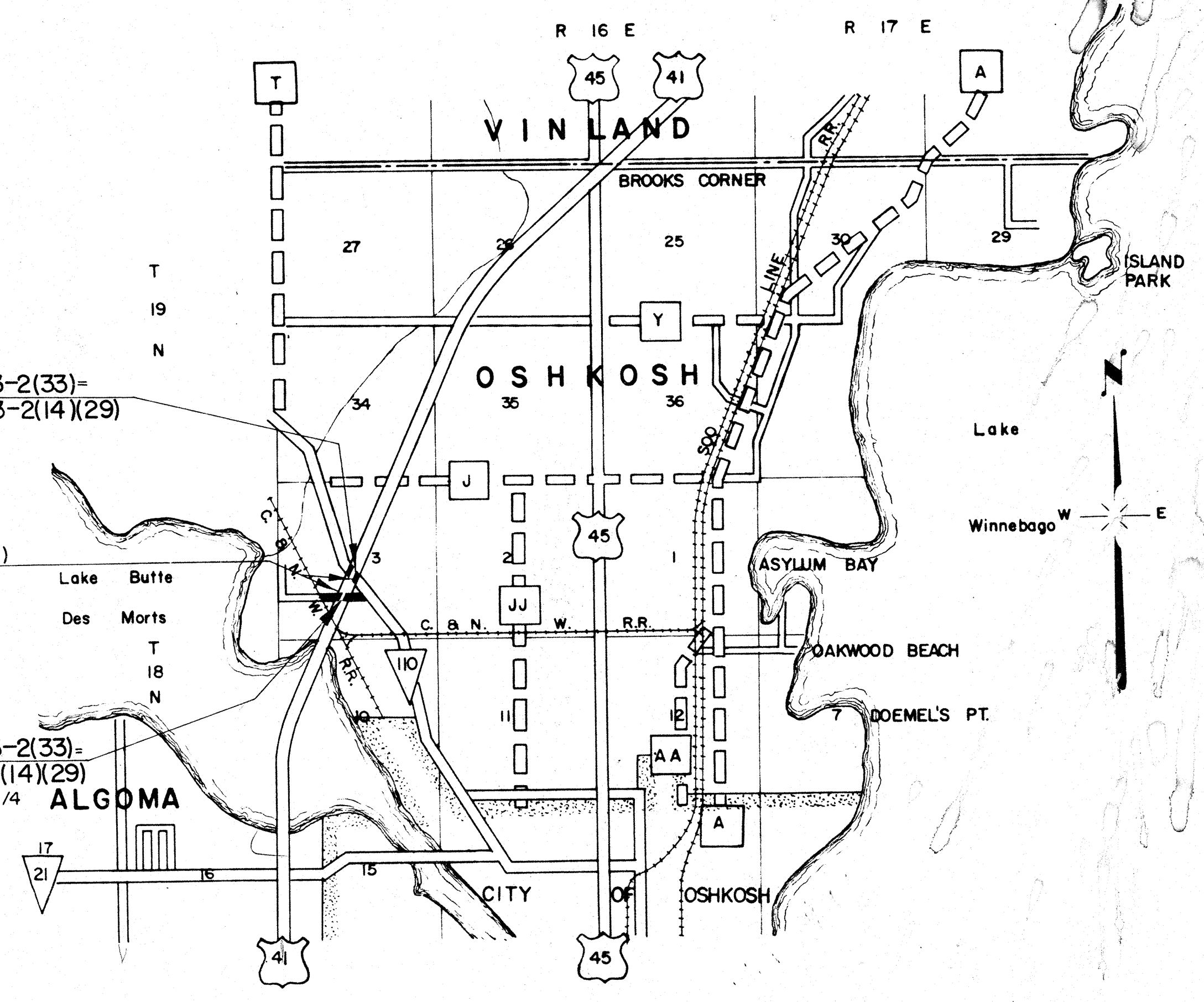
PLAN AND PROFILE OF PROPOSED
OSHKOSH - NEENAH ROAD
 (S.T.H. 110 INTERCHANGE)
 U.S.H. 41
WINNEBAGO COUNTY
PROJECT F 03-2 (33)

SCALES PLAN 1 IN. = 100 FT.
 PROFILE HOR. 1 IN. = 100 FT. VERT. 1 IN. = 10 FT.
 CROSS SECTIONS HOR. 1 IN. = 5 FT. VERT. 1 IN. = 5 FT.

STA. 658+56.13 END OF PROJ. F 03-2(33) =
 STA. 658+56.13 OF OLD PROJ. F 03-2(14)(29)
 372' S. & 2,859' W. OF S.E. COR. OF N.E. 1/4
 SEC. 3, T18 N., R16 E.

STA. 648+95.0 TO STA. 656+05.11 (710.11)
 EXCEPTION TO NET LENGTH OF C

STA. 647+84.44 BEG. OF PROJ. F 03-2(33) =
 STA. 647+84.44 OLD PROJ. F 03-2(14)(29)
 32' S. & 3,261' W. OF N.E. COR. OF S 1/2 OF SE. 1/4
 SEC. 3, T18 N., R16 E.



TOTAL NET LENGTH OF CENTERLINE = 0.068 MI.

CONVENTIONAL SIGNS

- | | | | |
|---------------------------|-------|-------------------------------|-------|
| STATE LINE | ----- | CULVERTS IN PLACE | ----- |
| COUNTY LINE | ----- | CULVERTS REQUIRED | ----- |
| TOWNSHIP OR RANGE LINE | ----- | DROP INLET | ----- |
| SECTION LINE | ----- | POWER POLE | ----- |
| NEW RIGHT OF WAY LINE | ----- | TELEPHONE OR TELEGRAPH POLE | ----- |
| PRESENT RIGHT OF WAY LINE | ----- | RIGHT OF WAY MARKERS | ----- |
| WIRE FENCE { WOVEN | ----- | REFERENCE STAKE FOR HUBS ONLY | ----- |
| { BARBED | ----- | MARSH | ----- |
| LOT LINE | ----- | HEDGE | ----- |
| CORPORATE OR CITY LIMITS | ----- | TREES | ----- |
| PROPERTY LINE | ----- | | |
| TRAVELED WAY OR P.E. | ----- | | |
| RAILROADS | ----- | | |
| BASE OR SURVEY LINE | ----- | | |

STATE HIGHWAY
 COMMISSION OF WISCONSIN
 MADISON, WIS.

SURVEYOR G.F.O. NOTE BOOK 10752, 10759
 DISTRICT COMPUTER C.P.K. M. O. CHECKER
 DISTRICT CHECKER R.J.H. CORRECT

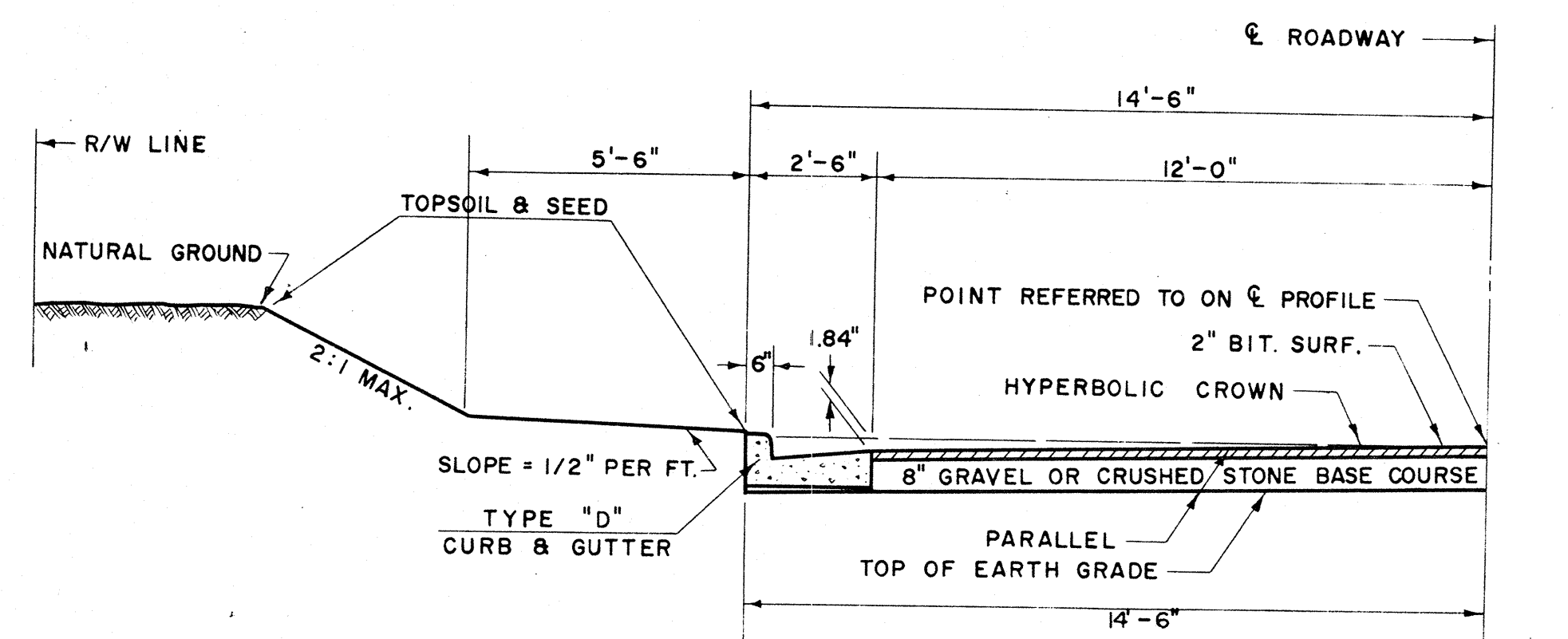
CORRECT:
 DATE 11-8-57 *L. O. Empey*
 DISTRICT ENGINEER

RECOMMENDED FOR APPROVAL:
 DATE 11-11-57 *J. J. Pitt*
 ENGINEER OF DESIGN

APPROVED:
 DATE 11-12-57 *E. C. Rost*
 STATE HIGHWAY ENGINEER

DEPARTMENT OF COMMERCE
 BUREAU OF PUBLIC ROADS

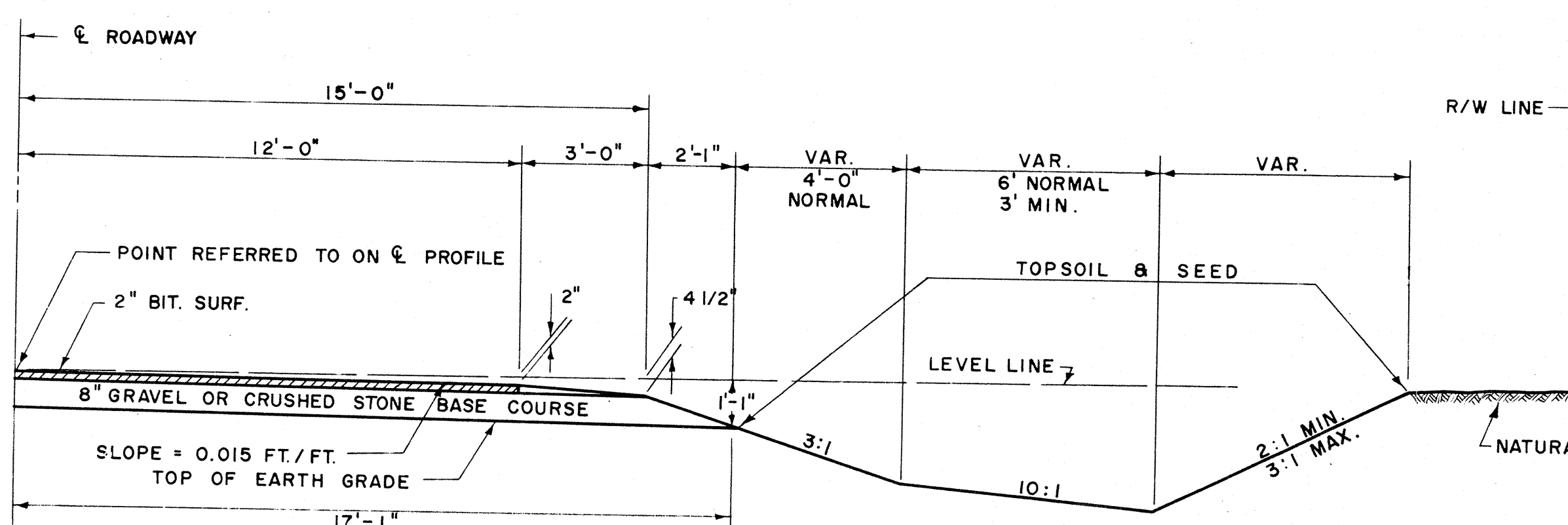
APPROVED:



GRAVEL OR CR. ST. BASE COURSE FOR ROADBED = 82.97 C.Y./STA. (INCLUDES 40% FOR COMPACTION.)

1/2 TYPICAL FINISHED SECTION SUNSET LANE

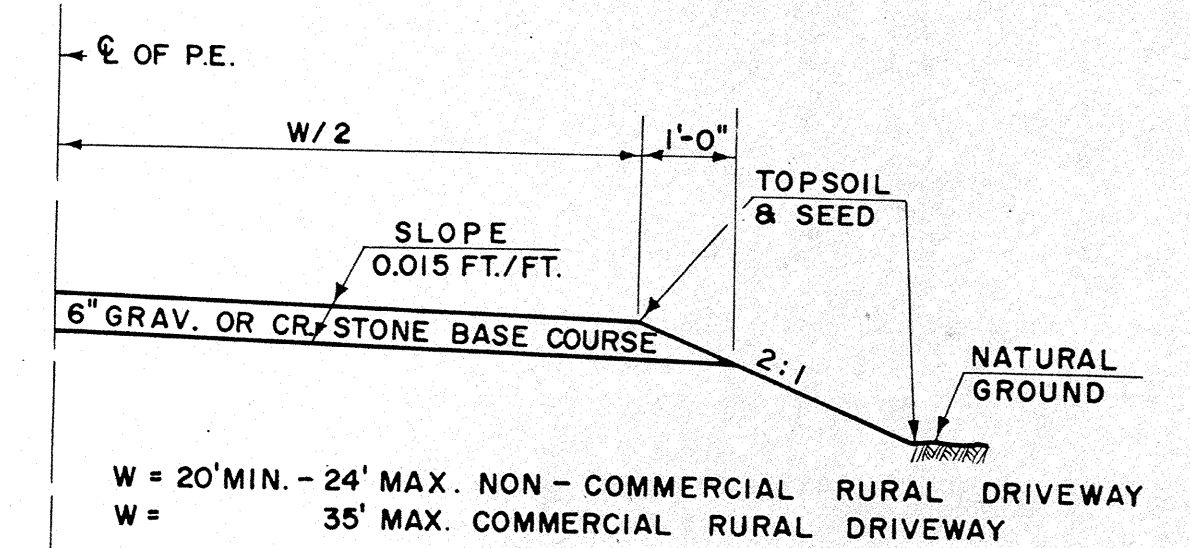
STA. 18± - STA. 21+50



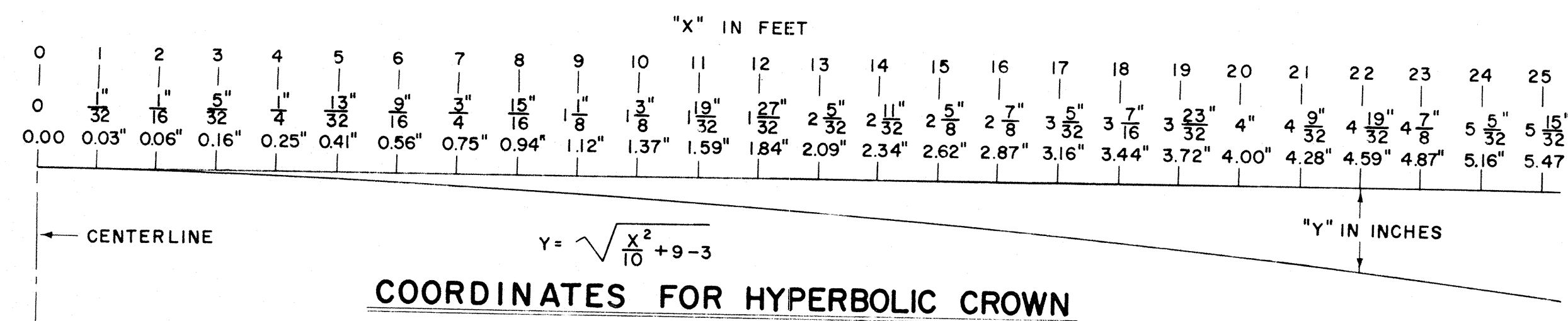
GRAVEL OR CR. ST. BASE COURSE FOR ROADBED = 110.90 C.Y./STA. (INC. 40% FOR COMPACTION)
 GRAVEL OR CR. ST. BASE COURSE FOR SHOULDERS = 3.33 C.Y./STA. (INC. 80% FOR COMPACTION)

1/2 TYPICAL FINISHED SECTION SUNSET LANE

STA. 16+50 - STA. 18±
 21+50 - STA. 26



1/2 SECTION FOR P.E.'S.



COORDINATES FOR HYPERBOLIC CROWN

GRAV. OR CR. STONE BASE COURSE				
SECT. NO.	STATION TO STATION	℄ C.Y.	SH'LD'RS C.Y.	P.E.'S. C.Y.
1	U.S.H. 41 BY-PASS RD.			
	642+00-685+00	6,061	191	90
	SUNSET LANE			
	16+50 - 18±	166	5	24
	18± - 21+50	290		
	21+50 - 26+00	499	15	36
	UNDISTRIBUTED	23		

SODDING			
SECT. NO.	LOCATION	AT CULV. E'WALLS S.Y.	AT M.R.M. DITCH CKS. S.Y.
1	U.S.H. 41 BY-PASS RD.	22	
	SUNSET LANE	40	48

PIPE CULVERTS									
SECT. NO.	STATION	DIAMETER INCHES	LENGTH LIN. FT.	℄ RD'WY.	P.E.'S.		M.R.M. C.Y.	SOD S.Y.	MARKERS
					L	R			
1	643+70	36	48	✓				14	2
	648+25	24	48	✓					
	649+50	24	48	✓					
	658+55	18	52	✓					
	659+90	18	52	✓					
	672+75	18	6						
	"	18	6						
	672+74	28" x 36" C.M.P.A.	48						
	684+50	18	32	✓				8	
	17+08	18	32	✓				8	
	17+48	18	32	✓				8	
	21+20	28" x 18" C.M.P.A.	66	✓			2		2
	15+25	18	32		✓			8	
	24+02	18	32	✓				8	
	24+48	18	32	✓				8	

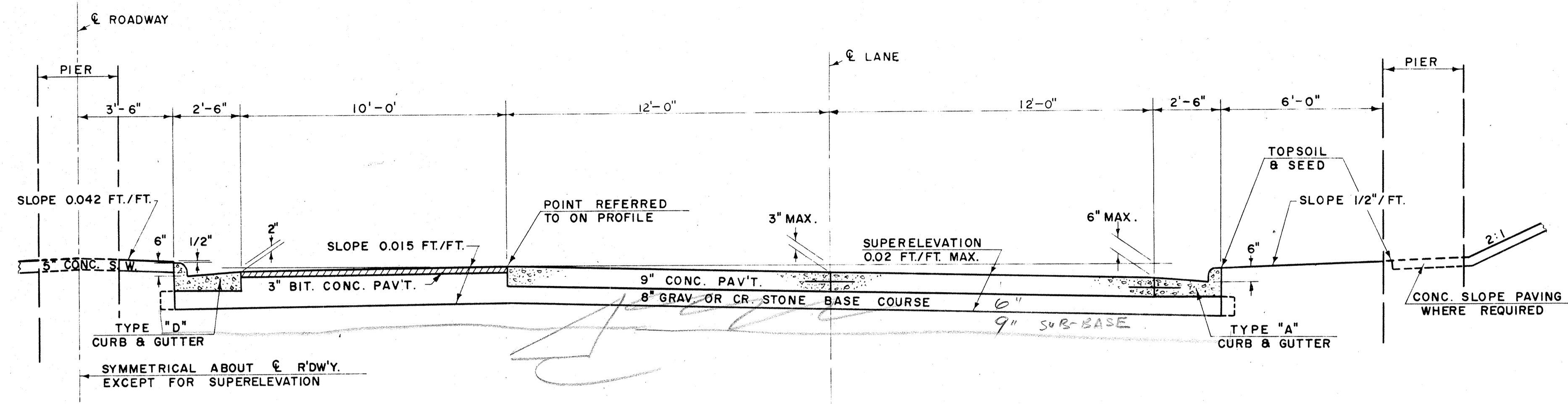
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, APPROVED STANDARD DRAWINGS, AND THE APPLICABLE SPECIAL PROVISIONS.

APPROVED STANDARD DRAWINGS

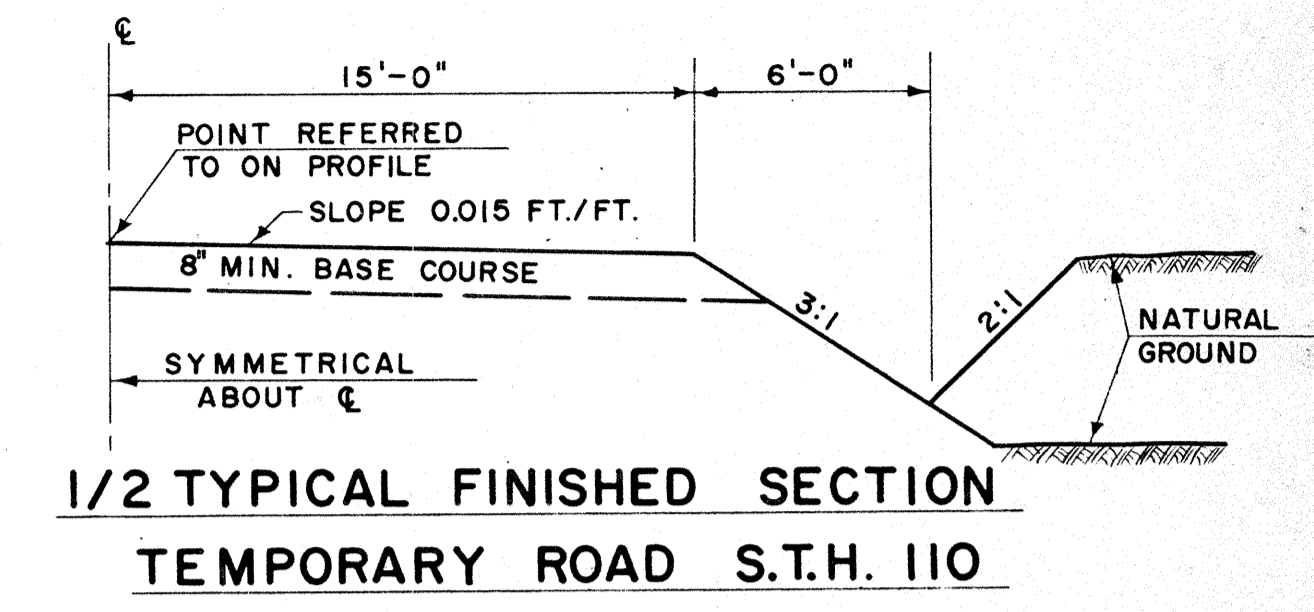
- EXCAVATION & BACKFILL FOR PIPE CULVERTS - BOX CULVERTS - ABUTMENTS. 1-2.1.1
- DETAILS OF PRIVATE ENTRANCE & SIDE ROAD APPROACHES. 1-3.1.1
- CONCRETE CURB, GUTTER, COMBINATION CURB & GUTTER, SURFACE DRAIN. 3-1.1.4
- MORTAR RUBBLE MASONRY OR RIPRAP FOR CULVERT & CATTLE PASS ENDWALLS. 6-2.4.2
- RIPRAP AT SIDE RD. CULVERTS & RIPRAP OR SOD AT P.E. CULVERTS. 6-2.5.1
- STRUCTURAL PLATE PIPE ARCH & CORR. METAL PIPE ARCH. 6-5.3.1
- MARKER POSTS & MARKER POSTS FOR RIGHT OF WAY. 7-1.3.3
- CONSTRUCTION BARRICADE. 7-4.1.2
- DITCH CHECKS, MORTAR RUBBLE MASONRY & SOD. 8-1.3.1

TYPICAL CROSS SECTION FOR
 29' & 30' ROADWAYS SUNSET LANE
 MISCELLANEOUS CONSTRUCTION DETAILS
 AND
 MISCELLANEOUS QUANTITIES

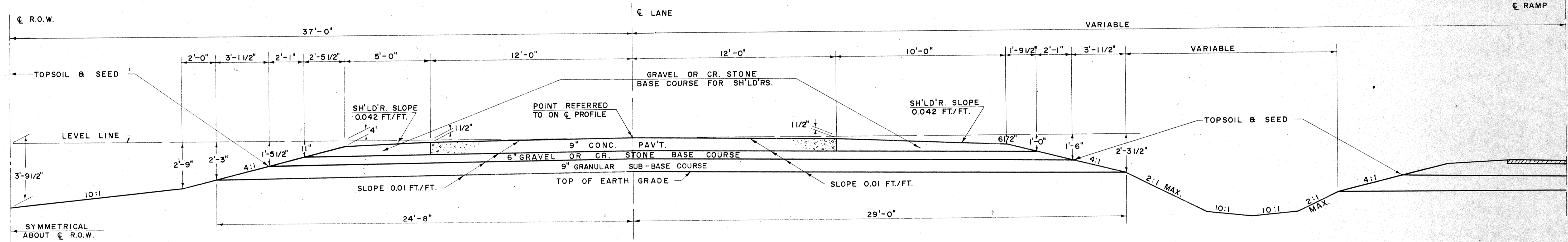


1/2 TYPICAL ULTIMATE FINISHED SECTION S.T.H. 110

STA. 80+85 - STA. 85+60

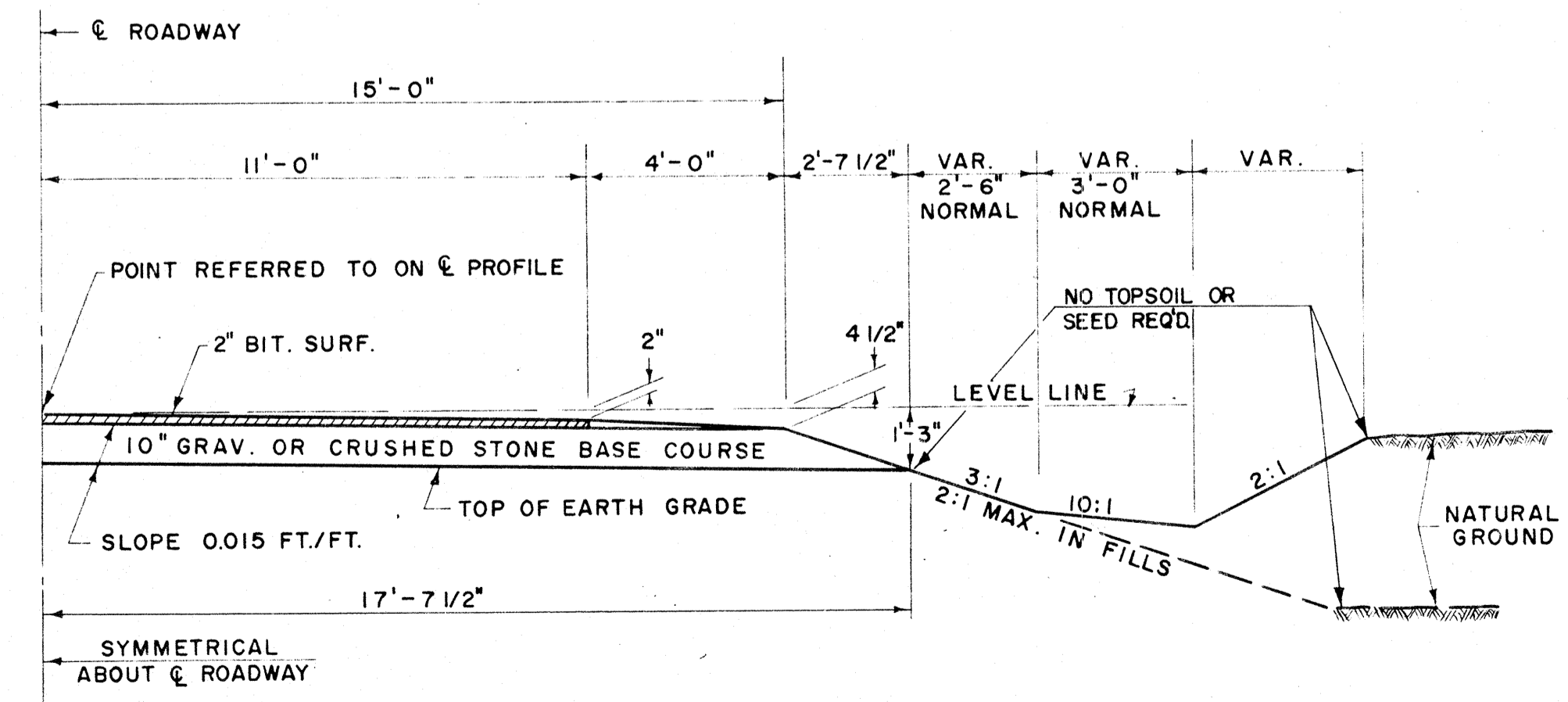


**1/2 TYPICAL FINISHED SECTION
TEMPORARY ROAD S.T.H. 110**



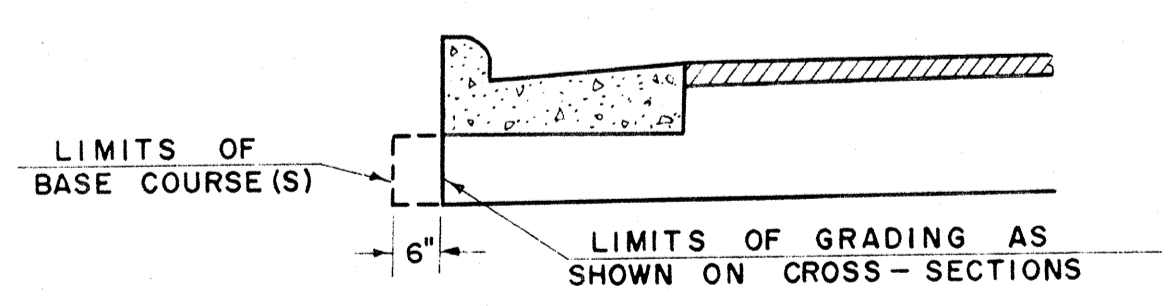
**1/2 TYPICAL ULTIMATE FINISHED SECTION OF BRIDGE APPROACHES
ON U.S.H. 41**

37
12
25
2
23



1/2 TYPICAL FINISHED SECTION OF BY-PASS ROAD U.S.H. 41

GRAVEL OR CR. ST. BASE COURSE FOR ROADBED = 140.95 C.Y./STA. (INCL. 40% FOR COMPACTION)
GRAVEL OR CR. ST. BASE COURSE FOR SHOULDERS = 4.45 C.Y./STA. (INCL. 80% FOR COMPACTION)

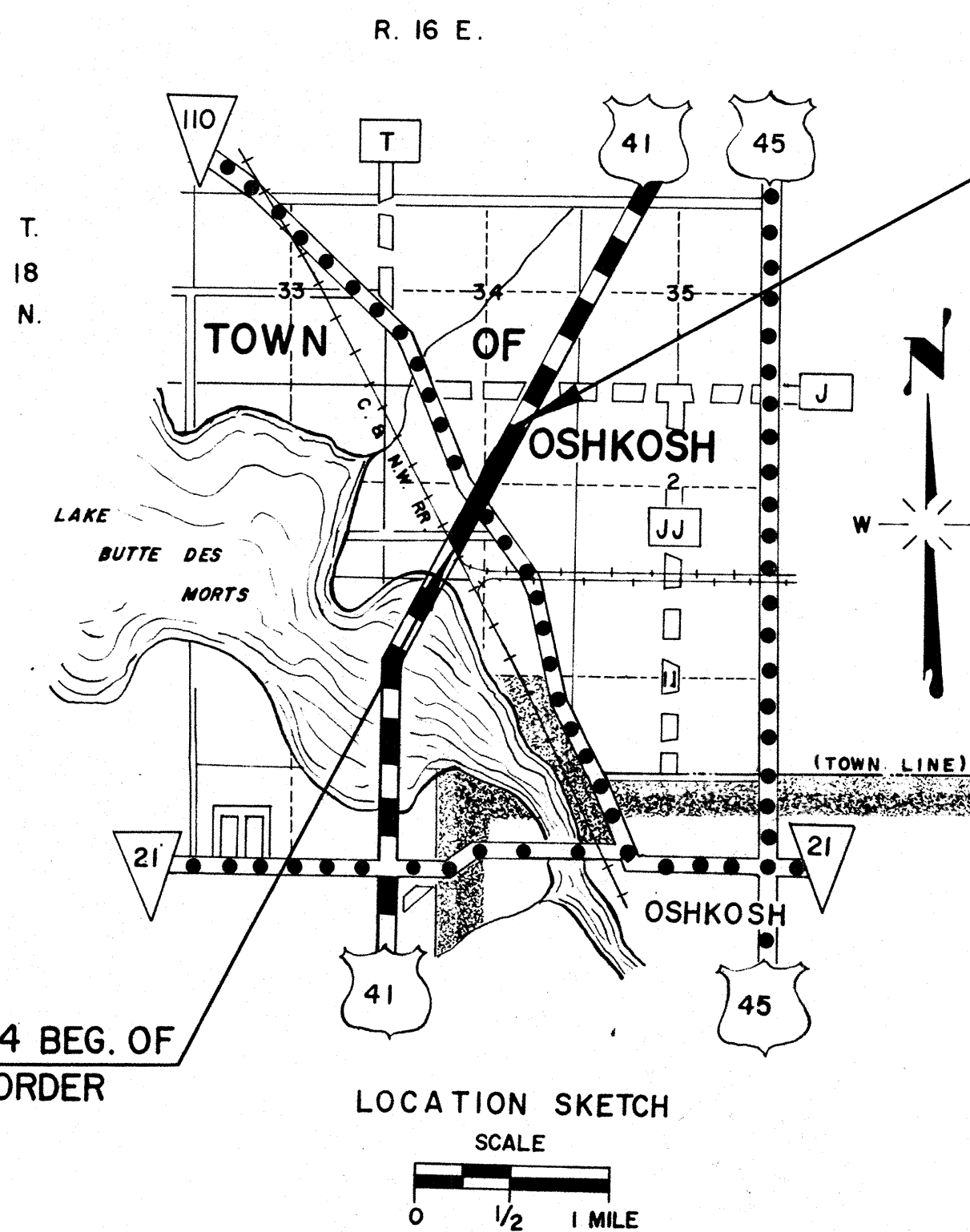


**DETAIL SHOWING LIMITS OF
GRADING & BASE COURSE(S)
WHERE CURB & GUTTER IS REQ'D.**

**TYPICAL CROSS SECTION
FOR
ULTIMATE S.T.H. 110
ULTIMATE STRUCT. APPROACHES U.S.H. 41
TEMPORARY ROAD S.T.H. 110
BY-PASS ROAD U.S.H. 41
& MISC. CONSTR. DETAILS**

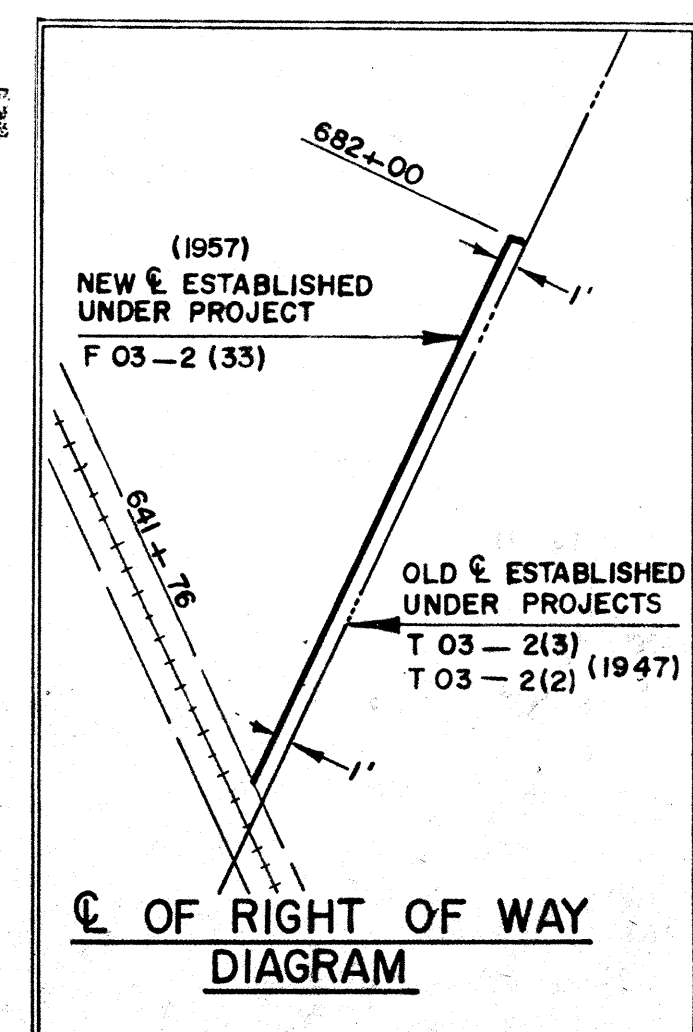
SCALE: VARIABLE

COUNTY AND HIGHWAY	ROUTE AND SECTION	CLASS AND AGREEMENT STATE FEDERAL	FEDERAL DIVISION OFFICE	SHEET NUMBER	TOTAL SHEETS
70.1	3.2	11.33	4	4	



STA. 682+00 END OF RELOCATION ORDER

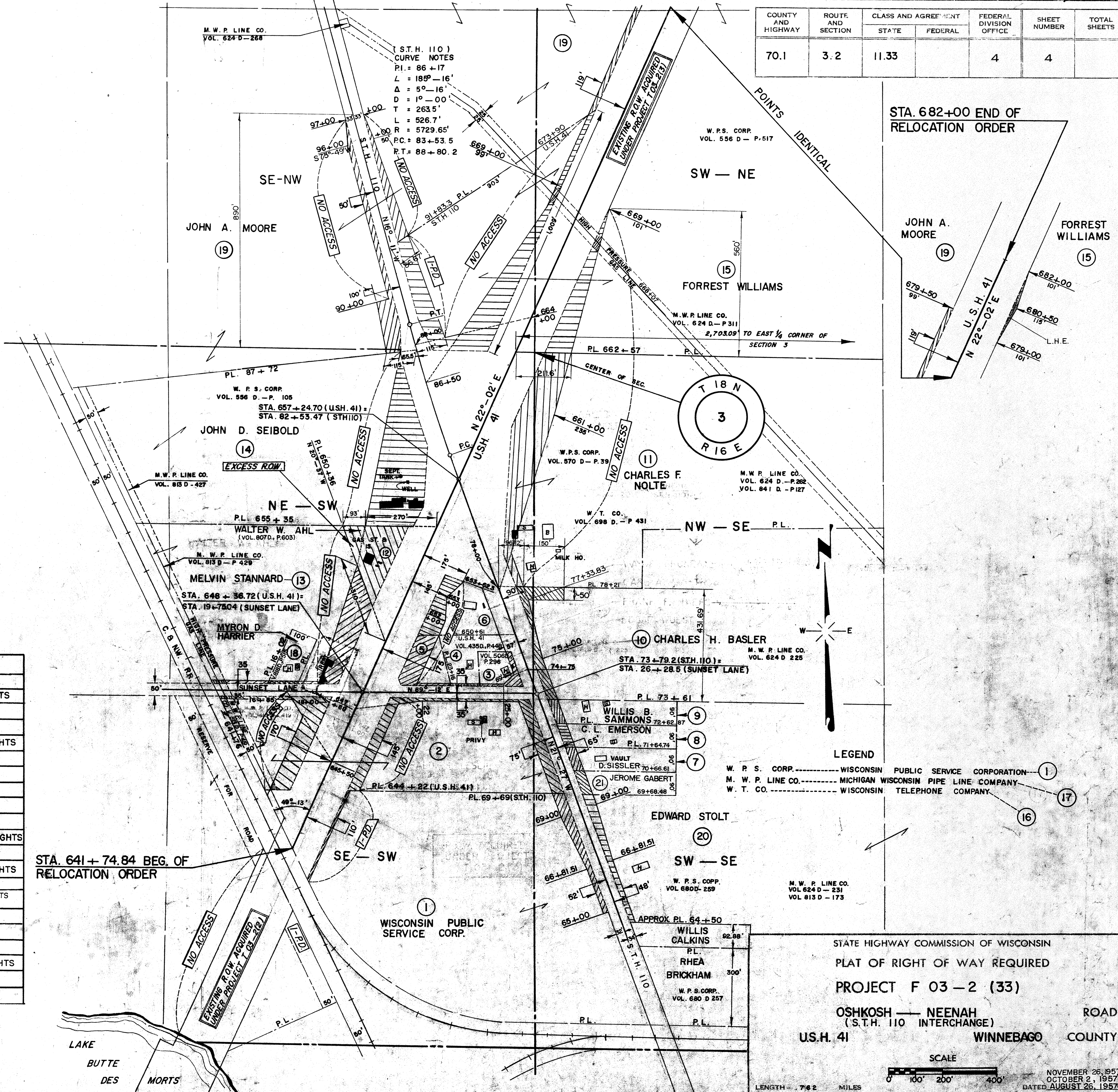
STA. 641+74.84 BEG. OF RELOCATION ORDER



SCHEDULE OF LAND AND INTEREST REQUIRED

* TO BE ACQUIRED BY THE STATE OF WISCONSIN

NO.	OWNER	ACRES REQ. D.	RODS OF FENCING TO MOVE	RODS OF FENCING TO BUILD	INTEREST REQUIRED
1	WISCONSIN PUBLIC SERVICE CORP.	0.43			QUIT CLAIM DEED
2	GERALD S. LESLIE	1.59			FEE SIMPLE INCLUDING ACCESS RIGHTS
3	EDGAR WUEST	0.18			FEE SIMPLE
4	FRANK F. HAEFS	0.03			" "
5	CARL A. ZASTROW	0.40			FEE SIMPLE INCLUDING ACCESS RIGHTS
6	WAYNE GRUNEWALD	0.38			" " " "
7	DONALD SISSLER	0.07			FEE SIMPLE
8	C. L. EMERSON	0.07			" "
9	WILLIS B. SAMMONS	0.07			" "
10	CHARLES H. BASLER	0.36			" "
11	CHARLES F. NOLTE	1.95			FEE SIMPLE INCL. PARTIAL ACCESS RIGHTS
12	WALTER W. AHL	0.95			FEE SIMPLE
13	MELVIN STANNARD	0.86			FEE SIMPLE INCLUDING ACCESS RIGHTS
14	JOHN D. SEIBOLD	2.00			" " " "
15	FORREST WILLIAMS	2.63			FEE SIMPLE INCLUDING PARTIAL ACCESS RIGHTS AND LIMITED HIGHWAY EASEMENT
16	WIS. TELEPHONE CO.				QUIT CLAIM DEED
17	MICHIGAN WIS. PIPE LINE CO.	0.09			" "
18	MYRON D. HARRIER	0.03			FEE SIMPLE
19	JOHN A. MOORE	1.29			FEE SIMPLE INCL. PARTIAL ACCESS RIGHTS
20	EDWARD STOLT	0.26			FEE SIMPLE
21	JEROME GABERT	0.07			" "



STA. 641+74.84 BEG. OF RELOCATION ORDER

STA. 682+00 END OF RELOCATION ORDER

STATE HIGHWAY COMMISSION OF WISCONSIN
 PLAT OF RIGHT OF WAY REQUIRED
 PROJECT F 03-2 (33)
 OSHKOSH - NEENAH ROAD
 (S.T.H. 110 INTERCHANGE)
 WINNEBAGO COUNTY

SCALE
 0 100' 200' 400'
 LENGTH 7.42 MILES
 NOVEMBER 26, 1957
 OCTOBER 2, 1957
 DATED AUGUST 26, 1957

NOTE: ON THIS SHEET USE ONLY THE NOTES AND PLAN APPLICABLE TO STRUCTURES.

BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
1.	640+65	SPIKE IN	100' RT. 754.44
2.	647+80	"	105' LT. 756.03
3.	653+10	PT. MK. W. END GAS P. ISLAND	155' RT. 766.82
4.	659+75	SPIKE IN 40' OAK	220' RT. 764.92

STA. 643+55 - LT. 105'
36" X 48" C.P. IN PLACE
REMOVE 16' OF C.P. FROM RT. END
36" X 16" EXTENSION REQ'D. ON LT.
SKEW 23° - R.H.F.

STA. 649+00 LT. 99'
TYPE I-J M.H. REQ'D.
F.L. = 769.00
BOTT. = 768.29
DEPTH = 2.75'
12" X 34" CGS. MCP REQ'D.
I = 769.00 D = 757.00

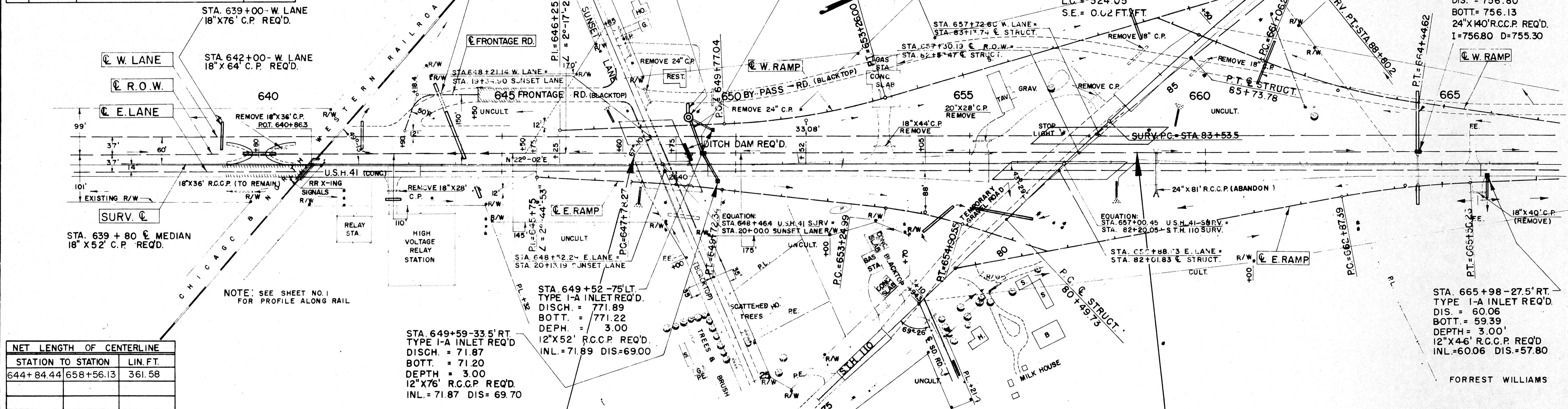
STA. 649+30 LT. 23'
SPECIAL TYPE INLET REQ'D.
TOP = 770.60
DIS. = 769.60
BOTT. = 768.93
18" X 72" C.P. REQ'D.
I = 769.60 D = 769.00

STRUCT. CURVE NOTES

P.I. = 83+11.96
L = 185° - 16'
Δ = 5° - 16'
D = 1° - 00' - 18"
T = 262.23
L.C. = 524.05'
S.E. = 0.02 FT. FT.

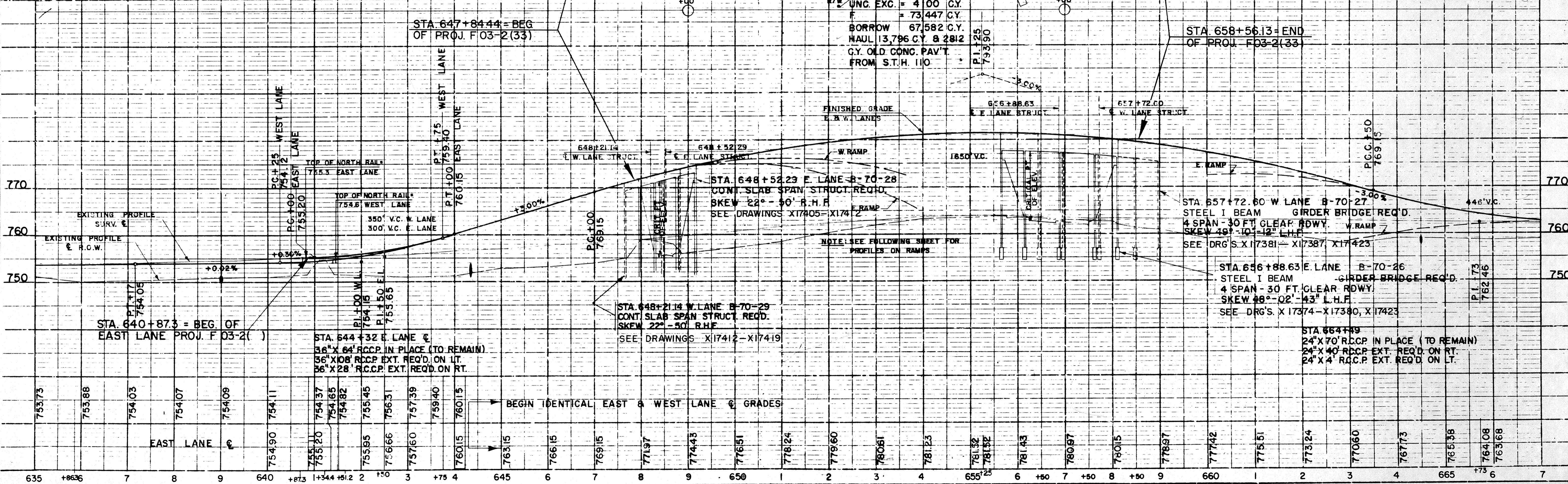
B.P.R. DIVISION	PROJECT	SHEET NUMBER	TOTAL SHEETS
4	F 03-2(33)	5	61

STA. 664+49 LT. 23'
SPECIAL TYPE INLET REQ'D.
TOP = 762.65
DIS. = 756.80
BOTT. = 756.13
24" X 140" R.C.C.P. REQ'D.
I = 756.80 D = 755.30



NET LENGTH OF CENTERLINE		
STATION TO STATION	LIN. FT.	
644+84.44	658+56.13	361.58

STA. 649+59-33.5' RT.
TYPE I-A INLET REQ'D.
DISCH. = 71.87
BOTT. = 71.20
DEPTH = 3.00
12" X 76" R.C.C.P. REQ'D.
INL. = 71.87 DIS. = 69.70



STATION	ELEVATION	STATION	ELEVATION	STATION	ELEVATION
635	753.73	640	754.09	645	761.15
636	753.88	641	754.11	646	761.15
637	754.03	642	754.11	647	761.15
638	754.07	643	754.11	648	761.15
639	754.09	644	754.11	649	761.15
640	754.11	645	754.11	650	761.15
641	754.11	646	754.11	651	761.15
642	754.11	647	754.11	652	761.15
643	754.11	648	754.11	653	761.15
644	754.11	649	754.11	654	761.15
645	754.11	650	761.15	655	761.15
646	754.11	651	761.15	656	761.15
647	754.11	652	761.15	657	761.15
648	754.11	653	761.15	658	761.15
649	754.11	654	761.15	659	761.15
650	761.15	655	761.15	660	761.15
651	761.15	656	761.15	661	761.15
652	761.15	657	761.15	662	761.15
653	761.15	658	761.15	663	761.15
654	761.15	659	761.15	664	761.15
655	761.15	660	761.15	665	761.15
656	761.15	661	761.15		
657	761.15	662	761.15		
658	761.15	663	761.15		
659	761.15	664	761.15		
660	761.15	665	761.15		

DATE: 6-56
BY: G.F.O. W.R.N.
SURVEYED: []
PLOTTED: []
CHECKED: []
NOTE BOOK NO. 10752
PT. OF WAY CHECKED: []
DATE: 10-56

DATE: 6-56
BY: G.F.O. W.R.N.
SURVEYED: []
PLOTTED: []
CHECKED: []
NOTE BOOK NO. 10752
STRUCTURE NOTATION DRG. NO. 10752

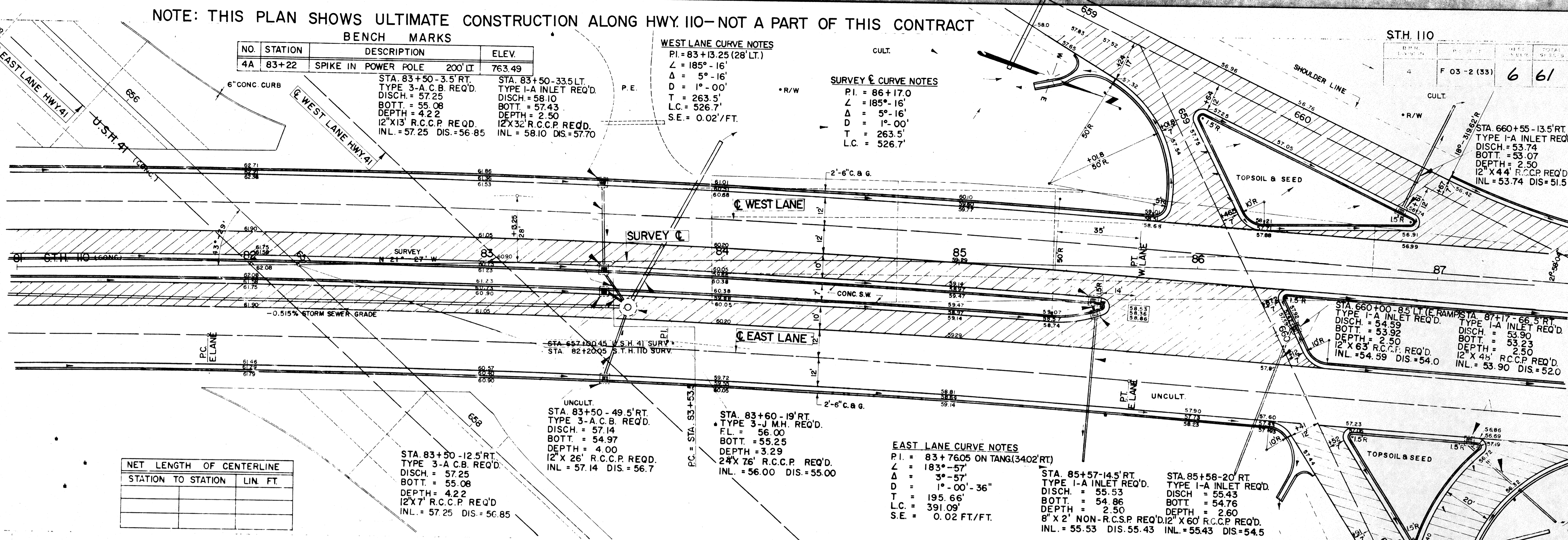
NOTE: THIS PLAN SHOWS ULTIMATE CONSTRUCTION ALONG HWY. 110—NOT A PART OF THIS CONTRACT

BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
4A	83+22	SPIKE IN POWER POLE 200' LT.	763.49

WEST LANE CURVE NOTES
 P.I. = 83+13.25 (28' LT.)
 L = 185° - 16'
 Δ = 5° - 16'
 D = 1° - 00'
 T = 263.5'
 L.C. = 526.7'
 S.E. = 0.02'/FT.

SURVEY & CURVE NOTES
 P.I. = 86+17.0
 L = 185° - 16'
 Δ = 5° - 16'
 D = 1° - 00'
 T = 263.5'
 L.C. = 526.7'

STH. 110		F 03 - 2 (33)		6 61	
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NET LENGTH OF CENTERLINE	
STATION TO STATION	LIN. FT.

STA. 83+50 - 12.5' RT.
 TYPE 3-A C.B. REQ'D.
 DISCH. = 57.25
 BOTT. = 55.08
 DEPTH = 4.22
 12' X 7' R.C.C.P. REQ'D
 INL. = 57.25 DIS. = 56.85

UNCULT.
 STA. 83+50 - 49.5' RT.
 TYPE 3-A C.B. REQ'D.
 DISCH. = 57.14
 BOTT. = 54.97
 DEPTH = 4.00
 12' X 26' R.C.C.P. REQ'D.
 INL. = 57.14 DIS. = 56.7

STA. 83+60 - 19' RT.
 TYPE 3-J M.H. REQ'D.
 F.L. = 56.00
 BOTT. = 55.25
 DEPTH = 3.29
 24' X 76' R.C.C.P. REQ'D.
 INL. = 56.00 DIS. = 55.00

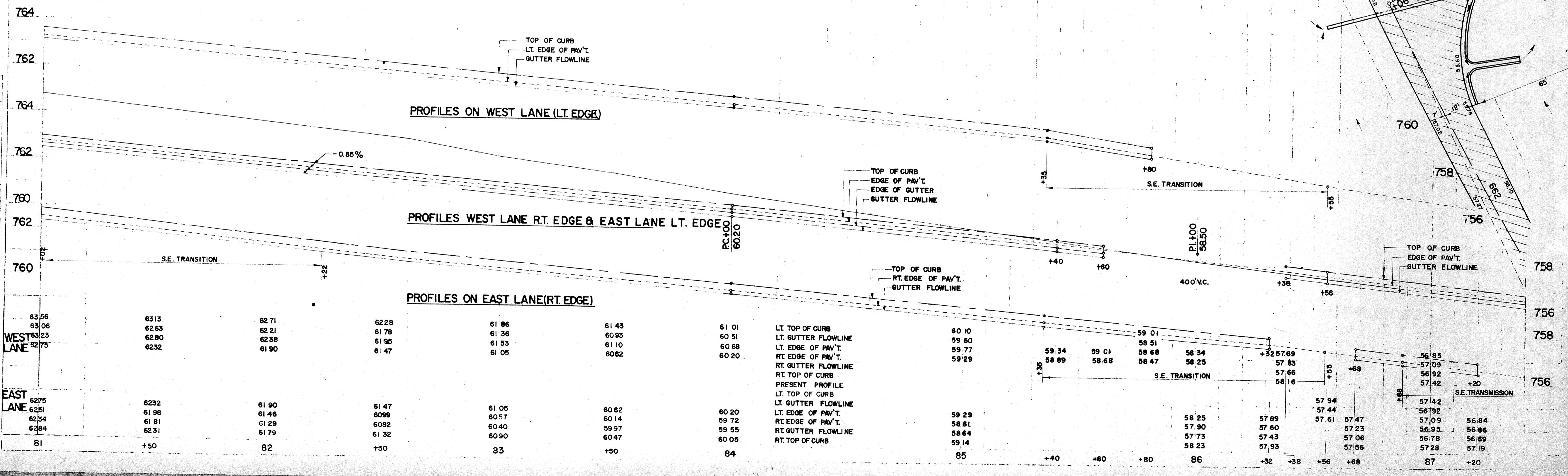
EAST LANE CURVE NOTES
 P.I. = 83+76.05 ON TANG.(3402' RT.)
 L = 183° - 57'
 Δ = 3° - 57'
 D = 1° - 00' - 36"
 T = 195.66'
 L.C. = 391.09'
 S.E. = 0.02 FT./FT.

STA. 85+57-14.5' RT.
 TYPE I-A INLET REQ'D.
 DISCH. = 55.53
 BOTT. = 54.86
 DEPTH = 2.50
 8' X 2' NON-R.C.S.P. REQ'D. 12' X 60' R.C.C.P. REQ'D.
 INL. = 55.53 DIS. 55.43

STA. 85+58-20' RT.
 TYPE I-A INLET REQ'D.
 DISCH. = 55.43
 BOTT. = 54.76
 DEPTH = 2.60
 8' X 2' NON-R.C.S.P. REQ'D. 12' X 60' R.C.C.P. REQ'D.
 INL. = 55.43 DIS. 54.5

STA. 660+00 - 8.5' LT. (E RAMP)
 TYPE I-A INLET REQ'D.
 DISCH. = 54.59
 BOTT. = 53.92
 DEPTH = 2.50
 12' X 63' R.C.C.P. REQ'D.
 INL. = 54.59 DIS. = 54.0

STA. 660+55 - 13.5' RT.
 TYPE I-A INLET REQ'D.
 DISCH. = 53.74
 BOTT. = 53.07
 DEPTH = 2.50
 12' X 44' R.C.C.P. REQ'D.
 INL. = 53.74 DIS. = 51.5



PROFILES ON WEST LANE (LT. EDGE)

PROFILES WEST LANE RT. EDGE & EAST LANE LT. EDGE

PROFILES ON EAST LANE (RT. EDGE)

63.56	63.13	62.71	62.28	61.86	61.43	61.01	60.58	60.15	59.72	59.29	58.86	58.43	58.00	57.57	57.14	56.71	56.28	55.85	55.42	54.99	54.56	54.13	53.70	53.27	52.84	52.41	51.98	51.55	51.12	50.69	50.26	49.83	49.40	48.97	48.54	48.11	47.68	47.25	46.82	46.39	45.96	45.53	45.10	44.67	44.24	43.81	43.38	42.95	42.52	42.09	41.66	41.23	40.80	40.37	39.94	39.51	39.08	38.65	38.22	37.79	37.36	36.93	36.50	36.07	35.64	35.21	34.78	34.35	33.92	33.49	33.06	32.63	32.20	31.77	31.34	30.91	30.48	30.05	29.62	29.19	28.76	28.33	27.90	27.47	27.04	26.61	26.18	25.75	25.32	24.89	24.46	24.03	23.60	23.17	22.74	22.31	21.88	21.45	21.02	20.59	20.16	19.73	19.30	18.87	18.44	18.01	17.58	17.15	16.72	16.29	15.86	15.43	15.00	14.57	14.14	13.71	13.28	12.85	12.42	11.99	11.56	11.13	10.70	10.27	9.84	9.41	8.98	8.55	8.12	7.69	7.26	6.83	6.40	5.97	5.54	5.11	4.68	4.25	3.82	3.39	2.96	2.53	2.10	1.67	1.24	0.81	0.38	-0.05	-0.48	-0.91	-1.34	-1.77	-2.20	-2.63	-3.06	-3.49	-3.92	-4.35	-4.78	-5.21	-5.64	-6.07	-6.50	-6.93	-7.36	-7.79	-8.22	-8.65	-9.08	-9.51	-9.94	-10.37	-10.80	-11.23	-11.66	-12.09	-12.52	-12.95	-13.38	-13.81	-14.24	-14.67	-15.10	-15.53	-15.96	-16.39	-16.82	-17.25	-17.68	-18.11	-18.54	-18.97	-19.40	-19.83	-20.26	-20.69	-21.12	-21.55	-21.98	-22.41	-22.84	-23.27	-23.70	-24.13	-24.56	-24.99	-25.42	-25.85	-26.28	-26.71	-27.14	-27.57	-28.00	-28.43	-28.86	-29.29	-29.72	-30.15	-30.58	-31.01	-31.44	-31.87	-32.30	-32.73	-33.16	-33.59	-34.02	-34.45	-34.88	-35.31	-35.74	-36.17	-36.60	-37.03	-37.46	-37.89	-38.32	-38.75	-39.18	-39.61	-40.04	-40.47	-40.90	-41.33	-41.76	-42.19	-42.62	-43.05	-43.48	-43.91	-44.34	-44.77	-45.20	-45.63	-46.06	-46.49	-46.92	-47.35	-47.78	-48.21	-48.64	-49.07	-49.50	-49.93	-50.36	-50.79	-51.22	-51.65	-52.08	-52.51	-52.94	-53.37	-53.80	-54.23	-54.66	-55.09	-55.52	-55.95	-56.38	-56.81	-57.24	-57.67	-58.10	-58.53	-58.96	-59.39	-59.82	-60.25	-60.68	-61.11	-61.54	-61.97	-62.40	-62.83	-63.26	-63.69	-64.12	-64.55	-64.98	-65.41	-65.84	-66.27	-66.70	-67.13	-67.56	-67.99	-68.42	-68.85	-69.28	-69.71	-70.14	-70.57	-71.00	-71.43	-71.86	-72.29	-72.72	-73.15	-73.58	-74.01	-74.44	-74.87	-75.30	-75.73	-76.16	-76.59	-77.02	-77.45	-77.88	-78.31	-78.74	-79.17	-79.60	-80.03	-80.46	-80.89	-81.32	-81.75	-82.18	-82.61	-83.04	-83.47	-83.90	-84.33	-84.76	-85.19	-85.62	-86.05	-86.48	-86.91	-87.34	-87.77	-88.20	-88.63	-89.06	-89.49	-89.92	-90.35	-90.78	-91.21	-91.64	-92.07	-92.50	-92.93	-93.36	-93.79	-94.22	-94.65	-95.08	-95.51	-95.94	-96.37	-96.80	-97.23	-97.66	-98.09	-98.52	-98.95	-99.38	-99.81	-100.24	-100.67	-101.10	-101.53	-101.96	-102.39	-102.82	-103.25	-103.68	-104.11	-104.54	-104.97	-105.40	-105.83	-106.26	-106.69	-107.12	-107.55	-107.98	-108.41	-108.84	-109.27	-109.70	-110.13	-110.56	-110.99	-111.42	-111.85	-112.28	-112.71	-113.14	-113.57	-114.00	-114.43	-114.86	-115.29	-115.72	-116.15	-116.58	-117.01	-117.44	-117.87	-118.30	-118.73	-119.16	-119.59	-120.02	-120.45	-120.88	-121.31	-121.74	-122.17	-122.60	-123.03	-123.46	-123.89	-124.32	-124.75	-125.18	-125.61	-126.04	-126.47	-126.90	-127.33	-127.76	-128.19	-128.62	-129.05	-129.48	-129.91	-130.34	-130.77	-131.20	-131.63	-132.06	-132.49	-132.92	-133.35	-133.78	-134.21	-134.64	-135.07	-135.50	-135.93	-136.36	-136.79	-137.22	-137.65	-138.08	-138.51	-138.94	-139.37	-139.80	-140.23	-140.66	-141.09	-141.52	-141.95	-142.38	-142.81	-143.24	-143.67	-144.10	-144.53	-144.96	-145.39	-145.82	-146.25	-146.68	-147.11	-147.54	-147.97	-148.40	-148.83	-149.26	-149.69	-150.12	-150.55	-150.98	-151.41	-151.84	-152.27	-152.70	-153.13	-153.56	-153.99	-154.42	-154.85	-155.28	-155.71	-156.14	-156.57	-157.00	-157.43	-157.86	-158.29	-158.72	-159.15	-159.58	-160.01	-160.44	-160.87	-161.30	-161.73	-162.16	-162.59	-163.02	-163.45	-163.88	-164.31	-164.74	-165.17	-165.60	-166.03	-166.46	-166.89	-167.32	-167.75	-168.18	-168.61	-169.04	-169.47	-169.90	-170.33	-170.76	-171.19	-171.62	-172.05	-172.48	-172.91	-173.34	-173.77	-174.20	-174.63	-175.06	-175.49	-175.92	-176.35	-176.78	-177.21	-177.64	-178.07	-178.50	-178.93	-179.36	-179.79	-180.22	-180.65	-181.08	-181.51	-181.94	-182.37	-182.80	-183.23	-183.66	-184.09	-184.52	-184.95	-185.38	-185.81	-186.24	-186.67	-187.10	-187.53	-187.96	-188.39	-188.82	-189.25	-189.68	-190.11	-190.54	-190.97	-191.40	-191.83	-192.26	-192.69	-193.12	-193.55	-193.98	-194.41	-194.84	-195.27	-195.70	-196.13	-196.56	-196.99	-197.42	-197.85	-198.28	-198.71	-199.14	-199.57	-200.00
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G. F. O. 6-56
 W. R. N. 3-57
 NO. 10752

B.P.R. DIVISION	PROJECT	SHEET NUMBER	TOTAL SHEETS
4	F 03-2(33)	7	61

BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
1	640+65	SPIKE IN ■	100' RT 754.44
2	647+80	" " ■	105' LT 756.03
3	653+10	PT. MK. W. END GAS ISL.	155' RT 766.82
4	659+75	SPIKE IN 40' OAK	220' RT 764.92

BY-PASS CURVE
 P.I. = 642+08.52 -14' RT.
 L = 130°-47'
 Δ = 49°-13'
 D = 28°-00'
 T = 94.7'
 S.E. = 0.065 FT./FT.

BY-PASS CURVE
 P.I. = 643+50 -150' LT. (SURV. C.)
 L = 229°-13'
 Δ = 49°-13'
 D = 28°-00'
 T = 94.7'
 S.E. = 0.065 FT./FT.

BY-PASS ROAD
 P.I. = 647+83.24 -150' LT.
 L = 185°-48'
 Δ = 5°-48'

BY-PASS CURVE
 P.I. = 649+80 -130' LT.
 L = 160°-03'
 Δ = 19°-57'
 D = 6°-00'
 T = 168.00'
 S.E. = .05 FT./FT.

BY-PASS CURVE
 P.I. = 655+80 -281.29' LT.
 L = 204°-16'
 Δ = 24°-16'
 D = 6°-00'
 T = 205.4'
 S.E. = 0.05 FT./FT.

CURVE NOTES FOR TEMP. HWY 110 RD.
 P.I. = 83+13.25 (110 RD.)
 L = 185°-16'
 Δ = 1°-00'
 D = 26.35'
 T = 32.6'
 S.E. = 0.02 FT./FT.

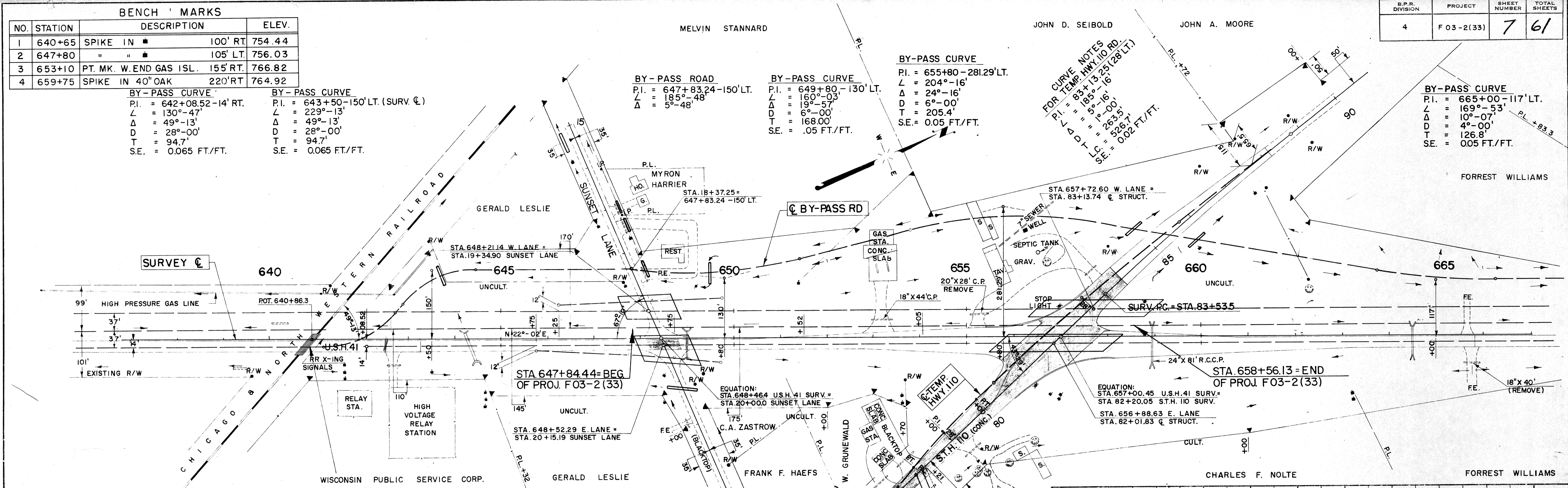
BY-PASS CURVE
 P.I. = 665+00 -117' LT.
 L = 169°-53'
 Δ = 10°-07'
 D = 4°-00'
 T = 126.8'
 S.E. = 0.05 FT./FT.

PLAN

DATE	7-26
BY	G.F.O.
CHECKED	W.B.N.
NO. OF SHEETS	7
NO. OF SHEETS CHECKED	7
NO. OF SHEETS NOT CHECKED	0

PROFILE

DATE	9-26
BY	G.F.O.
CHECKED	W.B.N.
NO. OF SHEETS	7
NO. OF SHEETS CHECKED	7
NO. OF SHEETS NOT CHECKED	0



LEGEND

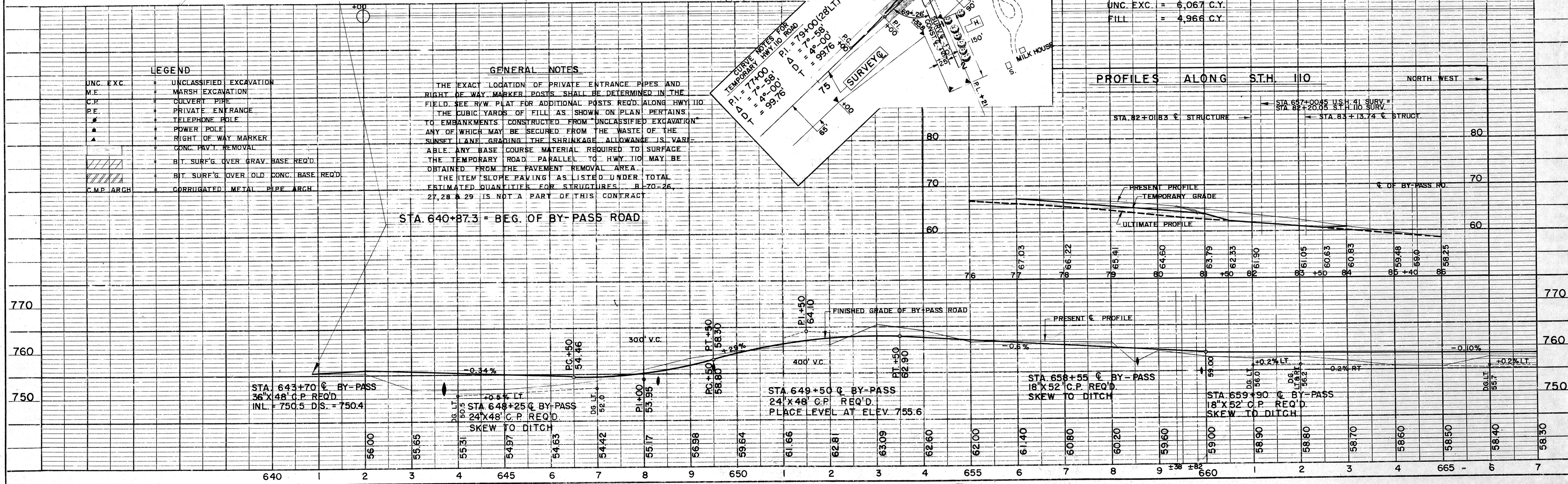
UNC. EXC.	UNCLASSIFIED EXCAVATION
M.E.	MARSH EXCAVATION
C.P.	CULVERT PIPE
P.E.	PRIVATE ENTRANCE
▲	TELEPHONE POLE
▲	POWER POLE
▲	RIGHT OF WAY MARKER
▲	CONC. PAVT. REMOVAL
▨	BT. SURF'G. OVER GRAV. BASE REQ'D
▨	BT. SURF'G. OVER OLD CONC. BASE REQ'D
▨	C.M.P. ARCH

GENERAL NOTES

THE EXACT LOCATION OF PRIVATE ENTRANCE PIPES AND RIGHT OF WAY MARKER POSTS SHALL BE DETERMINED IN THE FIELD. SEE R/W PLAT FOR ADDITIONAL POSTS REQ'D. ALONG HWY 110. THE CUBIC YARDS OF FILL AS SHOWN ON PLAN PERTAINS TO EMBANKMENTS CONSTRUCTED FROM "UNCLASSIFIED EXCAVATION" ANY OF WHICH MAY BE SECURED FROM THE WASTE OF THE SUNSET LANE. GRADING THE SHRINKAGE ALLOWANCE IS VARIABLE. ANY BASE COURSE MATERIAL REQUIRED TO SURFACE THE TEMPORARY ROAD PARALLEL TO HWY 110 MAY BE OBTAINED FROM THE PAVEMENT REMOVAL AREA. THE ITEM SLOPE PAVING AS LISTED UNDER TOTAL ESTIMATED QUANTITIES FOR STRUCTURES B-20-26, 27, 28 & 29 IS NOT A PART OF THIS CONTRACT.

PROFILES ALONG S.T.H. 110

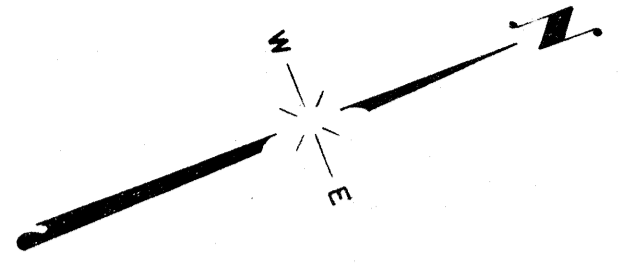
UNC. EXC.	= 6,067 C.Y.
FILL	= 4,966 C.Y.



B.P.R. DIVISION	PROJECT	SHEET NUMBER	TOTAL SHEETS
4	F03-2(33)	8	61

BENCH MARKS

NO	STATION	DESCRIPTION	ELEV.
5	668+65	SPIKE IN 115' RT.	760.99
6	672+16	" " 30' OAK 34' LT.	758.40
7	680+15	" " 115' RT.	760.72



JOHN A. MOORE

BY-PASS CURVE
 P.I. = 665+00 - 117' LT.
 $\Delta = 169^{\circ}-53'$
 $L = 10^{\circ}-07'$
 $D = 4^{\circ}-00'$
 $T = 126.8'$
 SE. = 0.05 FT./FT.

BY-PASS CURVE
 P.I. = 677+50 - 117' LT.
 $\Delta = 189^{\circ}-54'$
 $L = 9^{\circ}-54'$
 $D = 4^{\circ}-00'$
 $T = 124.1'$
 SE. = 0.065 FT./FT.

BY-PASS CURVE
 P.I. = 685+00 - 14' RT.
 $\Delta = 170^{\circ}-06'$
 $L = 3^{\circ}-54'$
 $D = 4^{\circ}-00'$
 $T = 124.1'$
 SE. = 0.065 FT./FT.

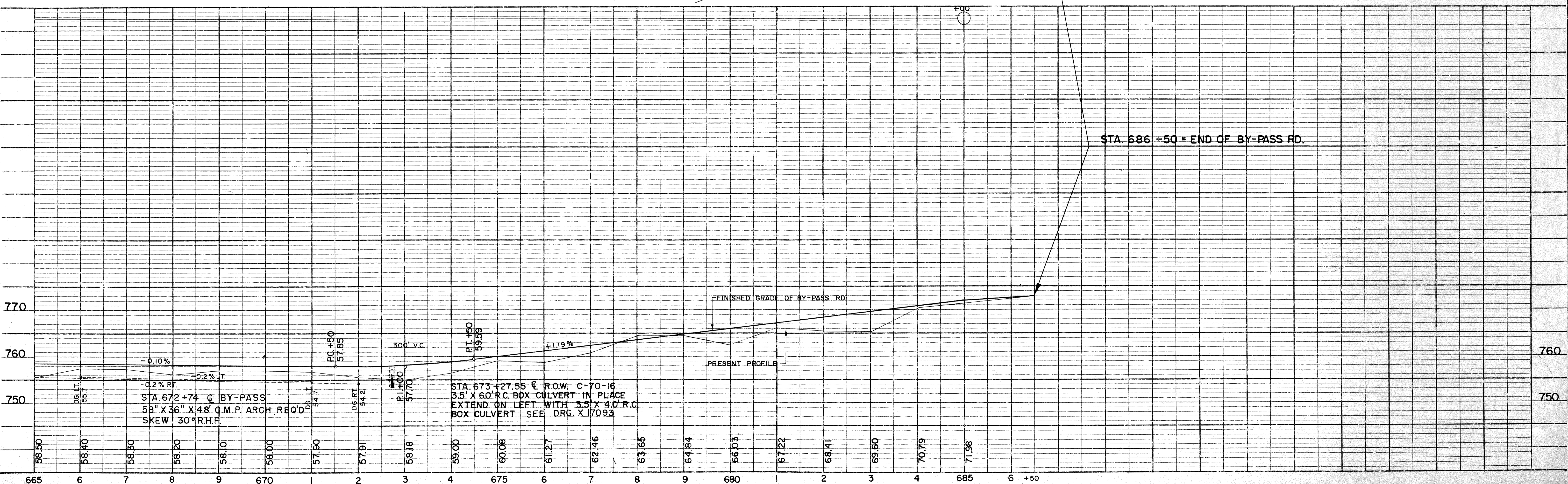
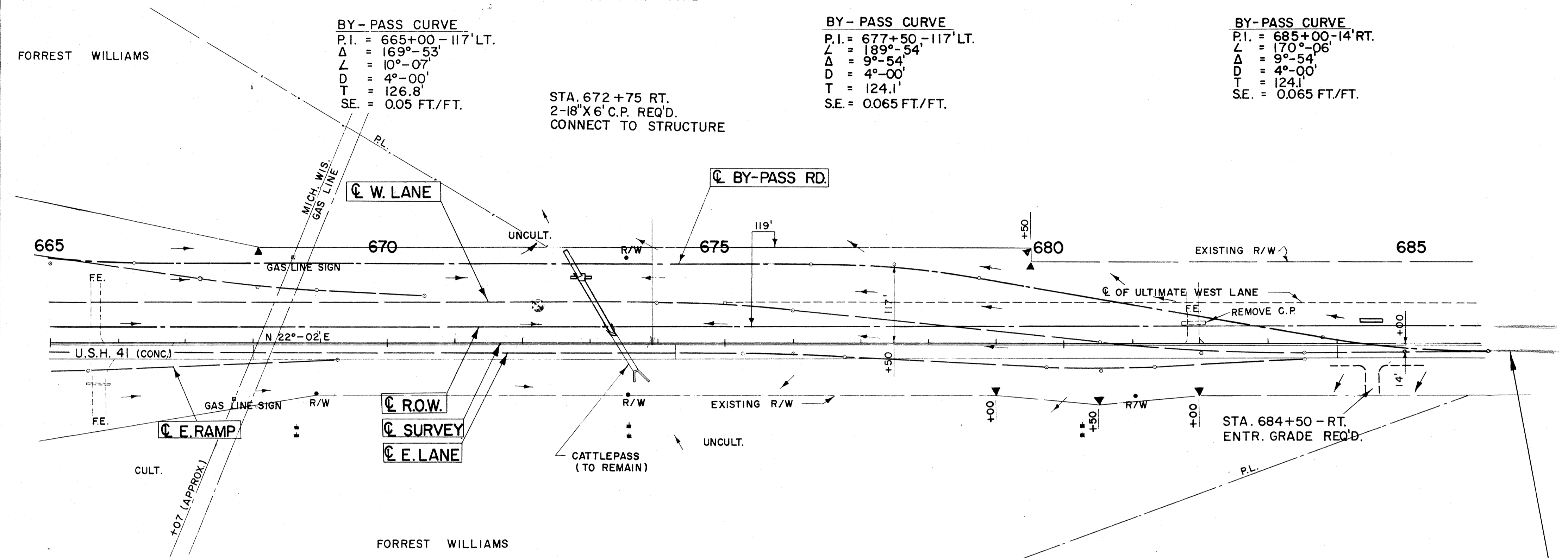
STA. 672+75 RT.
 2-18" X 6' C.P. REQ'D.
 CONNECT TO STRUCTURE

PLAN

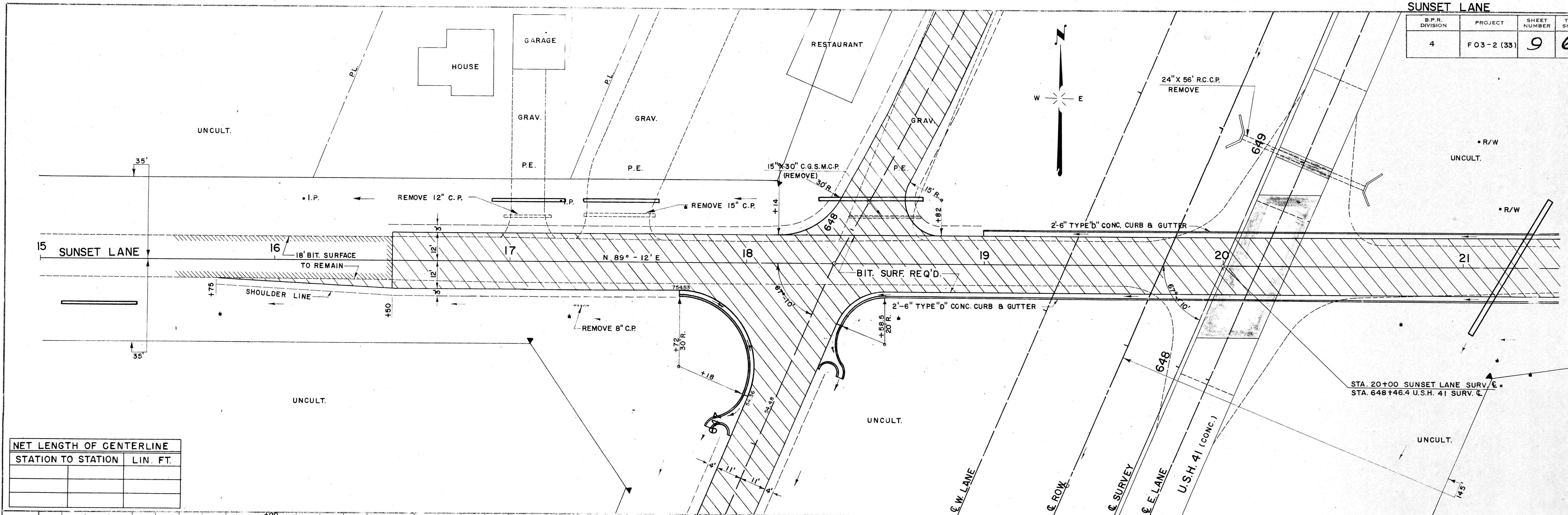
DATE	6-56
BY	G.F.O.
W.R.N.	
R.J.H.	
W.R.N.	
R.J.H.	
NO. 10752	

PROFILE

DATE	6-56
BY	G.F.O.
W.R.N.	
R.J.H.	
W.R.N.	
R.J.H.	
NO. 10752	



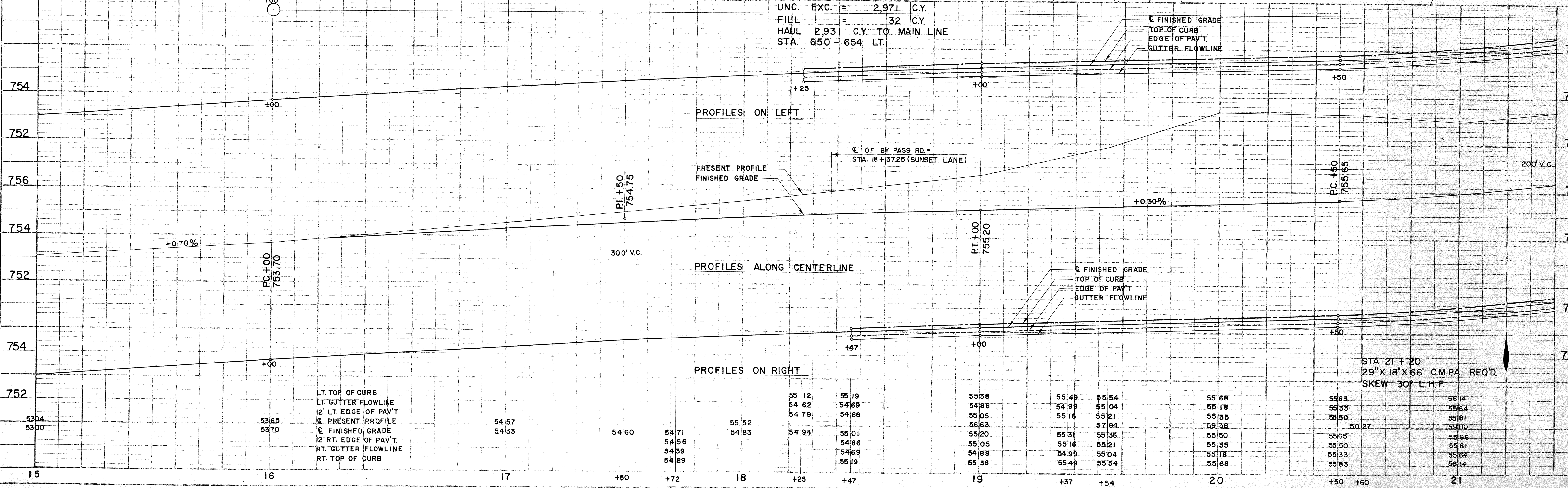
DATE: 6-56
 BY: G.F.O.
 SURVEYED: W.R.N.
 PLOTTED: W.R.N.
 CHECKED: W.R.N.
 NO. 10759



NET LENGTH OF CENTERLINE

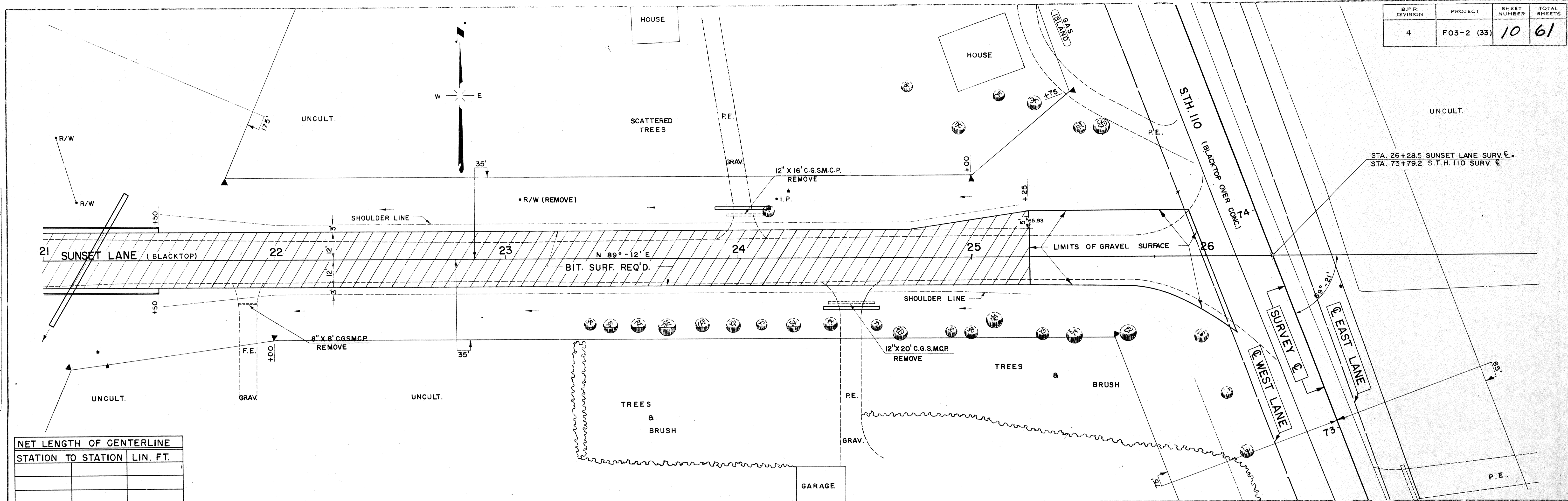
STATION TO STATION	LIN. FT.

DATE: 6-56
 BY: G.F.O.
 SURVEYED: W.R.N.
 PLOTTED: W.R.N.
 CHECKED: W.R.N.
 NO. 10759

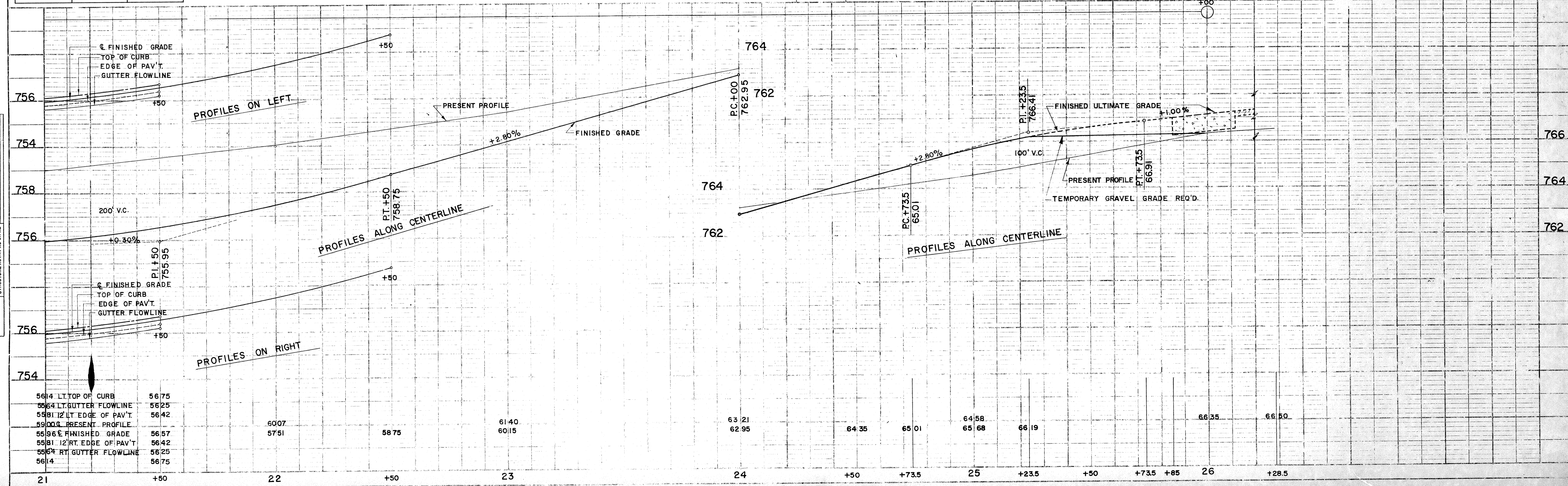


DATE: 6-1-56
 BY: G.F.O.
 W.R.N.
 CHECKED: W.R.N.
 NO. 10739

DATE: 6-1-56
 BY: G.F.O.
 W.R.N.
 CHECKED: W.R.N.
 NO. 10739



NET LENGTH OF CENTERLINE	
STATION TO STATION	LIN. FT.



5614 LT. TOP OF CURB	5675
5624 LT. GUTTER FLOWLINE	5625
5581 12" LT. EDGE OF PAV'T	5642
5900 PRESENT PROFILE	6007
5686 FINISHED GRADE	5657
5581 12" RT. EDGE OF PAV'T	5642
5567 RT. GUTTER FLOWLINE	5625
5614	5675

BILL OF BARS

DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT

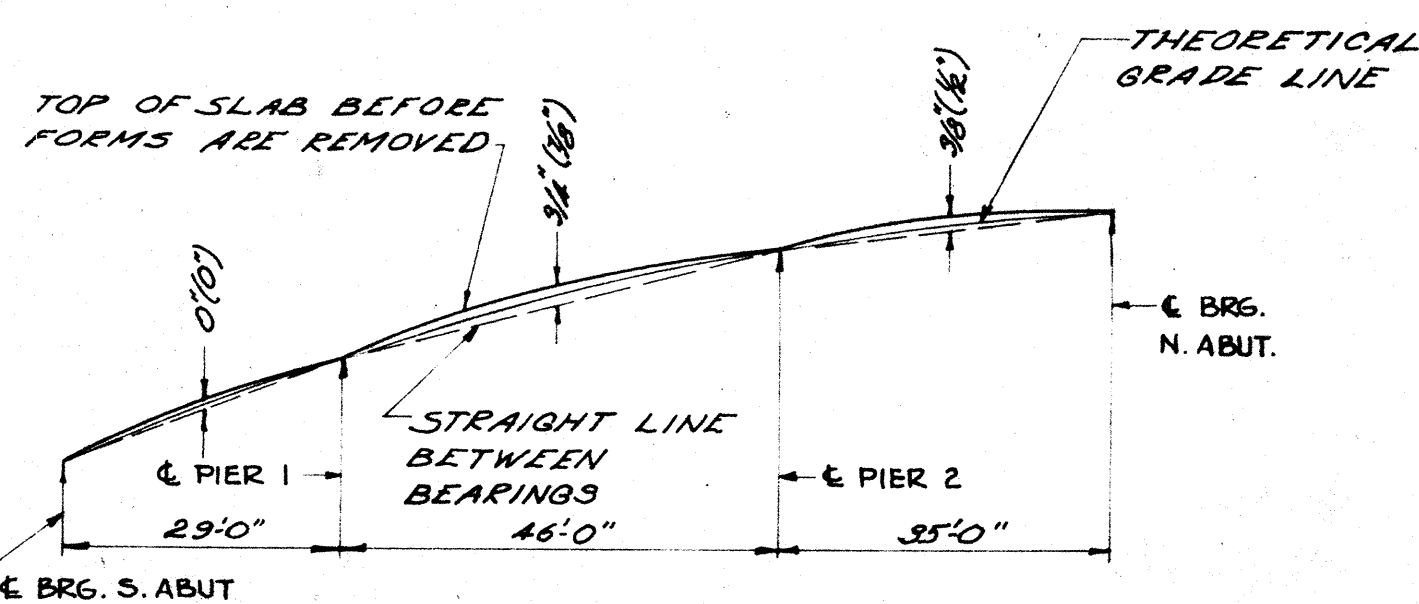
SUPERSTRUCTURE (54,170#)

MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
S 1	29	7	18-6	1-0	LONGIT. - BOTTOM - SPAN 1	
S 2	29	7	23-9	1-0	" " " " " " " "	
S 3	20	9	29-3	SHOWN	" " " " " " " "	
S 4	38	4	19-0	1-6	" " " " " " " "	A
S 5	29	9	25-0	1-0	" " " " " " " "	
S 6	29	9	34-6	1-0	" " " " " " " "	
S 7	35	8	29-3	0-10	" " " " " " " "	
S 8	34	8	22-6	0-10	" " " " " " " "	
S 9	66	4	33-0	SHOWN	TRANSV. - " " " "	
S 10	34	5	33-0	" " " " " " " "		
S 11	54	4	33-0	2-0	" " " " " " " "	
S 12	18	4	10-6	1-6	LONGIT. - TOP - SPAN 1	
S 13	27	9	33-6	1-0	" " " " " " " "	
S 14	28	9	15-6	1-0	" " " " " " " "	
S 15	18	4	17-0	1-6	" " " " " " " "	
S 16	23	10	35-0	1-2	" " " " " " " "	
S 17	24	10	15-6	1-2	" " " " " " " "	
S 18	18	4	18-0	1-6	" " " " " " " "	
S 19	78	5	7-0	1-0	CURB - STIRRUPS	B
S 20	20	9	28-9	SHOWN	LONGIT. - CURB - TOP	
S 21	108	4	3-9	1-0	TRANSV. - " " " "	C
S 22	30	5	2-9	SHOWN	CURB & RAIL POSTS	D
S 23	30	5	2-3	" " " " " " " "		
S 47	4	5	7-6	1-0	CURB - STIRRUPS - HAUNCHED SECT. AT PIERS	B
S 48	2	5	7-9	1-0	" " " " " " " "	B
S 49	4	5	8-0	1-0	" " " " " " " "	B
S 50	4	5	8-3	1-0	" " " " " " " "	B
S 51	2	5	8-9	1-0	" " " " " " " "	B
S 52	4	5	9-0	1-0	" " " " " " " "	B
S 53	4	5	9-3	1-0	" " " " " " " "	B
S 54	2	5	8-0	1-0	" " " " " " " "	B
S 55	2	5	7-9	1-0	" " " " " " " "	B
S 56	2	5	7-9	1-0	" " " " " " " "	B
S 57	2	5	7-6	1-0	" " " " " " " "	B
S 1	15	7	18-6	1-0	LONGIT. - BOTTOM - SPAN 1	
S 2	15	7	23-9	1-0	" " " " " " " "	
S 4	21	4	19-0	1-6	" " " " " " " "	A
S 5	17	9	25-0	1-0	" " " " " " " "	
S 6	16	9	34-6	1-0	" " " " " " " "	
S 7	21	8	29-3	0-10	" " " " " " " "	
S 8	22	8	22-6	0-10	" " " " " " " "	
S 12	10	4	10-6	1-6	LONGIT. - TOP - SPAN 1	
S 13	15	9	33-6	1-0	" " " " " " " "	
S 14	15	9	15-6	1-0	" " " " " " " "	
S 15	11	4	17-0	1-6	" " " " " " " "	
S 16	15	10	35-0	1-2	" " " " " " " "	
S 17	15	10	15-6	1-2	" " " " " " " "	
S 18	12	4	18-0	1-6	" " " " " " " "	
S 19	76	5	7-0	1-0	CURB - STIRRUPS	B
S 21	106	4	3-9	1-0	TRANSV. - CURB - TOP	C
S 22	30	5	2-9	SHOWN	CURB & RAIL POSTS	D
S 23	30	5	2-3	" " " " " " " "		
S 24	20	9	28-9	" " " " " " " "		
S 25	5	4	17-0	1-0	LONGIT. - BOTTOM - CURB	*
S 26	20	4	17-6	SHOWN	" " " " " " " "	
S 27	4	4	18-0	" " " " " " " "		
S 28	10	4	18-6	" " " " " " " "		
S 29	9	5	19-0	1-0	" " " " " " " "	
S 30	10	5	20-0	1-0	" " " " " " " "	
S 31	5	4	19-6	1-0	" " " " " " " "	
S 32	9	4	20-0	SHOWN	" " " " " " " "	
S 33	7	4	20-6	1-0	" " " " " " " "	
S 34	15	5	21-0	1-0	" " " " " " " "	
S 35	6	4	21-6	1-0	" " " " " " " "	
S 36	5	4	17-6	2-0	" " " " " " " "	
S 37	6	4	17-6	2-0	" " " " " " " "	
S 38	5	4	18-0	2-0	" " " " " " " "	
S 39	5	4	18-6	2-0	" " " " " " " "	
S 40	6	4	19-0	2-0	" " " " " " " "	
S 41	6	4	19-6	2-0	" " " " " " " "	
S 42	5	4	20-0	2-0	" " " " " " " "	
S 43	5	4	20-6	2-0	" " " " " " " "	
S 44	6	4	21-0	2-0	" " " " " " " "	
S 45	5	4	21-6	2-0	" " " " " " " "	
S 46	20	9	28-3	SHOWN	LONGIT. - CURB - TOP	
S 47	4	5	7-6	1-0	CURB-STIRRUPS - HAUNCHED SECT. AT PIERS	B
S 48	2	5	7-9	1-0	" " " " " " " "	B
S 49	4	5	8-0	1-0	" " " " " " " "	B
S 50	4	5	8-3	1-0	" " " " " " " "	B
S 51	2	5	8-9	1-0	" " " " " " " "	B
S 52	4	5	9-0	1-0	" " " " " " " "	B
S 53	4	5	9-3	1-0	" " " " " " " "	B
S 54	2	5	8-9	1-0	" " " " " " " "	B
S 55	2	5	7-9	1-0	" " " " " " " "	B
S 56	2	5	7-9	1-0	" " " " " " " "	B
S 57	2	5	7-6	1-0	" " " " " " " "	B

* BEND IN FIELD AS NECESSARY

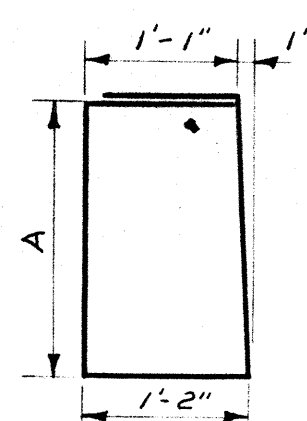
GENERAL NOTES

- DRAWINGS SHALL NOT BE SCALED.
- EXCAVATED VOLUME NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH GRANULAR BACKFILL TO THE ORIGINAL GROUND LINE. PAYMENT WILL BE MADE ONLY FOR THE MATERIAL ACTUALLY PLACED WITHIN THE LIMITS SPECIFIED IN THE STANDARD SPECIFICATIONS.
- THE USE OF STRUCTURAL GRADE BAR STEEL REINFORCEMENT IS PROHIBITED.
- BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED. BAR STEEL REINFORCEMENT IN FTGS. SHALL BE EMBEDDED 3" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.
- EXCAVATION QUANTITIES FOR PIERS AND ABUTMENTS FIGURED FROM EXISTING GROUND.
- ALL CONCRETE MASONRY SHALL BE GRADE "AA" EXCEPT RAIL AND RAIL POSTS SHALL BE GRADE "X".
- ALL TRANSVERSE BARS IN SLAB SHALL BE PLACED PARALLEL TO & OF PIERS & ABUTMENTS, SPACING CALLED FOR APPLIES ALONG CENTERLINE OF RDWK.
- BEVEL EXPOSED EDGES OF CONCRETE 1" UNLESS OTHERWISE SHOWN OR NOTED.

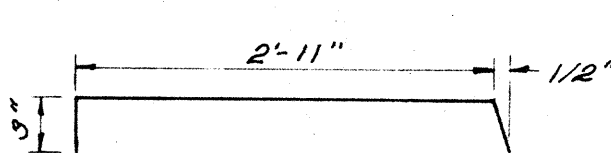


SHOWING ALLOWANCES TO BE MADE (BY CAMBERING OF SLAB FALSEWORK) FOR D.L. DEFLECTION, CREEP, & PLASTIC FLOW. NUMBERS IN PARENTHESES INCLUDE THE EFFECT OF THE VERTICAL CURVE ON THESE ALLOWANCES, BUT DO NOT INCLUDE ALLOWANCE FOR FALSEWORK SETTLEMENT.

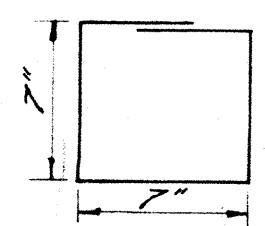
D.L. DEFLECTION AND FORMING DIAGRAM



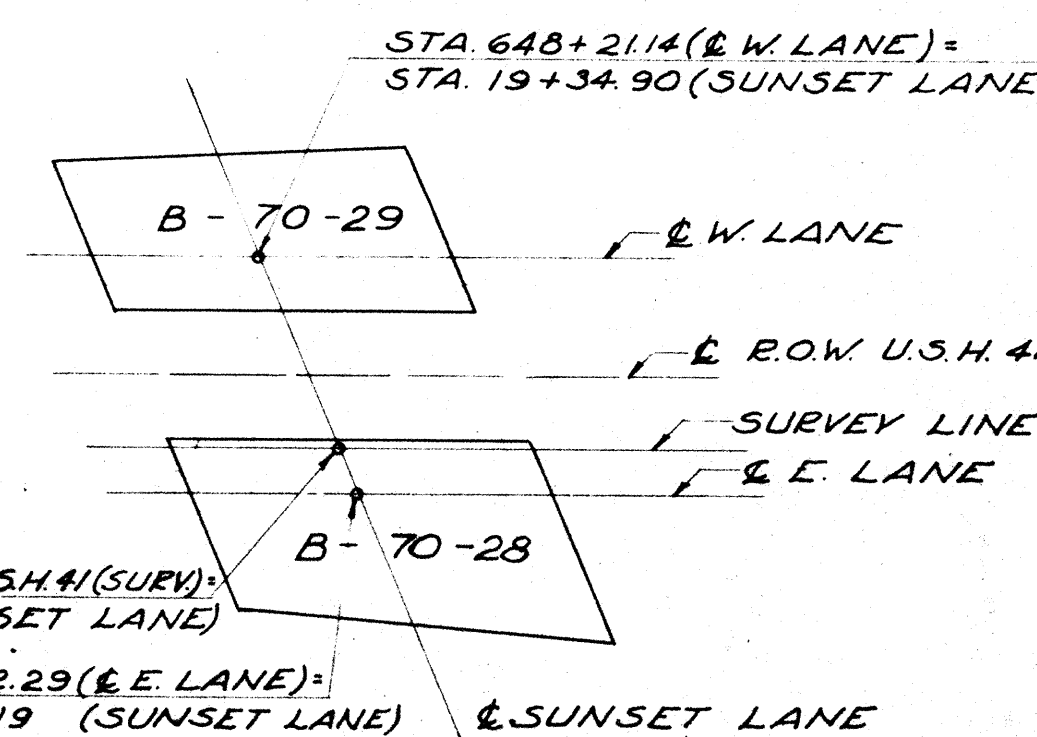
DETAIL "B"



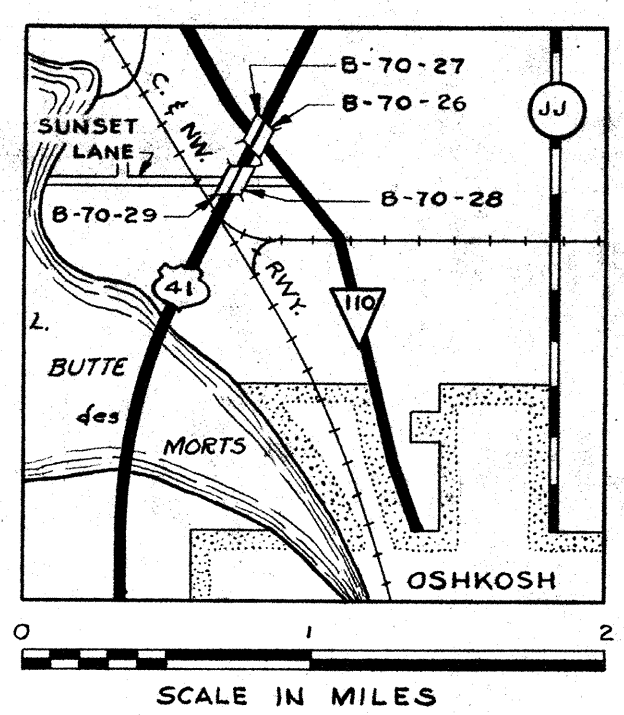
DETAIL C



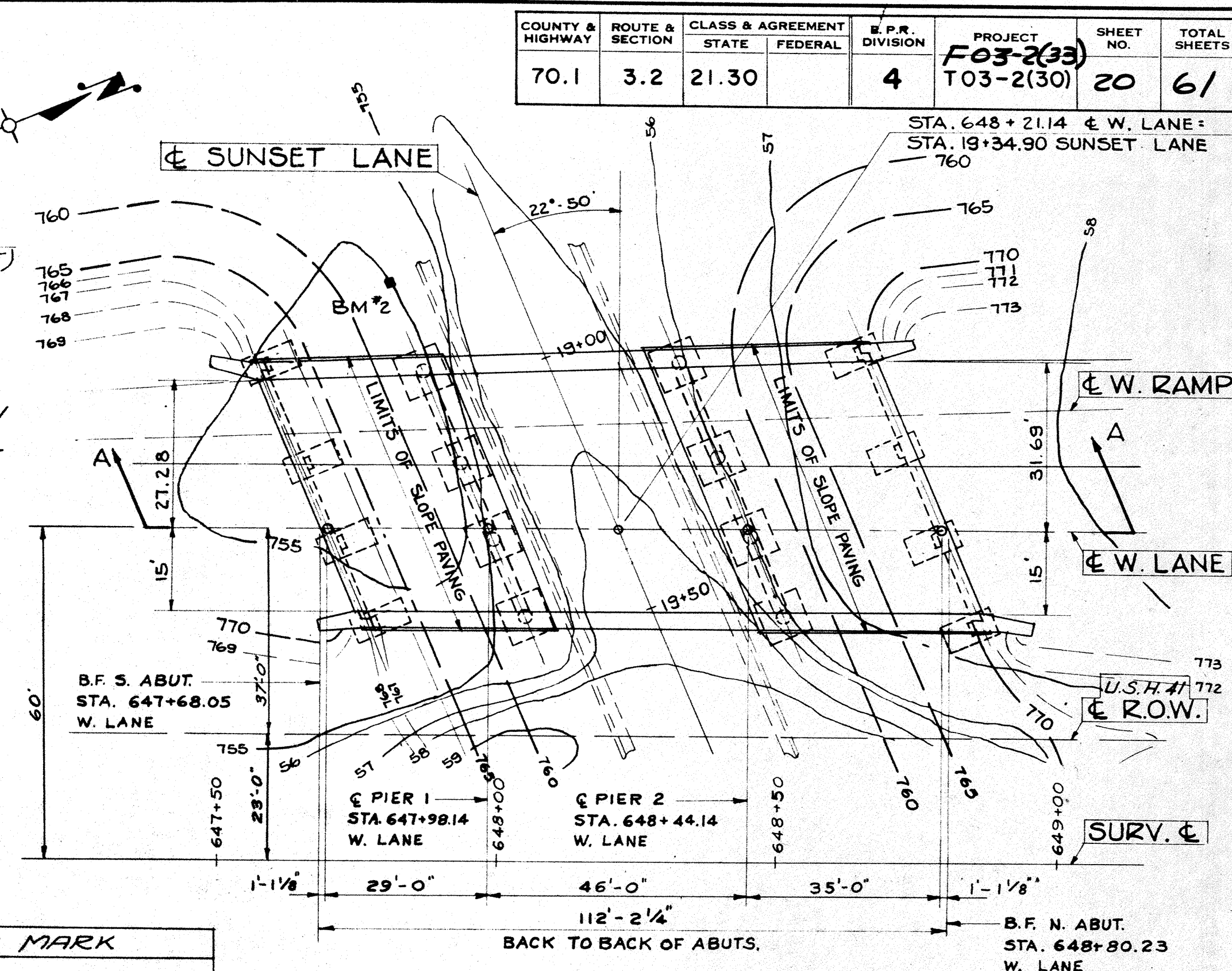
DETAIL D



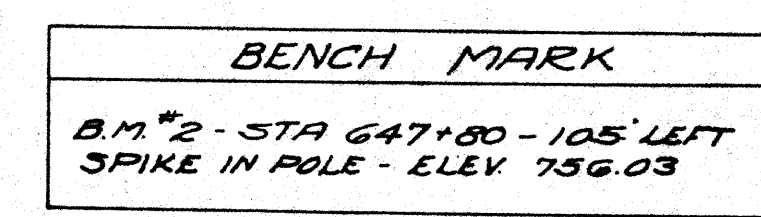
LOCATION B-70-28 & B-70-29



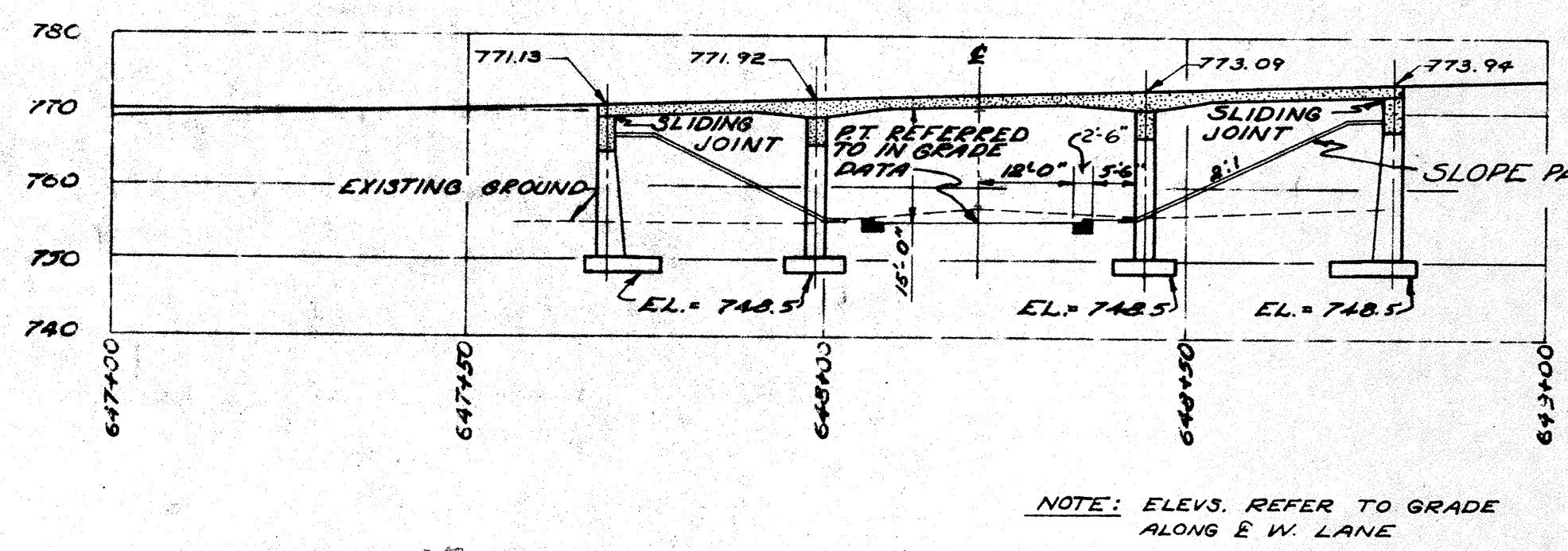
LOCALITY MAP



LAYOUT



BENCH MARK
B.M. #2 - STA. 647+80 - 105' LEFT SPIKE IN POLE - ELEV. 756.03



SECTION "A-A"

3 SPAN (29'-0" - 46'-0" - 35'-0") CONT. CONC. SLAB R.C. OPEN TYPE PIERS & ABUTMENTS

GRADE DATA

U.S.H. #1
STA. ELEV. GRADE
V.P. 647+00 767.15 +3.00%
V.P. 635+25 773.90
V.P. 663+50 769.15 -3.00%

SUNSET LANE

STA. ELEV. GRADE
19+00 755.20
20+50 755.65 +0.30%

RAMP DATA

TANGENT
P.I. - STA. 646+25
OFFSET OF P.I. FROM
& E. LANE - 13' LT.
& BETWEEN & E. LANES
& & E. RAMP -
2'-44'-53"

CURVE

P.I. - STA. 648+75.35
OFFSET OF P.I. FROM
& E. LANE - 26' 40" RT.
Δ = 3'-52'-53"
D = 2'-00"
T = 97.08'
L.C. = 194.07'
S.E. = 0.04 FT./FT.

TOTAL ESTIMATED QUANTITIES

BID ITEMS	UNIT	SUPER-STRUCTURE	SOUTH ABUTMENT	PIER 1	PIER 2	NORTH ABUTMENT	TOTAL
EXCAVATION FOR STRUCTURES	C.Y.	---	110	110	120	155	495
CONCRETE MASONRY - GRADE "AA"	C.Y.	335.2*	52.0	51.3	53.1	58.8	550.4
CONCRETE MASONRY - GRADE "X"	C.Y.	4.3	---	---	---	---	4.3
BAR STEEL REINFORCEMENT	L.B.	54,170	6060	6560	6690	6800	80,280
SHEET ZINC	L.B.	239	---	---	---	---	239
SLOPE PAVING	S.Y.	---	190	---	---	235	425
GRANULAR BACKFILL	C.Y.	---	85	85	100	130	400
NON-BID ITEMS							
EXPANSION JOINT FILLER	LUMP SUM	1' x 1 1/2"	---	---	---	---	1' x 1 1/2"
1/2" PREFORMED BIT FIBER TYPE FILLER	S.F.	---	---	101	105	---	206

* DESIGN STRESS: $f_c = 1400$ psi.

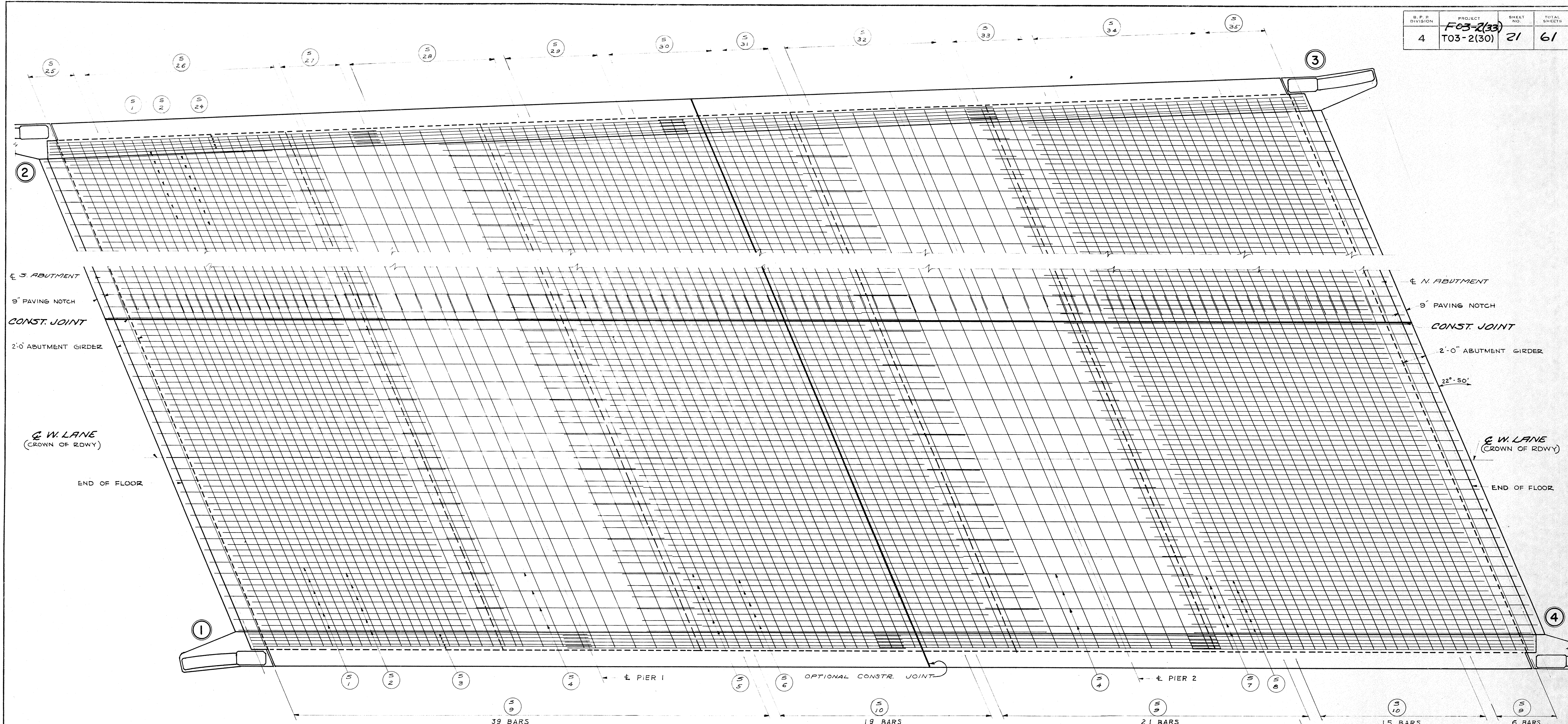
LIST OF DRAWINGS

- 1 LAYOUT X 17413
- 2 SUPERSTRUCTURE X 17414
- 3 SUPERSTRUCTURE X 17415
- 4 PIER 1 & RAILING DETAIL X 17416
- 5 PIER 2 X 17417
- 6 SOUTH ABUTMENT X 17418
- 7 NORTH ABUTMENT X 17419
- 8 LOG OF SOUNDINGS X 17412

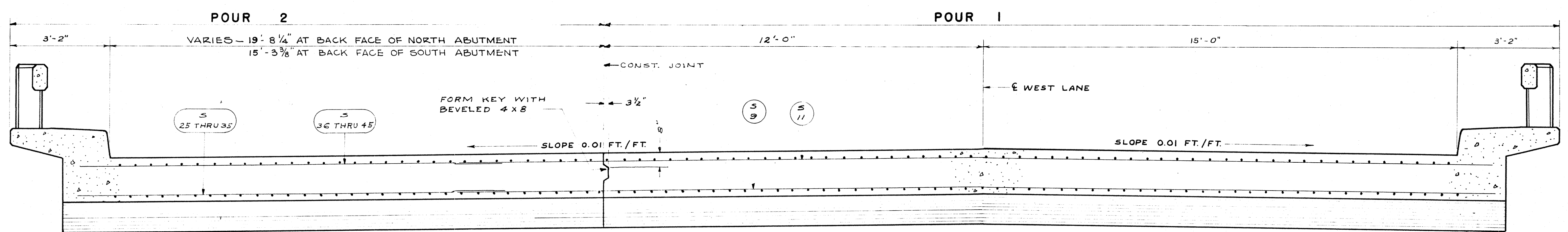
* THE DESIGN OF THIS STRUCTURE IS IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, A.A.S.H.O., EDITION OF 1953.

MEAD AND HUNT, INC.
ENGINEERS - ARCHITECTS
MADISON, WISCONSIN

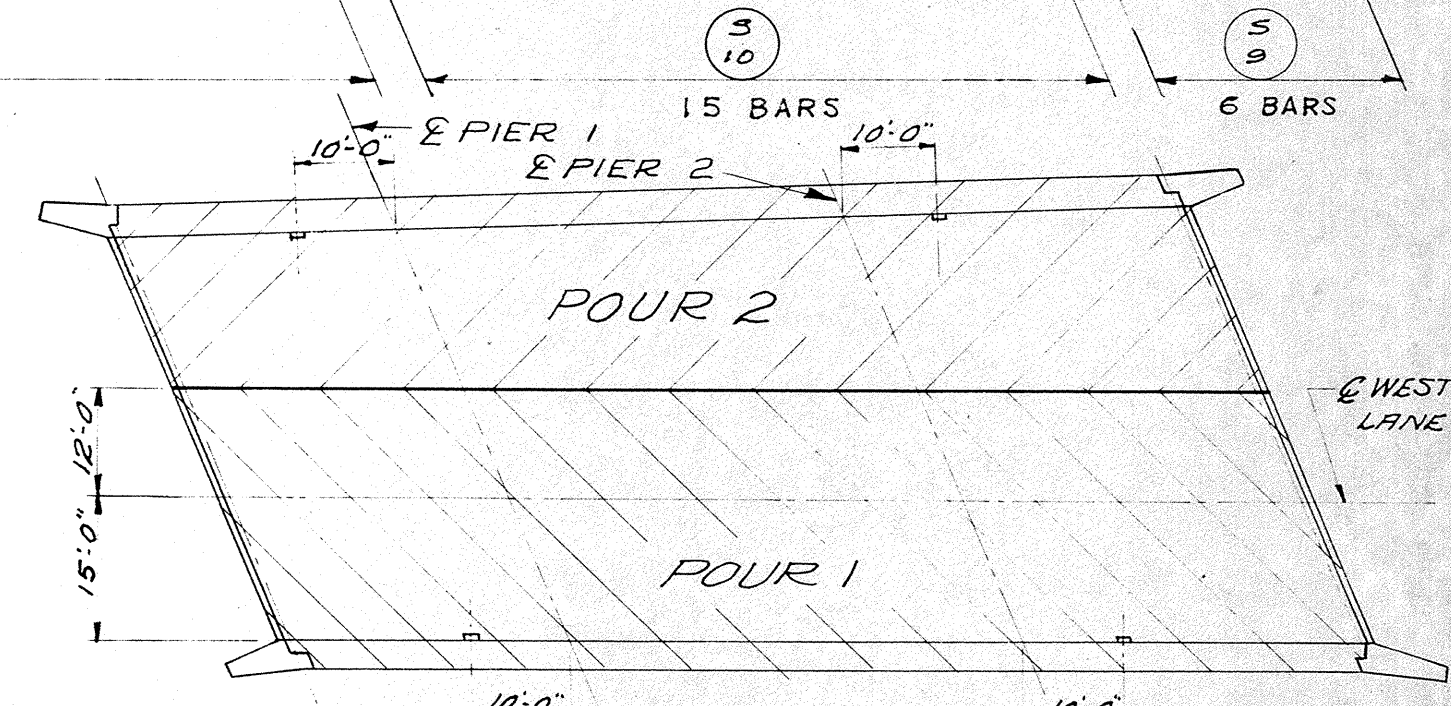
REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN		
LAYOUT			
CO. WINNEBAGO	TN. OSHKOSH	STA. 648+24.14	W. LANE
SECTION 3	TOWN 18N	RANGE 16E	
DATE 9-23-57	DRWN. D3	CRD. HES	SPEC. 1957 LOAD 120-518
CORRECT:	T. B. Schultz BRIDGE ENGINEER		
APPROVED:	E. L. Ruetten STATE HIGHWAY ENGINEER		
STRUCTURE B-70-29		SHEET 1 OF 8	



PLAN
SHOWING REINFORCEMENT IN BOTTOM OF SLAB

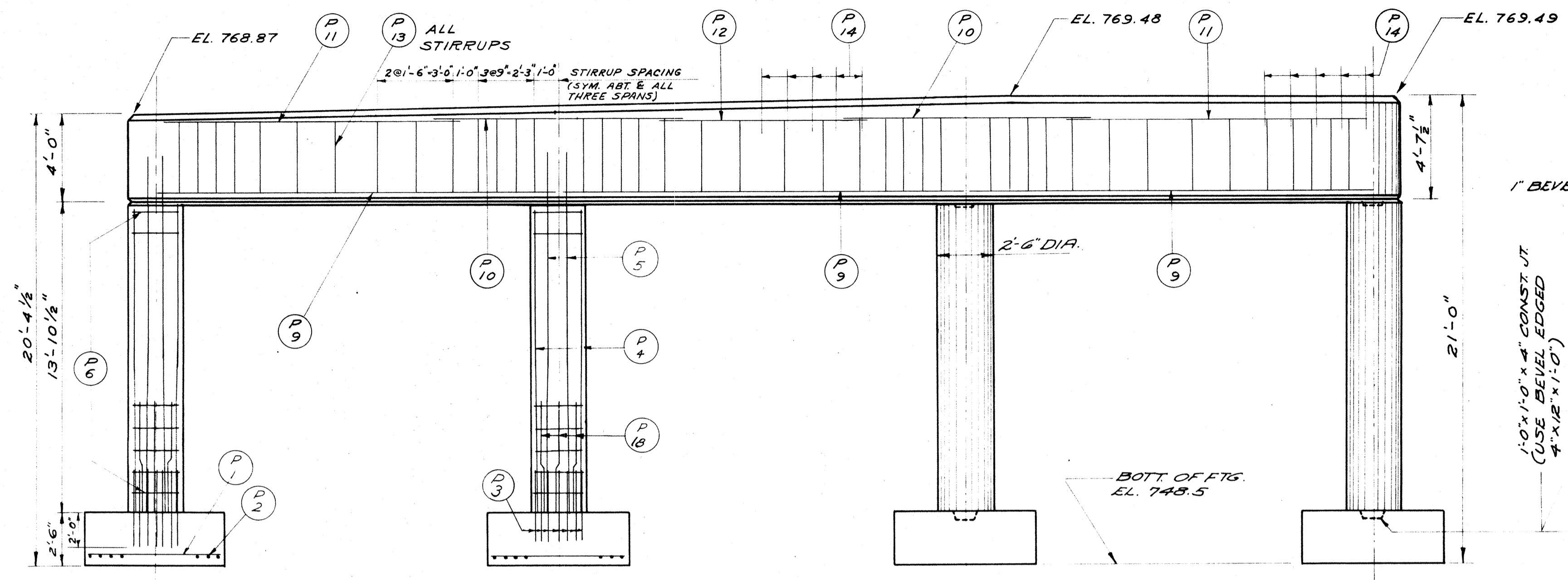


TRANSVERSE SECTION THRU ROADWAY



POURING DIAGRAM
SHOWING FLOOR DRAIN LOCATIONS

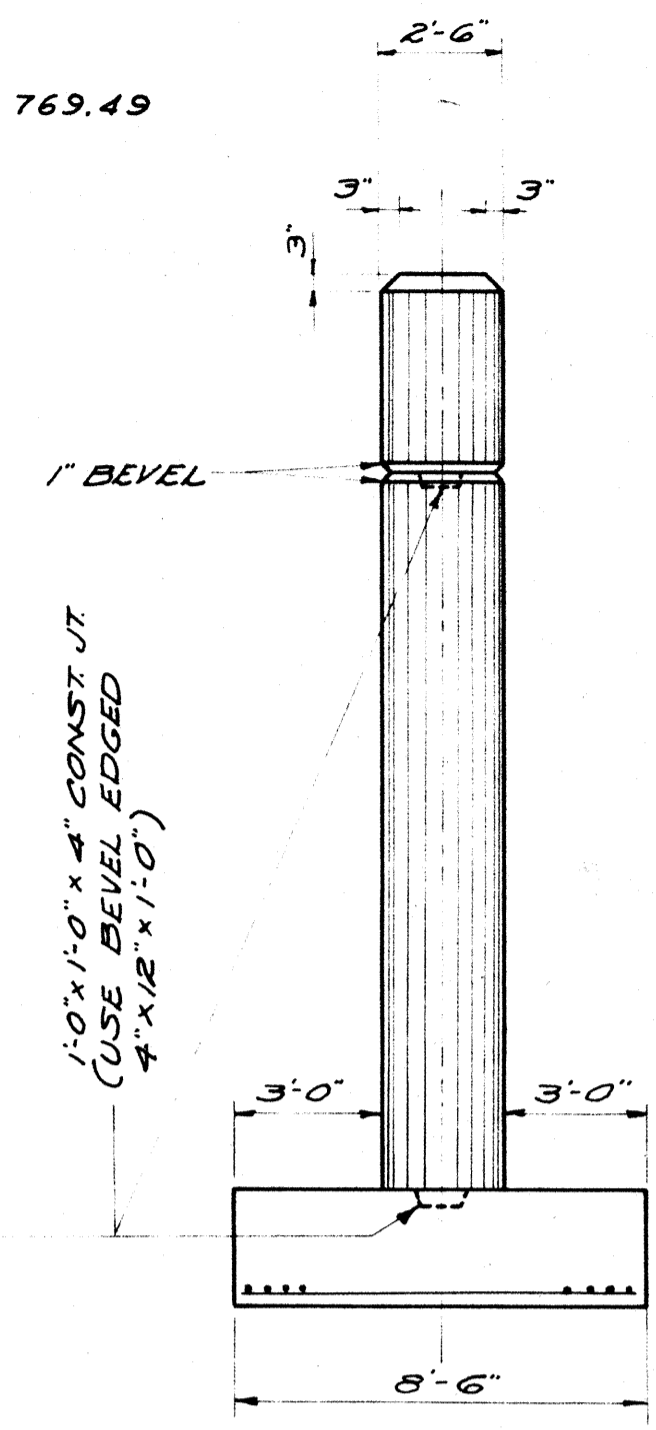
REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN		
	SUPERSTRUCTURE		
	DESIGN SPEC. AA SHO 53	LOADING H&O SIG	CONST. SPEC. 1957
	DATE 9-23-57	DESIGN FCK	DRAWN AMT CKD HFB
STRUCTURE	B - 70 - 29	SHEET	2 OF 8



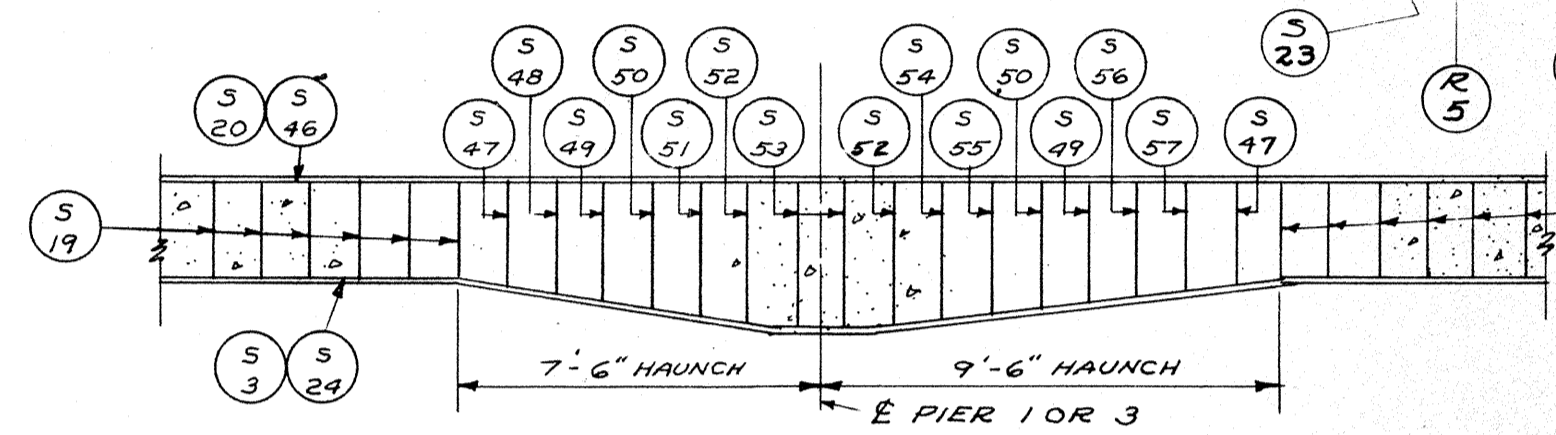
SHOWING COLUMN AND FOOTING STEEL

NOTE: SOIL BEARING PRESSURE: 4.0 TONS/SQ. FT.

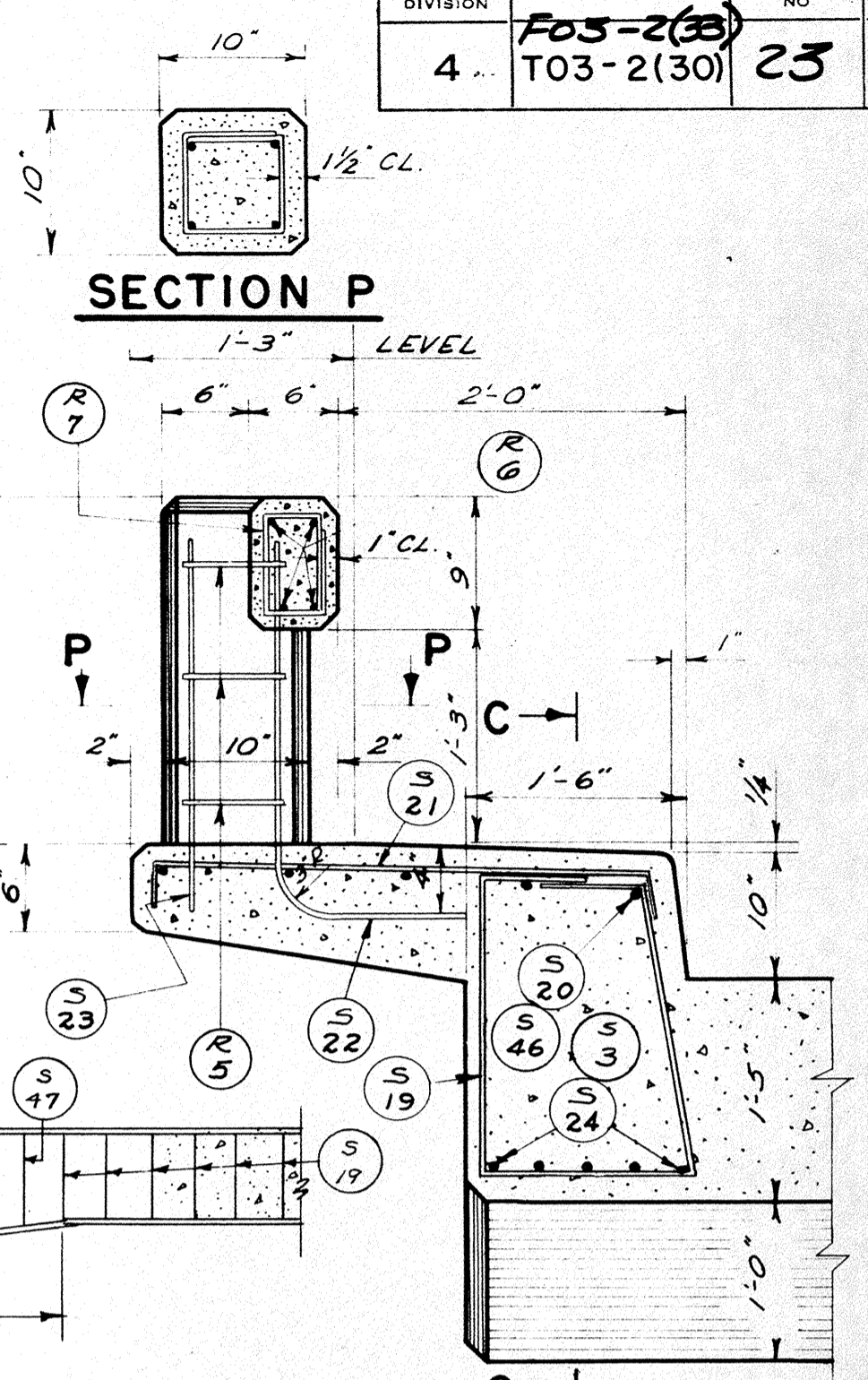
ELEVATION
LOOKING NORTH



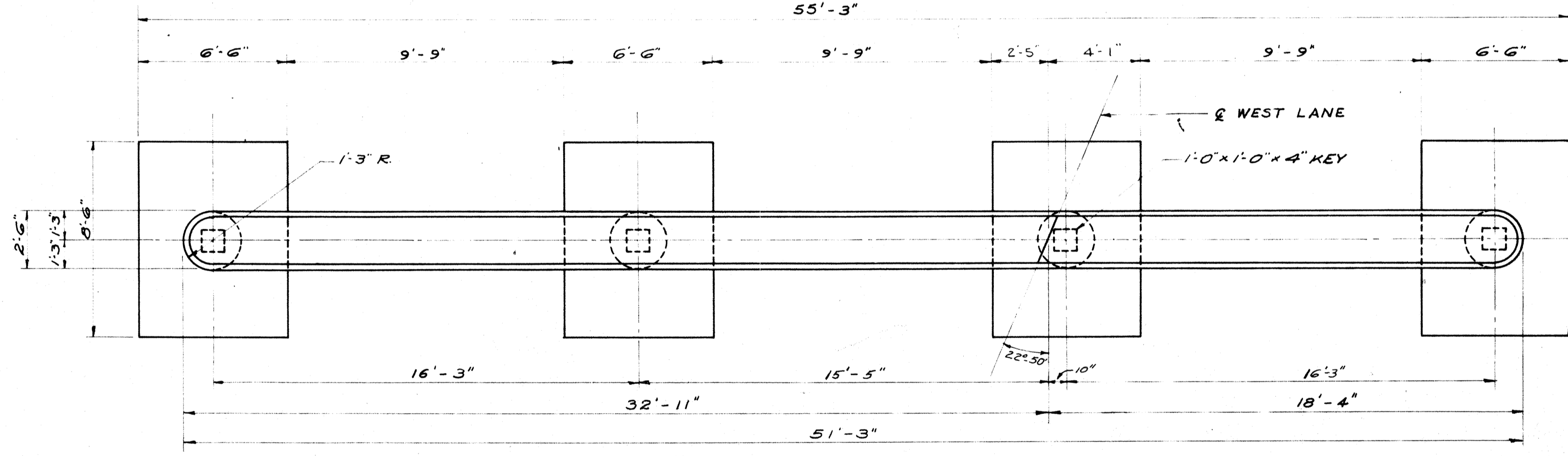
END VIEW



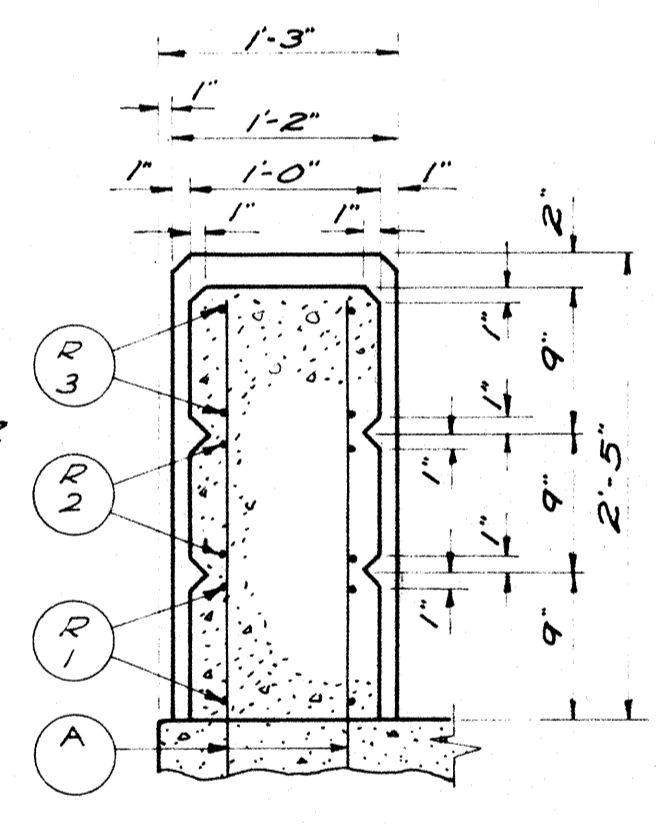
SECTION "C"



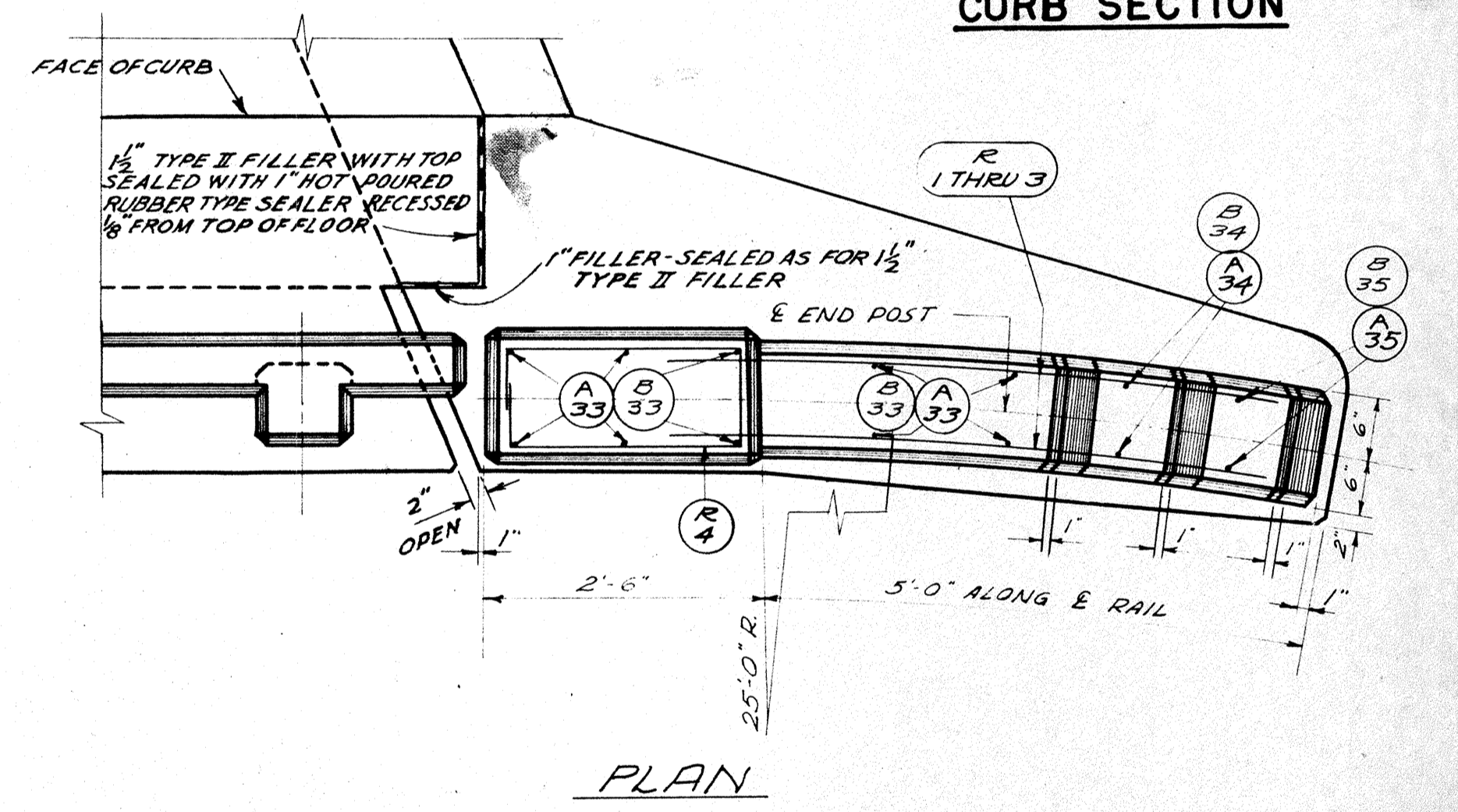
CURB SECTION



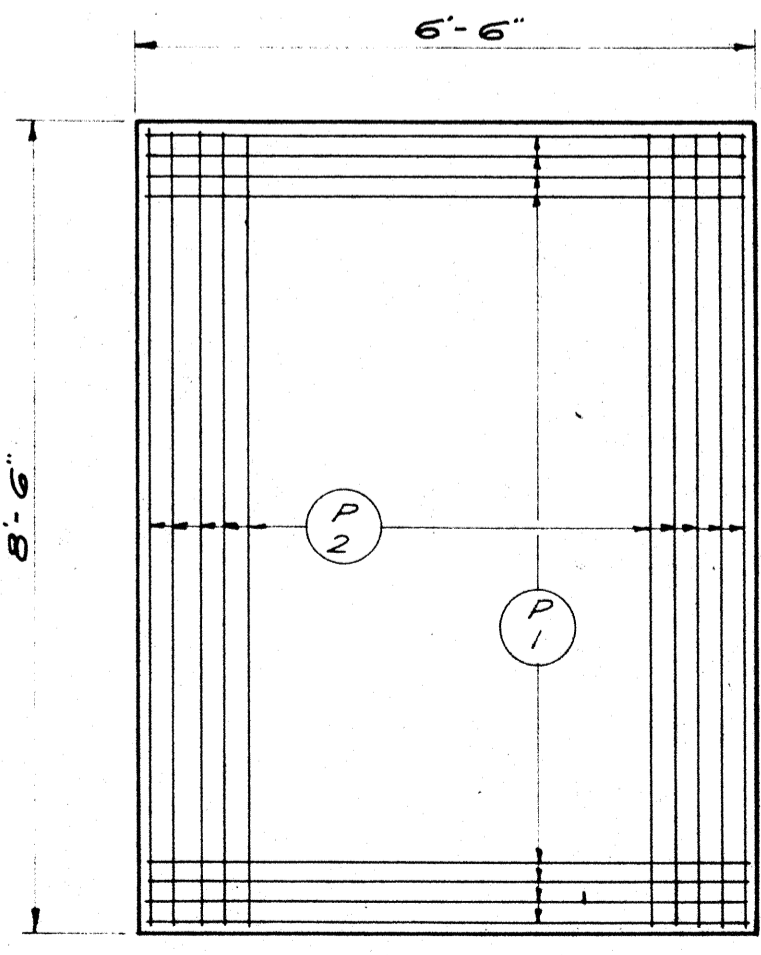
PLAN



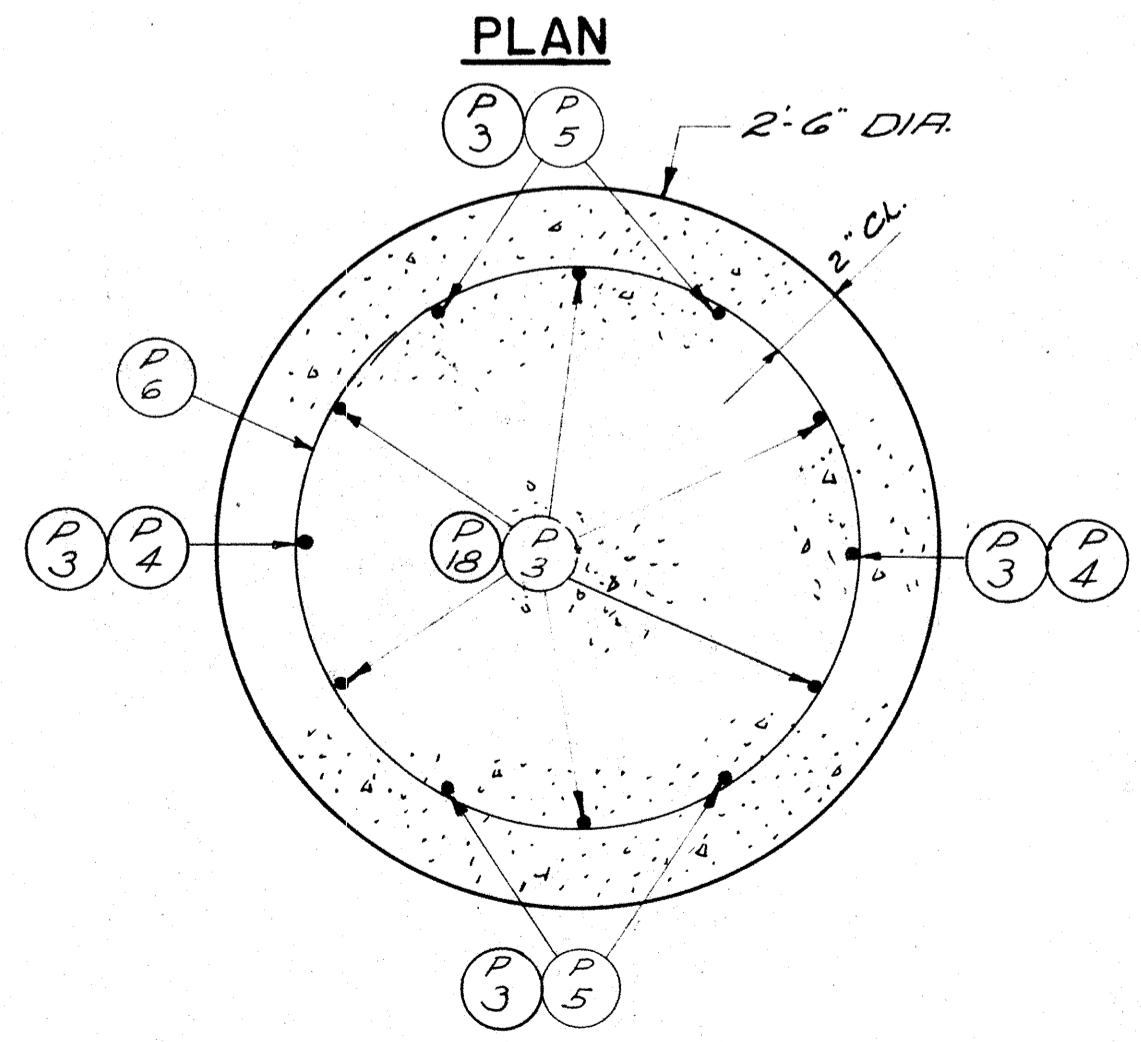
SECTION R1



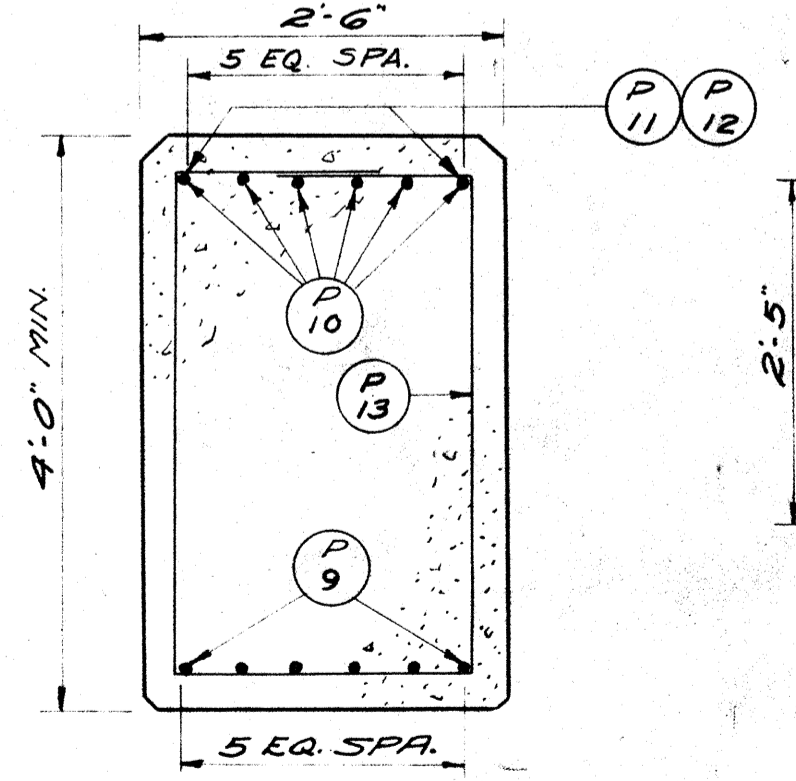
PLAN



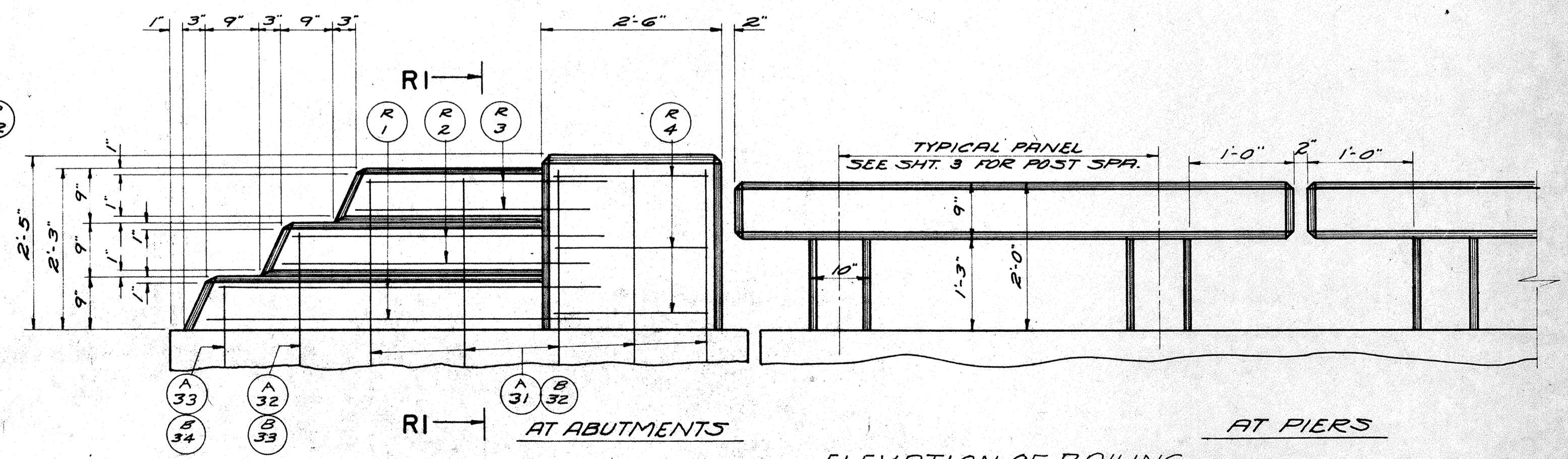
FOOTING PLAN
TYPICAL



SECTION THRU COLUMN



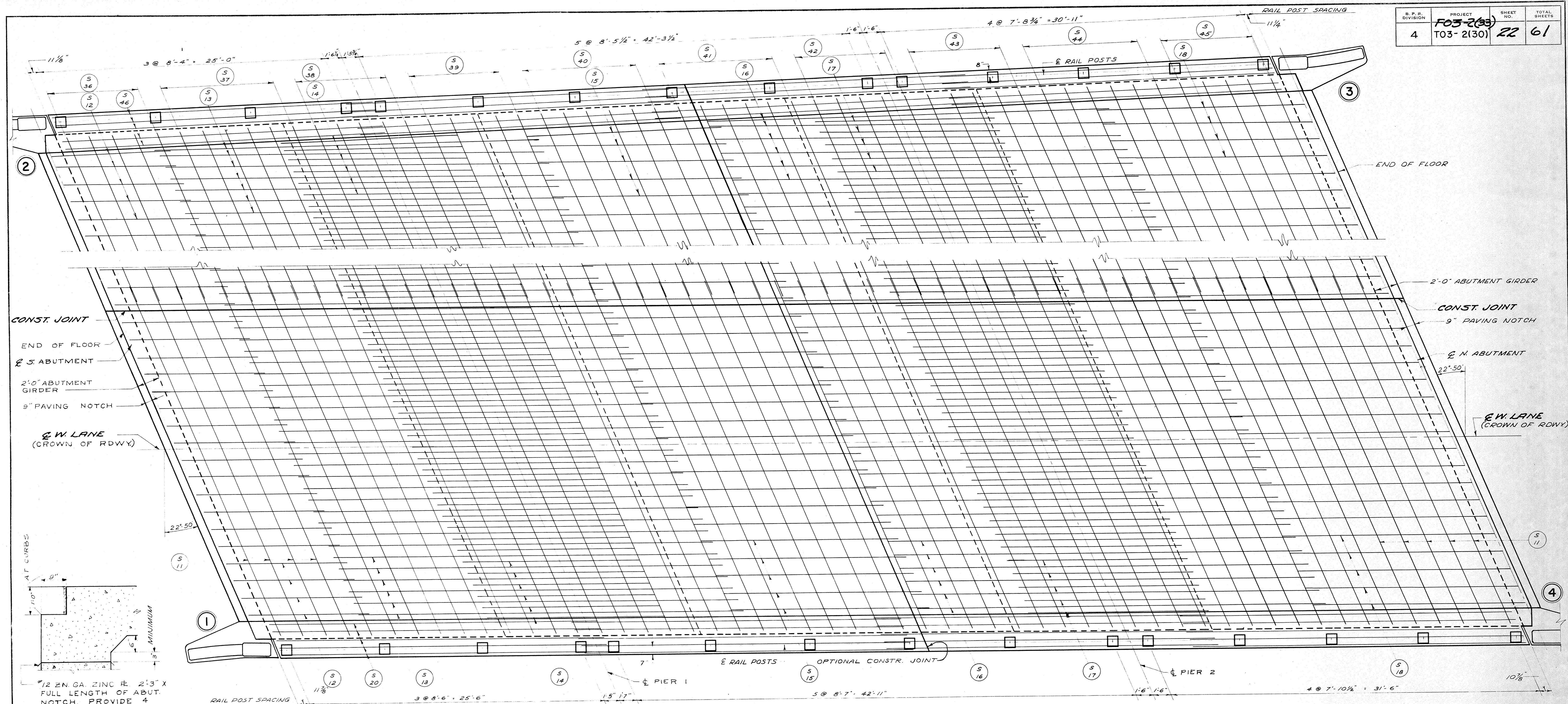
SECTION THRU GIRDER



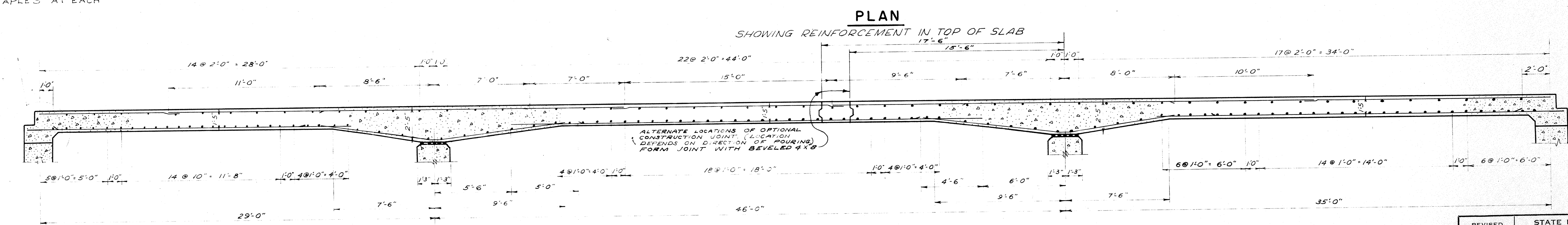
RAILING AND END POST DETAILS

ELEVATION OF RAILING

REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN
	PIER I & RAILING DETAILS
DESIGN SPEC. AASHO '53	LOADING H20-316 CONET 1957
DATE 9-23-57	DESIGN F.C.A. DRAWN J.W.K. H.E.B.
STRUCTURE B-70-29	SHEET 4 OF 8

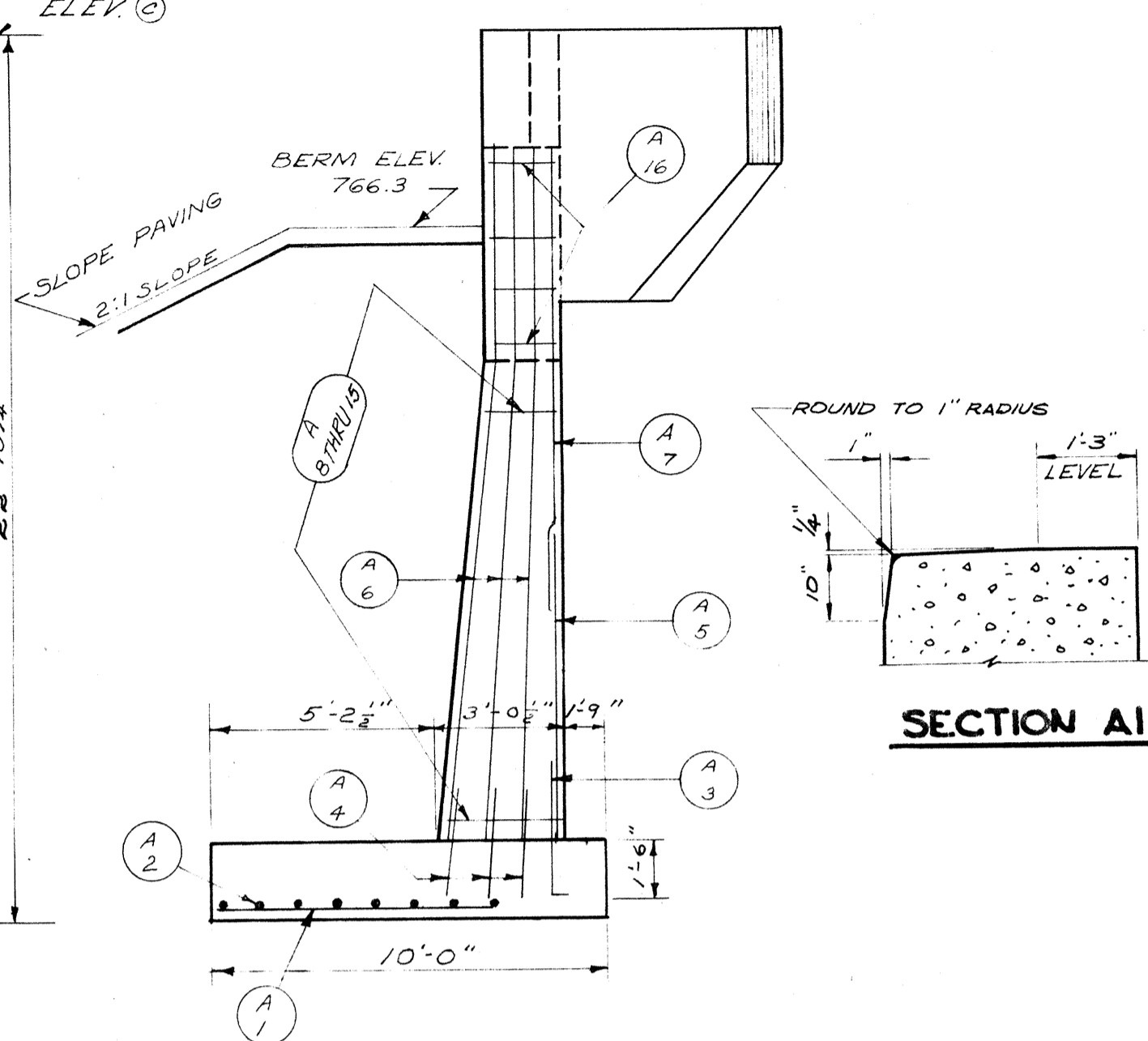
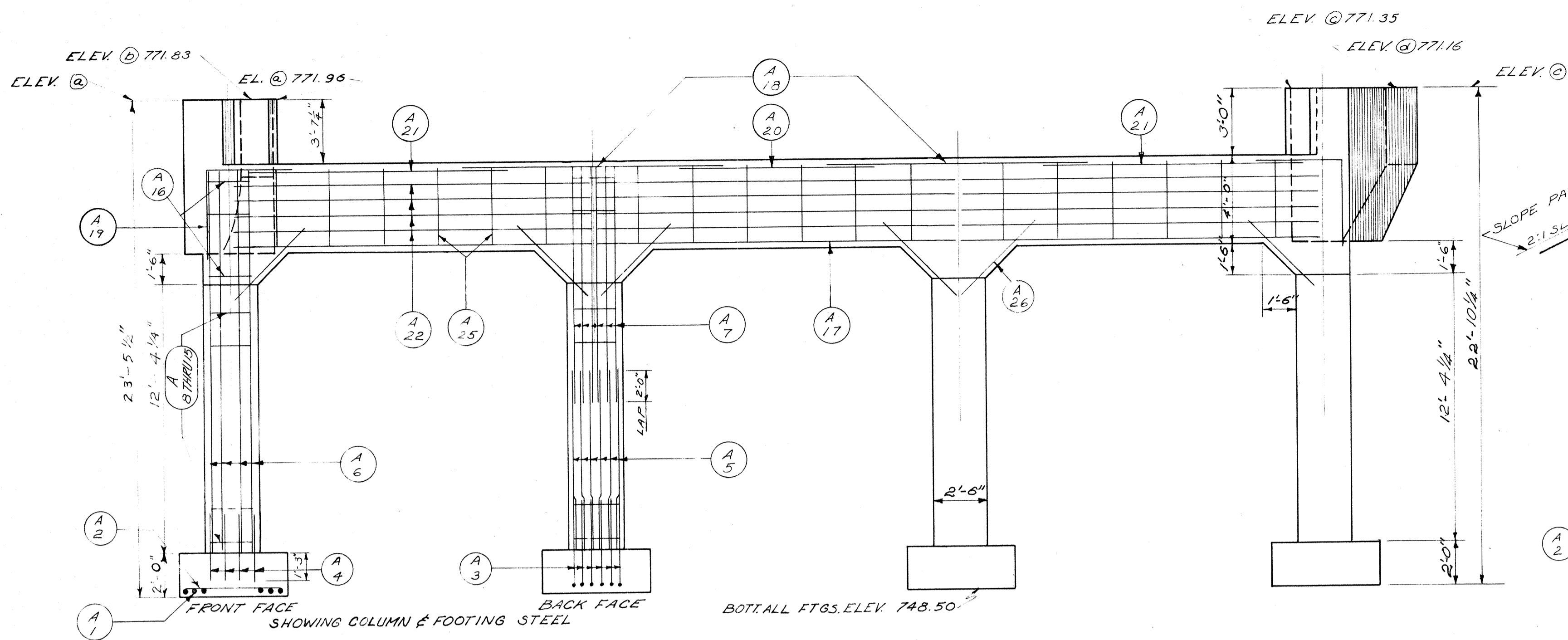


AT CURBS
1'-0"
MINIMUM
3"
#12 ZN. GA. ZINC # 2'-3" X FULL LENGTH OF ABUT. NOTCH. PROVIDE 4 WIRE STAPLES AT EACH SPLICE.



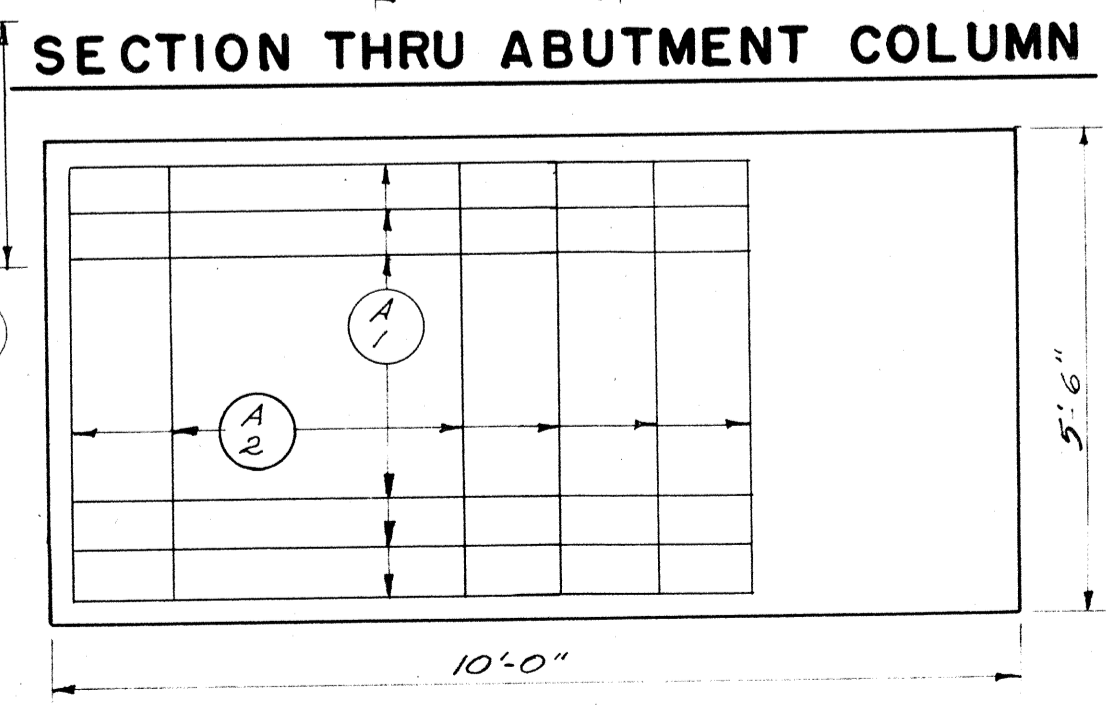
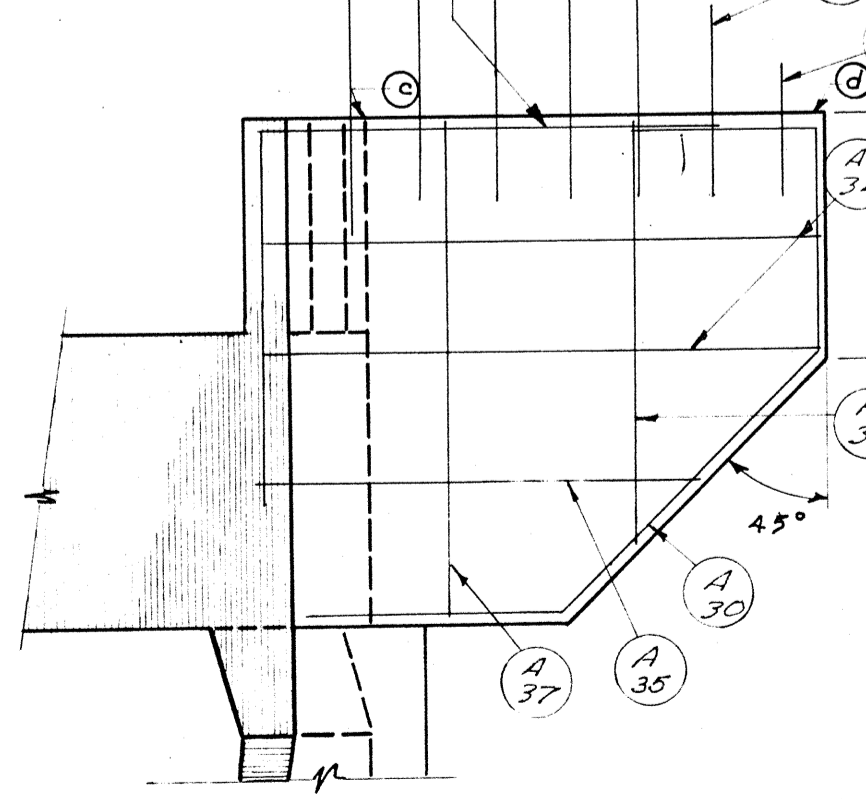
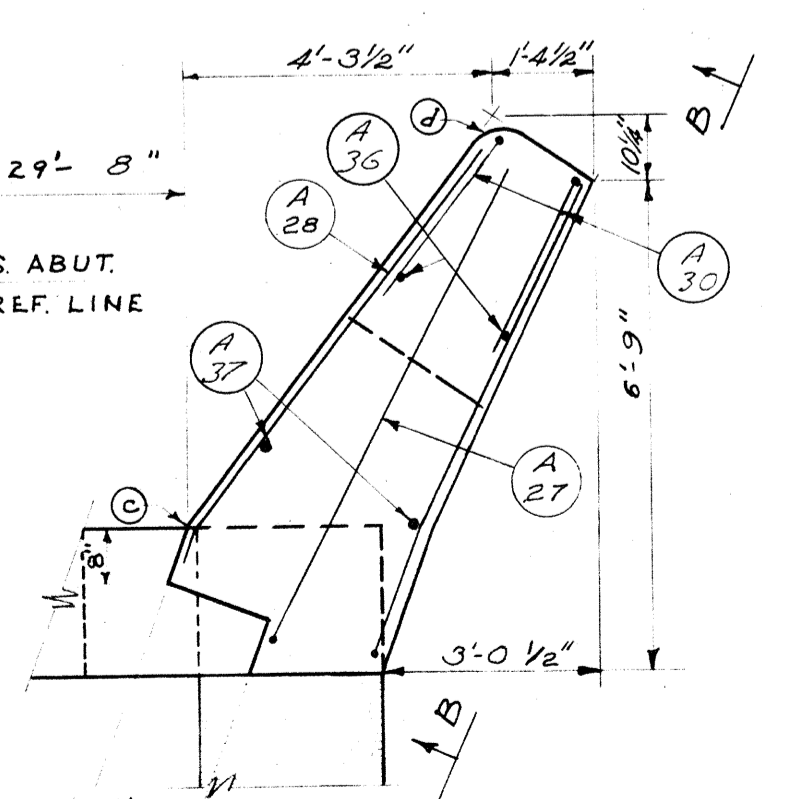
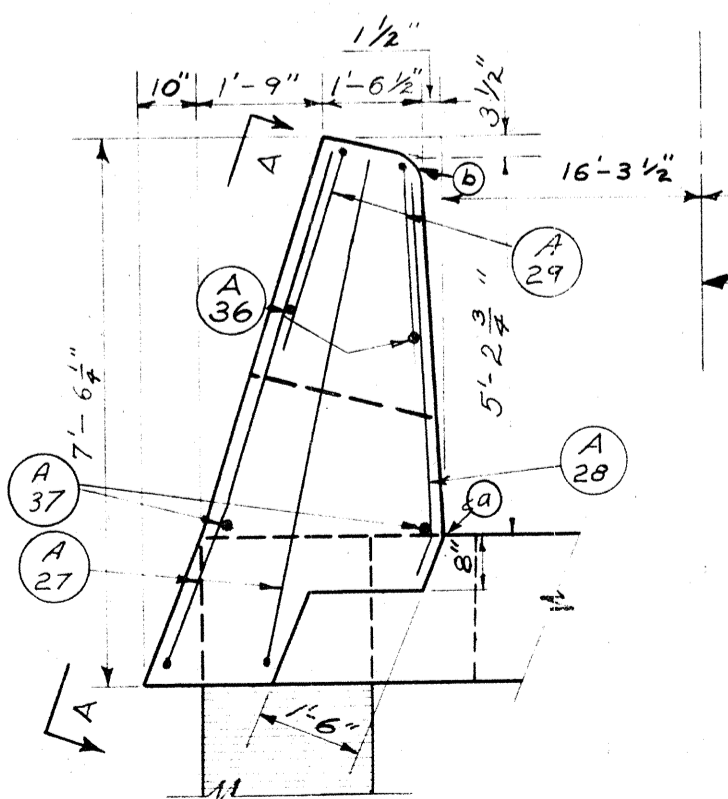
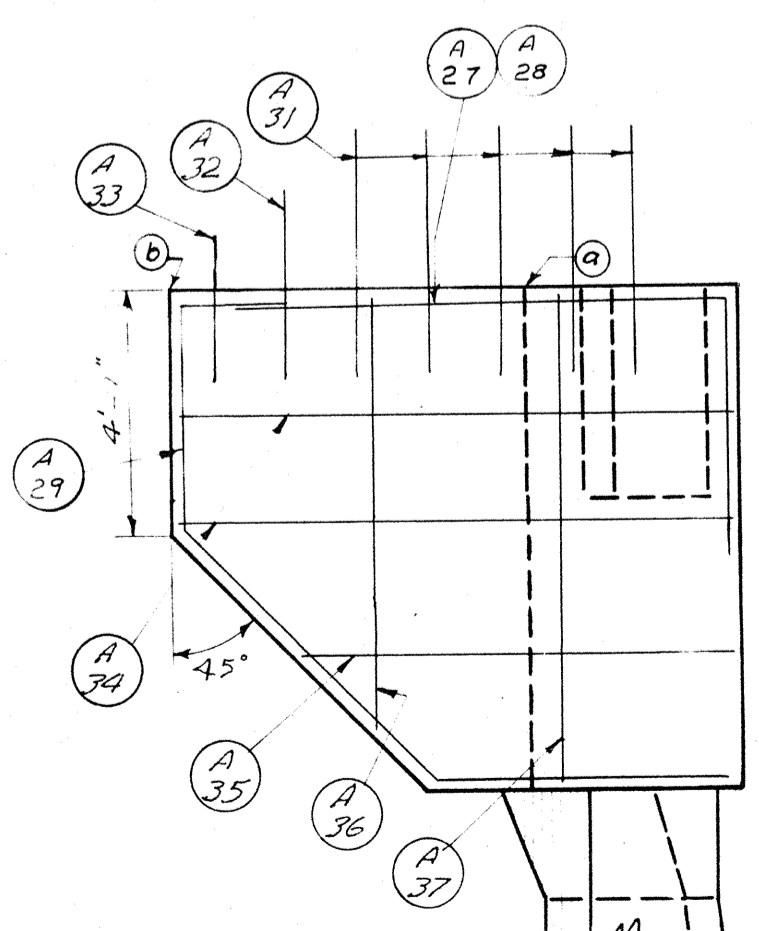
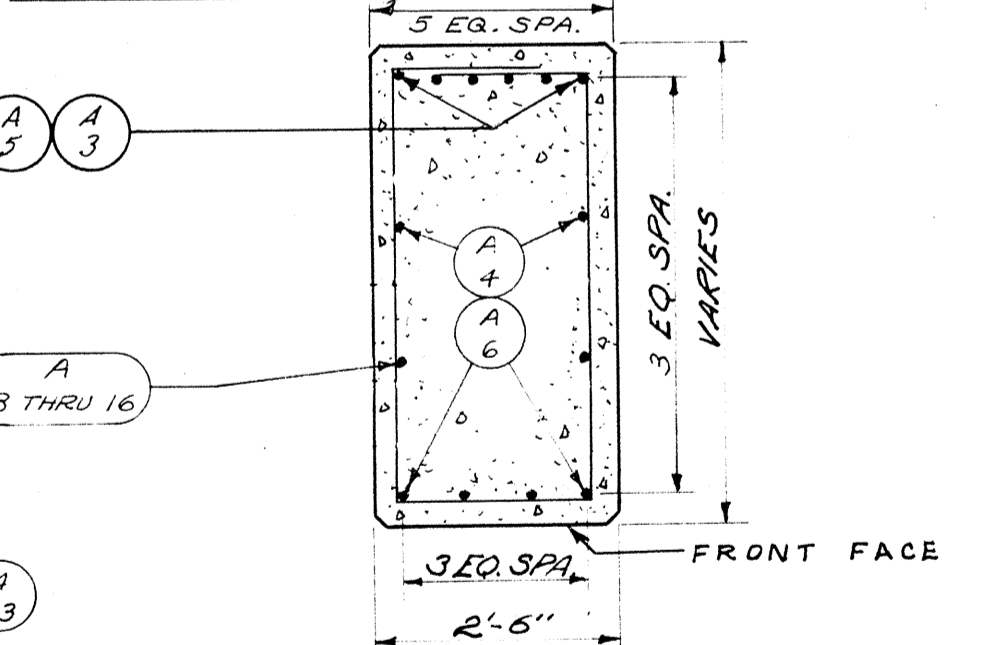
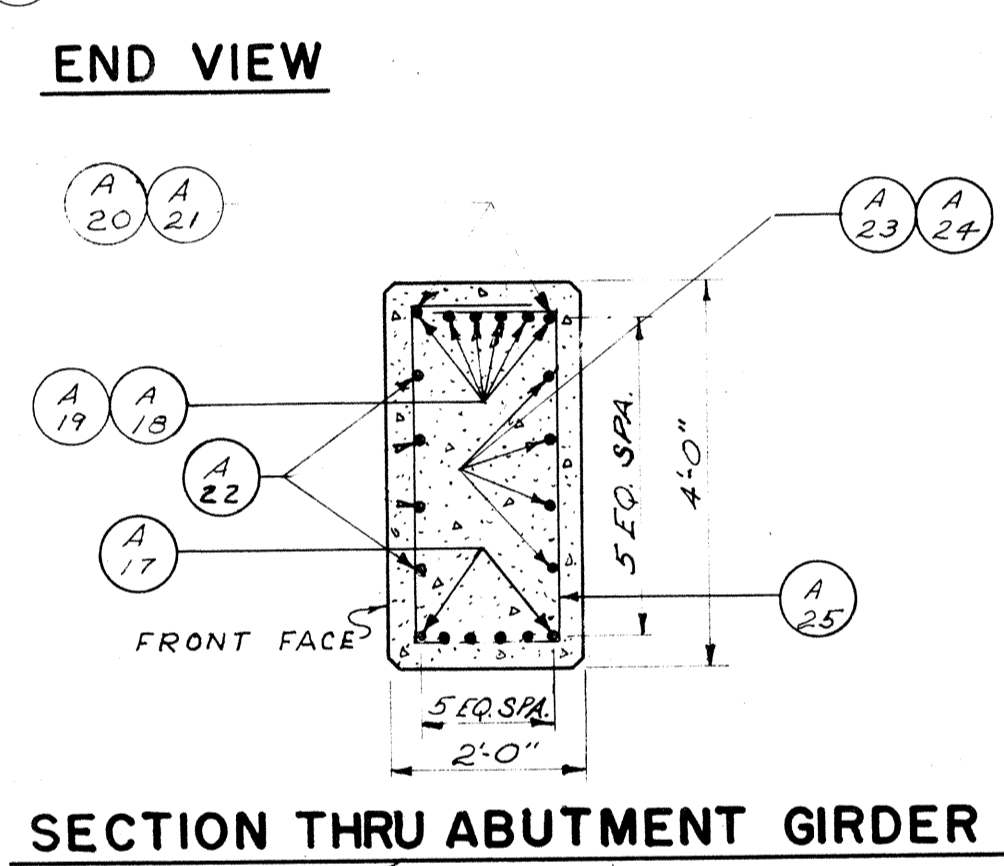
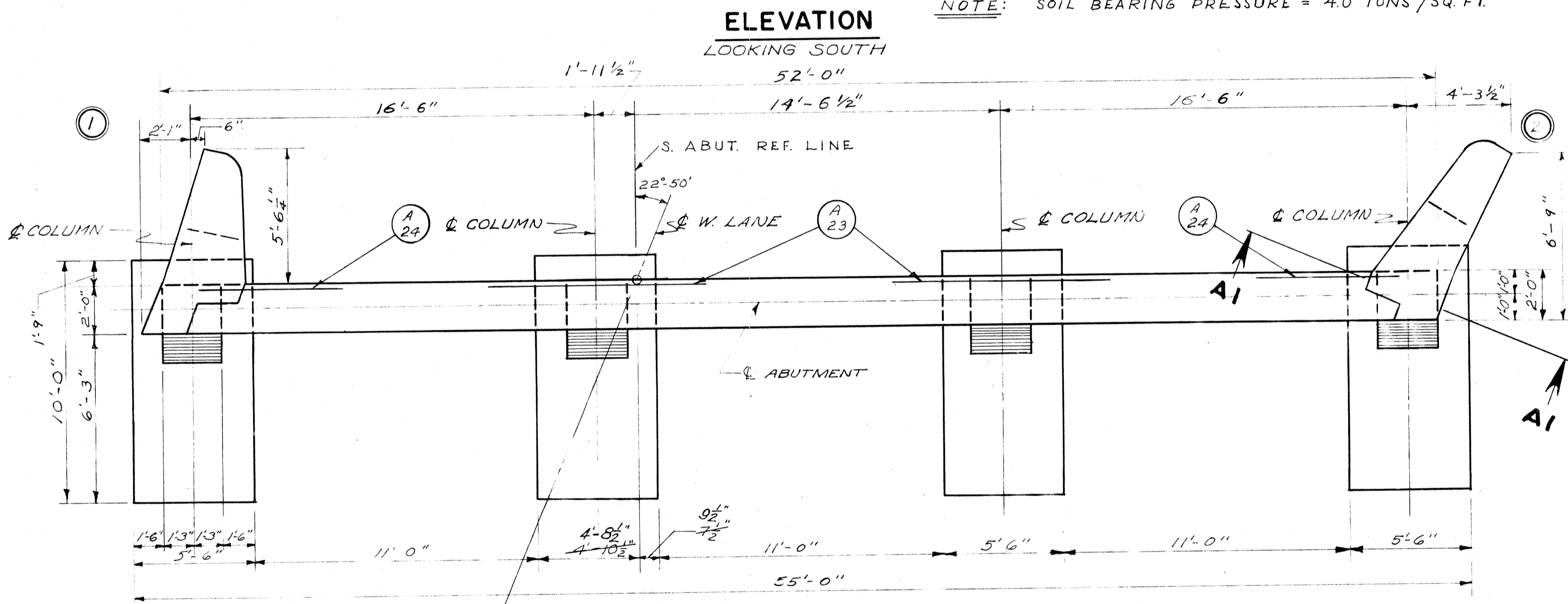
LONGITUDINAL SECTION

REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN		
	SUPERSTRUCTURE		
	DESIGN SPEC. AASHO '53	LOADING H20-S16	CONST. SPEC. 1957
	DATE 7-23-57	DESIGN C.C.K.	DRAWN C.U.L. CKD J.F.B.
STRUCTURE	B-70-29	SHEET	3 OF 8



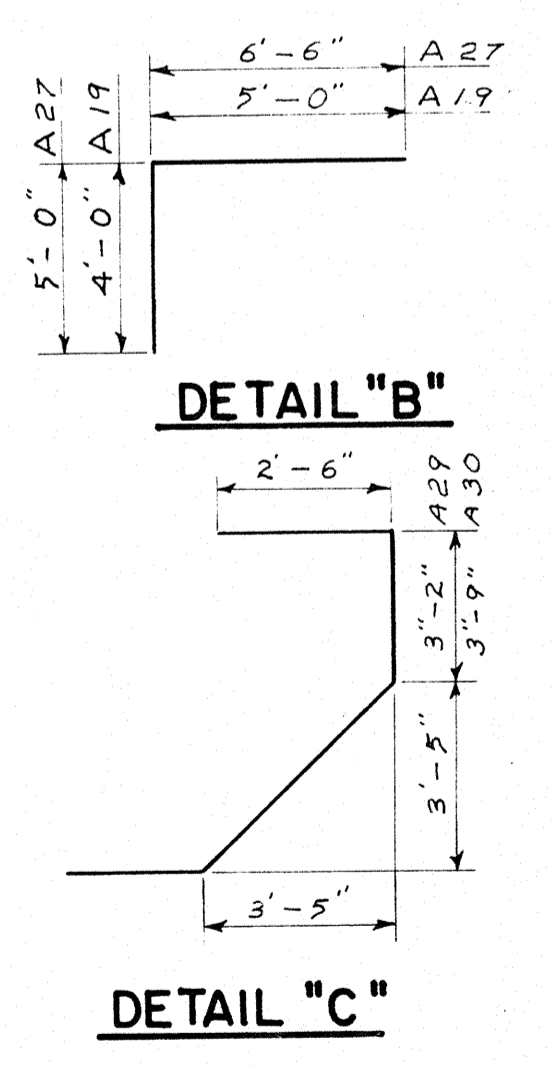
BILL OF BARS
DIMENSIONS IN BENDING ARE OUT TO OUT
SOUTH ABUTMENT (6060 #)

MARK	NO	SIZE	LENGTH	SPACING	LOCATION	DET.
A1	52	8	7-3	5"	FTG. - BOTT - LONGITUDINAL	
A2	32	4	5-0	1-0	" - " - TRANSVERSE	
A3	24	9	4-6	SHOWN	" & COL. - BACK FACE	D
A4	32	5	2-6	"	" - FRONT " & SIDES	
A5	24	9	10-0	SHOWN	COL. - BACK FACE	
A6	32	5	17-0	"	" - FRONT " & SIDES	
A7	24	8	9-0	"	" - BACK "	
A8	4	4	10-9	1-6	" - TIES "	A
A9	4	4	10-6	1-6	" - " "	A
A10	4	4	10-3	1-6	" - " "	A
A11	4	4	10-0	1-6	" - " "	A
A12	4	4	9-9	1-6	" - " "	A
A13	4	4	9-6	1-6	" - " "	A
A14	4	4	9-3	1-6	" - " "	A
A15	4	4	9-0	1-6	" - " "	A
A16	16	4	8-9	1-6	" - " "	A
A17	18	8	18-0	SHOWN	GIRDER - BOTTOM	
A18	12	9	10-0	"	" - TOP - INT. COLS.	
A19	12	5	9-0	"	" - " - EXT. COLS.	B
A20	2	4	8-6	"	" - STIER SUPP.	
A21	4	4	7-6	"	" - STIER SUPP.	
A22	12	6	16-0	"	" - FRONT FACE	
A23	8	6	8-0	"	" - BACK FACE - INT. COL.	
A24	8	5	5-0	"	" - BACK FACE - EXT. COL.	
A25	18	4	11-9	2-6	" - STIRRUPS	A
A26	24	5	4-6	SHOWN	" - HAUNCHES	
A27	4	4	11-6	"	" - WINGS 1 & 2	
A28	2	4	5-6	"	" - WINGS 1 & 2	
A29	2	4	15-0	"	" - " "	
A30	2	4	11-6	"	" - " "	
A31	20	4	3-6	"	" - " - DOWELS	
A32	4	4	2-6	"	" - " "	
A33	4	4	1-9	"	" - " "	
A34	8	4	7-6	"	" - " - HOR.	
A35	4	4	6-0	"	" - " - HOR.	
A36	4	4	5-9	"	" - " - VERT.	
A37	4	4	6-9	"	" - " - VERT.	



DETAIL "A"

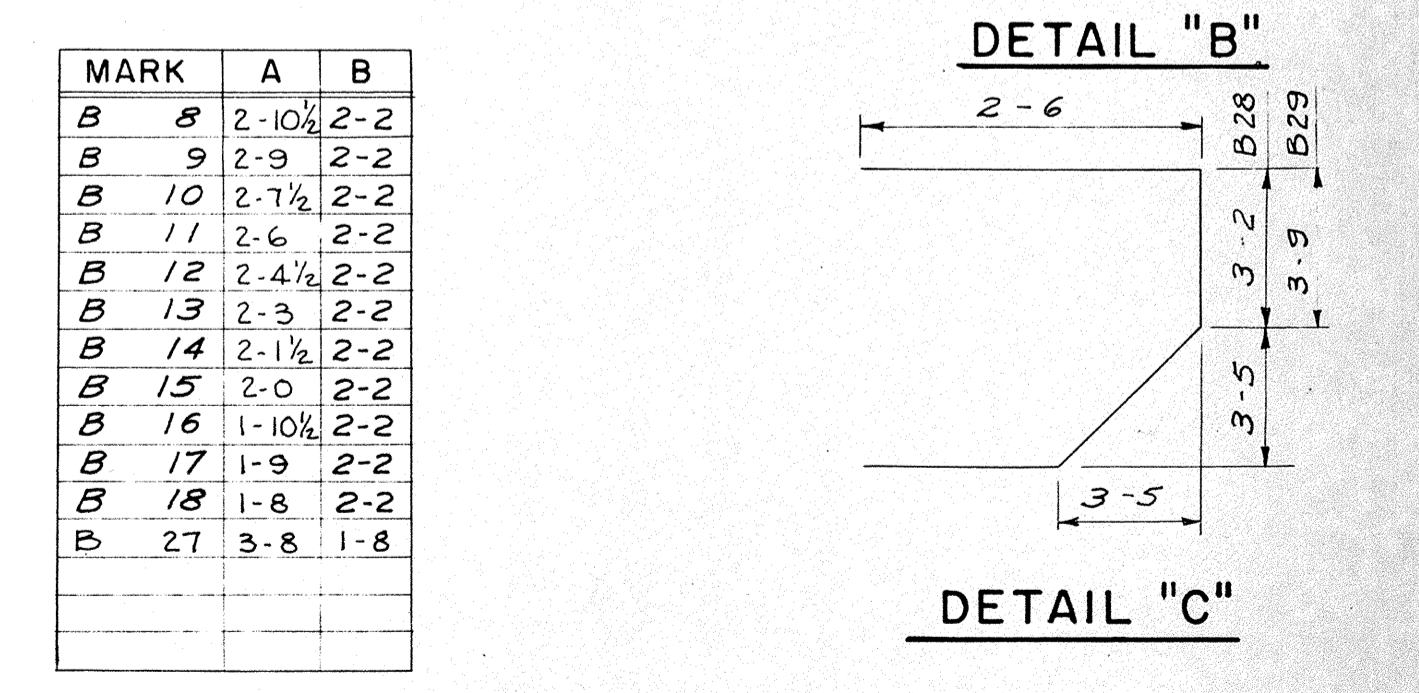
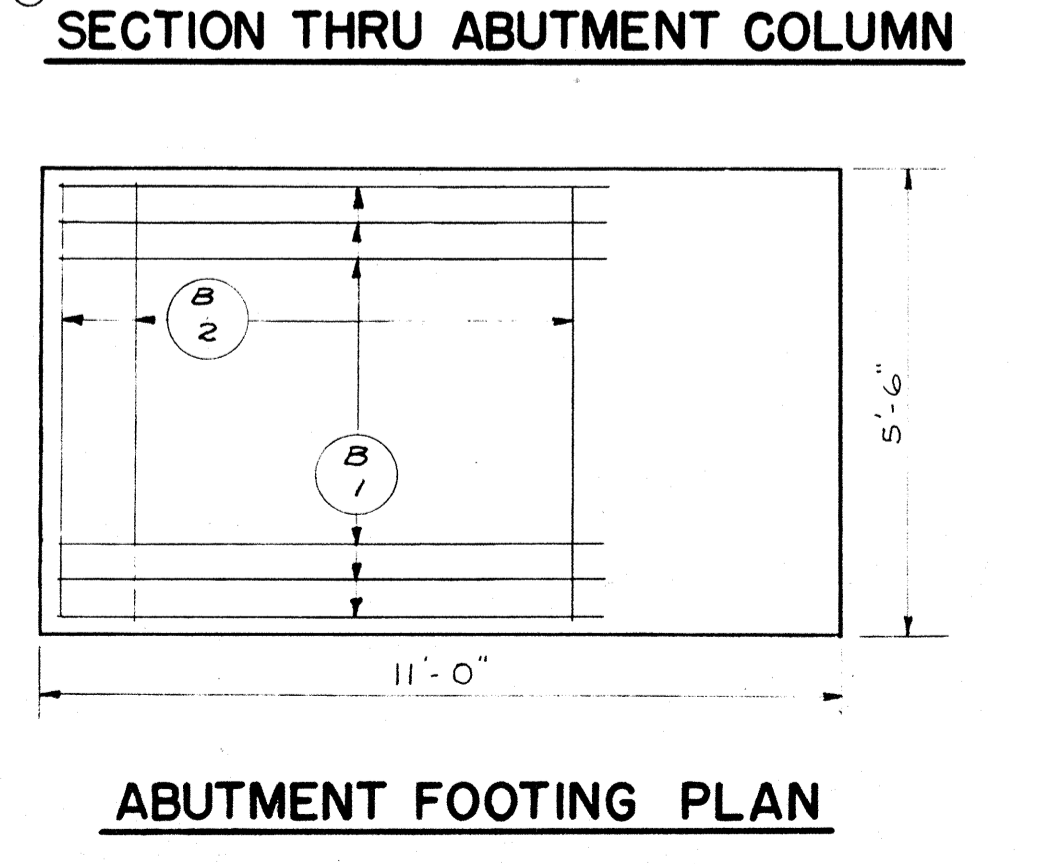
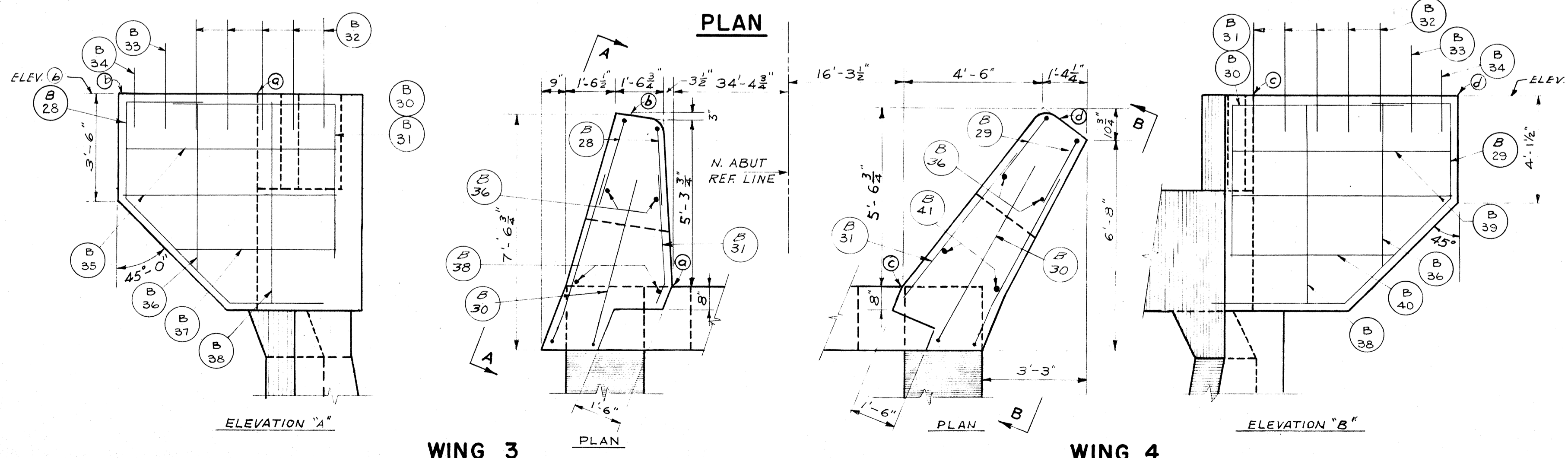
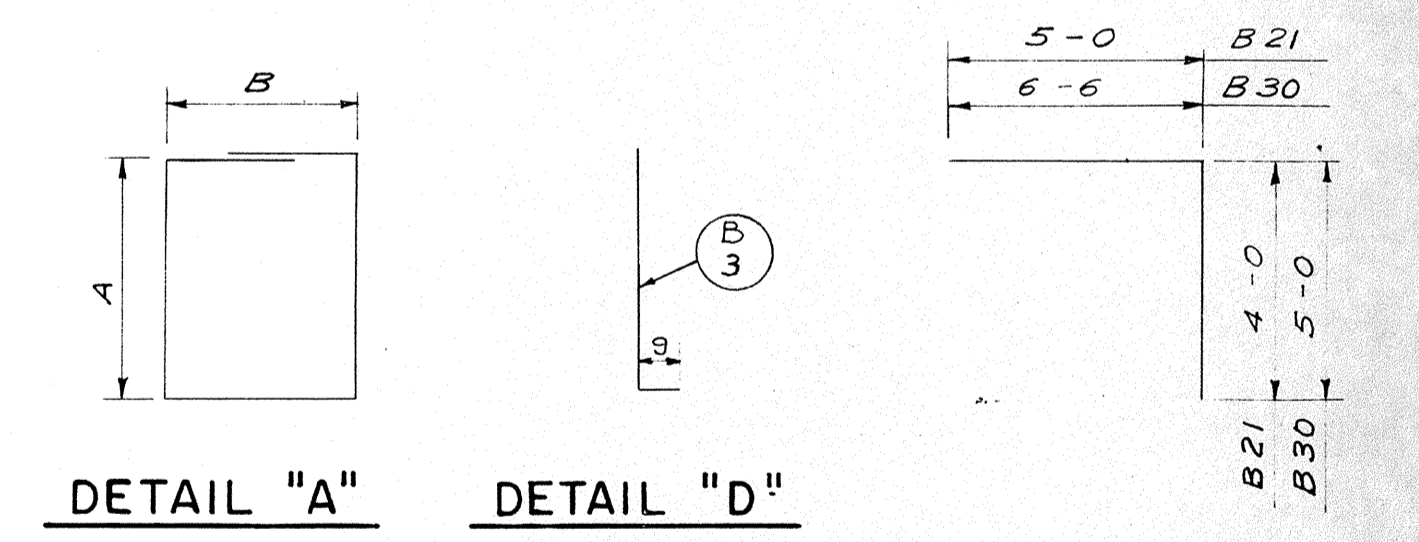
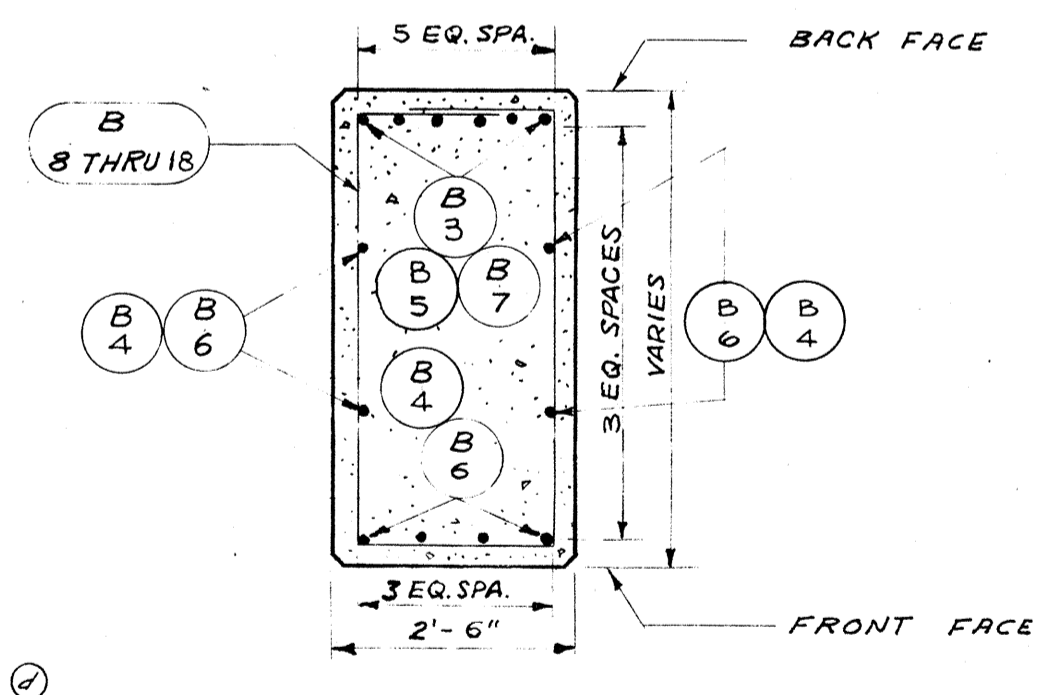
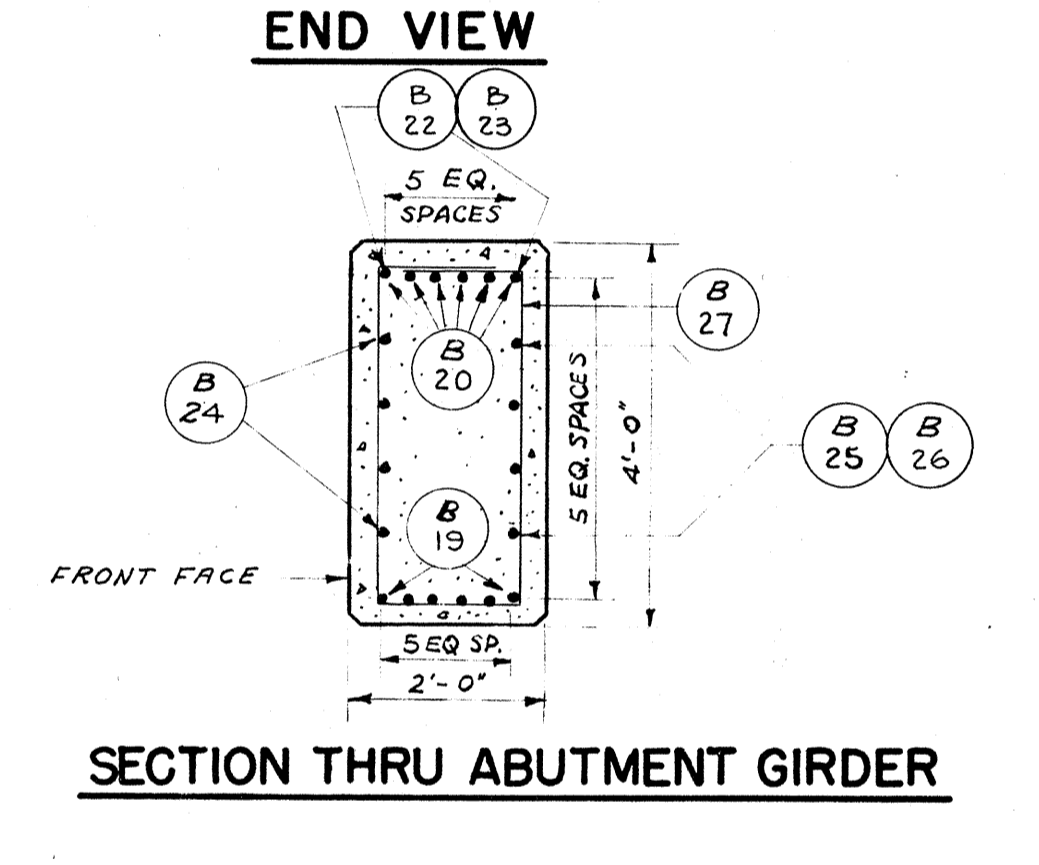
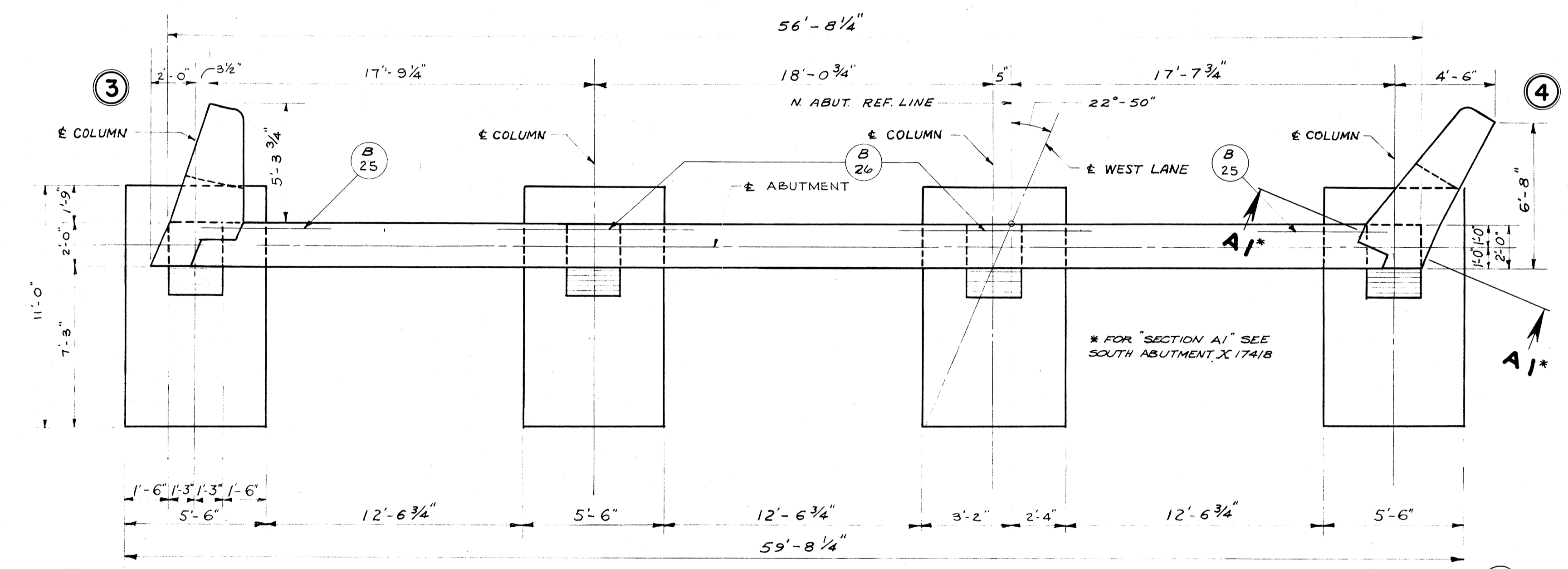
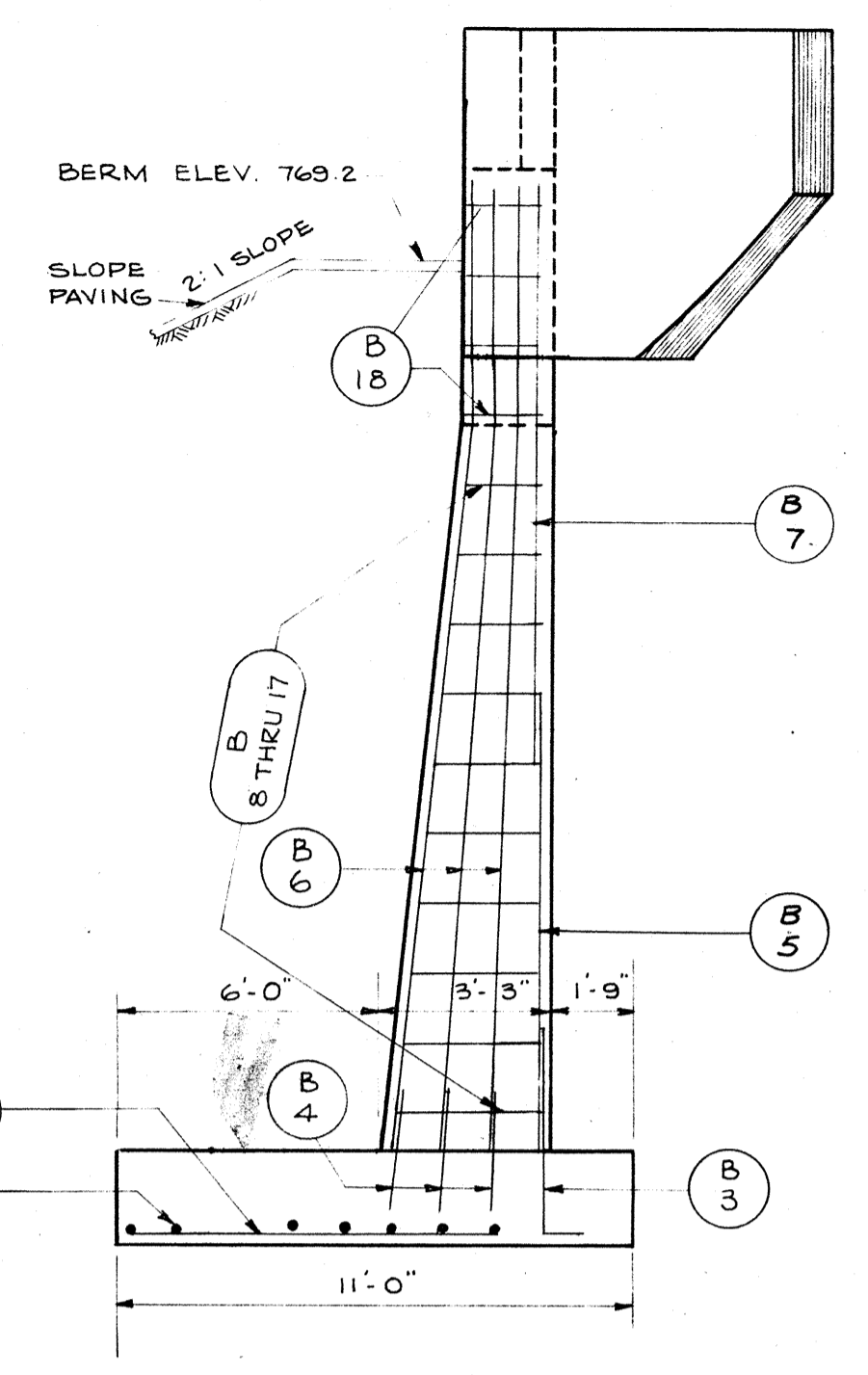
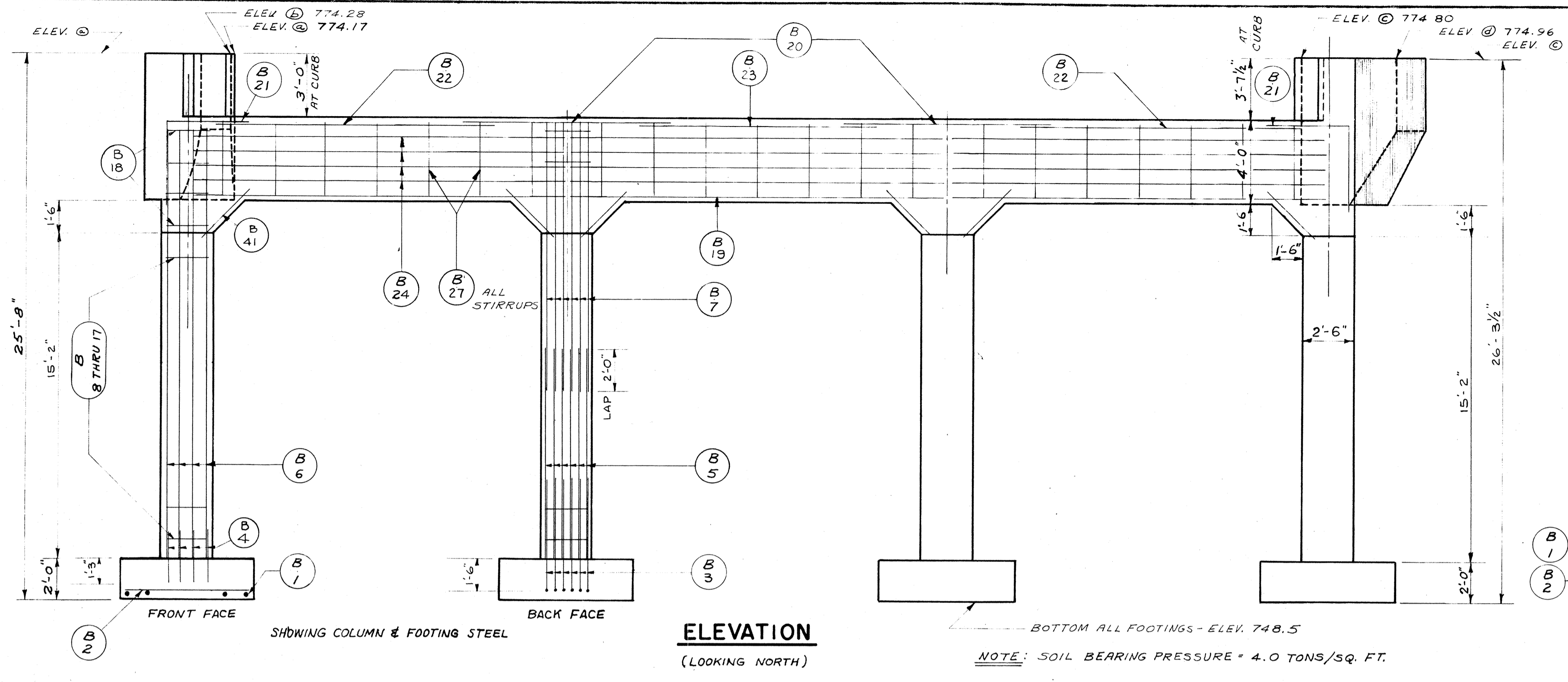
MARK	A	B
A8	2 - 7/8	2'-2"
A9	2 - 6	2'-2"
A10	2 - 4 1/2	2'-2"
A11	2 - 3	2'-2"
A12	2 - 1 1/2	2'-2"
A13	2 - 0	2'-2"
A14	1 - 10 1/2	2'-2"
A15	1 - 9	2'-2"
A16	1 - 8	2'-2"
A25	3 - 8	1-8



REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN
3-26-58	SOUTH ABUTMENT
3-2-58	DESIGN SPEC. AASHO 331 LOADING H20-S16 CONST. 1957
	DATE 9-23-57 DESIGN FOR DRAWN BR CKD HB
STRUCTURE B - 70 - 29	SHEET 6 OF 8

BILL OF BARS
DIMENSIONS IN BENDING ARE OUT TO OUT
NORTH ABUTMENT (6,800#)

FOUR	MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
FOOTINGS	B 1	52	8	8-0	5	FTG. - TOE - BOTT. - LONGIT.	
	B 2	32	4	5-0	1-0	" " " " - TRANSV.	
	B 3	24	9	4-6	1-6	FTG. & COL. - BACK FACE	D
	B 4	32	5	2-6	"	" " " " - FRONT FACE & SIDES	
COLUMNS, GIRDERS & WINGS	B 5	24	9	10-0	SHOWN	COL. - VERTICAL - BACK FACE	
	B 6	32	5	20-6	"	" " " " - FRONT FACE & SIDES	
	B 7	24	8	12-6	"	" " " " - BACK FACE	
	B 8	4	4	11-3	1-6	" " - TIES	A
	B 9	4	4	11-0	1-6	" " " "	A
	B 10	4	4	10-9	1-6	" " " "	A
	B 11	4	4	10-6	1-6	" " " "	A
	B 12	4	4	10-3	1-6	" " " "	A
	B 13	4	4	10-0	1-6	" " " "	A
	B 14	4	4	9-9	1-6	" " " "	A
	B 15	4	4	9-6	1-6	" " " "	A
	B 16	4	4	9-3	1-6	" " " "	A
	B 17	4	4	9-0	1-6	" " " "	A
	B 18	16	4	8-9	1-6	" " " "	A
	B 19	18	8	18-0	SHOWN	GIRDER - BOTTOM	
	B 20	12	9	10-0	"	" " - TOP - INT. COL'S	
	B 21	12	5	9-0	"	" " - EXT. COL'S	B
	B 22	4	4	9-3	"	" " - STIRRUP SUPPORTS	
	B 23	2	4	10-3	"	" " " "	
	B 24	12	6	18-0	"	" " - FRONT FACE	
B 25	8	5	5-0	"	" " - BACK FACE - EXT. COL.		
B 26	8	6	10-0	"	" " - INT. COL.		
B 27	21	4	11-9	2-6	" " - STIRRUPS	A	
B 28	2	4	14-6	SHOWN	WING 3	C	
B 29	2	4	15-0	"	WING 4	C	
B 30	4	4	11-6	"	WINGS 3 & 4	B	
B 31	2	5	6-0	"	WINGS 3 & 4 - HOR.	B	
B 32	20	5	3-6	"	" " " " - DOWELS		
B 33	4	5	2-9	"	" " " "		
B 34	4	5	2-0	"	" " " "		
B 35	4	4	7-9	"	WING 3 - HOR.		
B 36	4	4	5-9	"	WING 3 & 4 - VERT.		
B 37	2	4	5-3	"	" " 3 - HOR.		
B 38	4	4	6-9	"	WINGS 3 & 4 - VERT.		
B 39	4	4	7-0	"	WING 4 - HOR.		
B 40	2	4	4-6	"	" " " "		
B 41	24	5	5-0	"	GIRDER - HAUNCHES		



MARK	A	B
B 8	2-10 1/2	2-2
B 9	2-9	2-2
B 10	2-7 1/2	2-2
B 11	2-6	2-2
B 12	2-4 1/2	2-2
B 13	2-3	2-2
B 14	2-1 1/2	2-2
B 15	2-0	2-2
B 16	1-10 1/2	2-2
B 17	1-9	2-2
B 18	1-8	2-2
B 27	3-8	1-8

BILL OF BARS

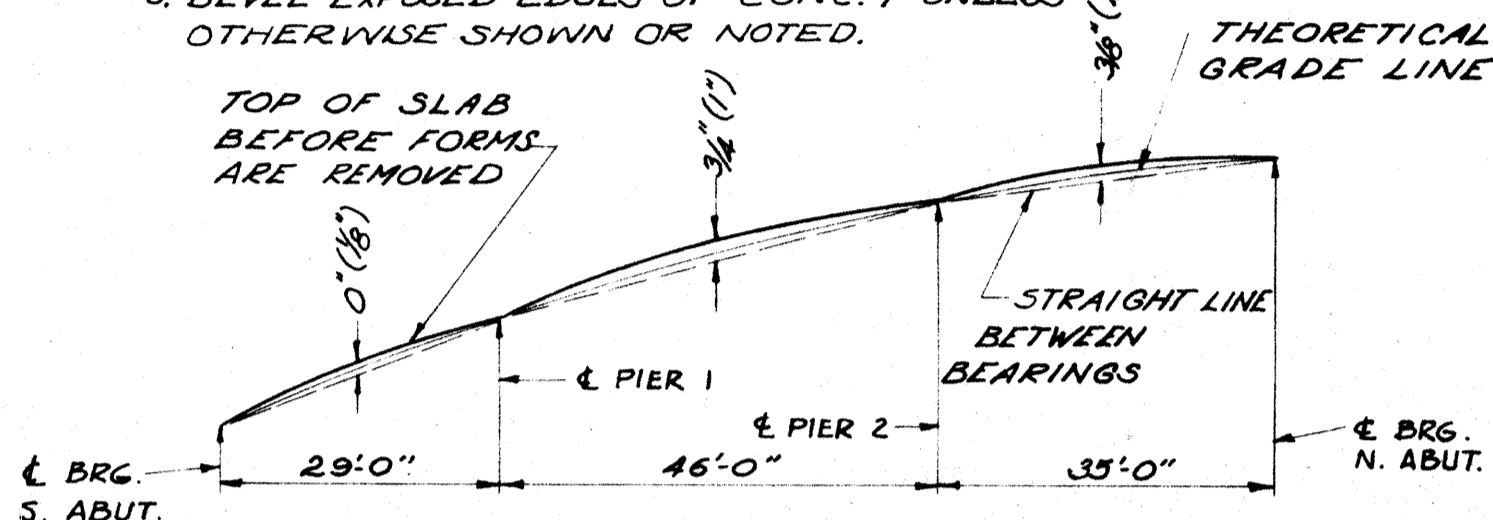
DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT

SUPERSTRUCTURE 64,090#

MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
S 1	27	7	18-6	1-0	LONGIT. - BOT. - SPAN 1	
S 2	27	7	23-9	1-0	" " " " " "	
S 3	20	9	29-3	SHOWN	" " " " " "	A
S 4	36	4	19-0	1-6	" " " " " "	
S 5	27	9	25-0	1-0	" " " " " "	
S 6	27	9	34-6	1-0	" " " " " "	
S 7	32	8	29-3	0-10	" " " " " "	
S 8	32	8	22-6	0-10	" " " " " "	
S 9	66	4	33-0	SHOWN	TRANSV. - BOT.	
S 10	34	5	33-0	"	" " " " " "	
S 11	54	4	33-0	2-0	" " " " " "	
S 12	18	4	10-6	1-6	LONGIT. - TOP - SPAN 1	
S 13	27	9	33-6	1-0	" " " " " "	
S 14	28	9	15-6	1-0	" " " " " "	
S 15	18	4	17-0	1-6	" " " " " "	
S 16	23	10	35-0	1-2	" " " " " "	
S 17	24	10	15-6	1-2	" " " " " "	
S 18	18	4	18-0	1-6	" " " " " "	
S 19	78	5	7-0	1-0	CURB - STIRRUPS	B
S 20	9	28-9	SHOWN	LONGIT. - CURB - TOP		
S 21	108	4	3-9	1-0	TRANSV. - CURB - TOP	C
S 22	30	5	2-9	SHOWN	CURB & RAIL POSTS	D
S 23	30	5	2-3	"	" " " " " "	
S 61	4	5	7-6	1-0	CURB - STIRRUPS - HAUNCHED SECT. AT PIERS	B
S 62	5	7-9	1-0	"	" " " " " "	B
S 63	4	5	8-0	1-0	" " " " " "	B
S 64	4	5	8-3	1-0	" " " " " "	B
S 65	2	5	8-9	1-0	" " " " " "	B
S 66	4	5	9-0	1-0	" " " " " "	B
S 67	4	5	9-3	1-0	" " " " " "	B
S 68	2	5	8-9	1-0	" " " " " "	B
S 69	2	5	7-9	1-0	" " " " " "	B
S 70	2	5	7-9	1-0	" " " " " "	B
S 71	2	5	7-6	1-0	" " " " " "	B
S 1	20	7	18-6	1-0	LONGIT. - BOT. SPAN 1	
S 2	19	7	23-9	1-0	" " " " " "	
S 4	33	4	19-0	1-6	" " " " " "	A
S 5	27	9	25-0	1-0	" " " " " "	
S 6	27	9	34-6	1-0	" " " " " "	
S 7	32	8	29-3	0-10	" " " " " "	
S 8	31	8	22-6	0-10	" " " " " "	
S 12	13	4	10-6	1-6	" " " " " "	
S 13	25	9	33-6	1-0	" " " " " "	
S 14	25	9	15-6	1-0	" " " " " "	
S 15	16	4	17-0	1-6	" " " " " "	
S 16	25	10	35-0	1-2	" " " " " "	
S 17	25	10	15-6	1-2	" " " " " "	
S 18	20	4	18-0	1-6	" " " " " "	
S 19	82	5	7-0	1-0	CURB - STIRRUPS	B
S 21	112	4	3-9	1-0	TRANSV. CURB - TOP	C
S 22	30	5	2-9	SHOWN	CURB & RAIL POSTS	D
S 23	30	5	2-3	"	" " " " " "	
S 24	3	7	20-3	1-0	LONGIT. - BOT. - SPAN 1	
S 25	4	7	23-9	1-0	" " " " " "	
S 26	6	4	19-9	1-6	" " " " " "	
S 27	5	8	24-0	0-10	" " " " " "	
S 28	5	8	30-0	0-10	" " " " " "	
S 29	5	4	24-3	1-0	TRANSV. " " " "	
S 30	5	4	24-6	0-10	" " " " " "	
S 31	5	4	25-0	0-10	" " " " " "	
S 32	5	4	25-6	0-10	" " " " " "	
S 33	5	4	26-0	1-0	" " " " " "	
S 34	4	4	26-6	SHOWN	" " " " " "	
S 35	5	4	27-3	"	" " " " " "	
S 36	5	4	27-9	1-0	" " " " " "	
S 37	6	5	28-6	"	" " " " " "	
S 38	7	5	29-0	"	" " " " " "	
S 39	6	5	29-9	"	" " " " " "	
S 40	5	4	30-0	"	" " " " " "	
S 41	5	4	30-9	SHOWN	" " " " " "	
S 42	4	4	31-3	"	" " " " " "	
S 43	7	4	32-0	1-0	" " " " " "	
S 44	5	5	32-9	"	" " " " " "	
S 45	5	5	33-3	"	" " " " " "	
S 46	5	5	33-9	"	" " " " " "	
S 47	6	6	33-9	"	" " " " " "	
S 48	20	9	30-9	SHOWN	LONGIT. " " " "	A
S 49	6	4	24-9	2-0	TRANSV. - TOP	
S 50	6	4	26-0	"	" " " " " "	
S 51	6	4	27-0	"	" " " " " "	
S 52	6	4	28-3	"	" " " " " "	
S 53	6	4	29-3	"	" " " " " "	
S 54	6	4	30-6	"	" " " " " "	
S 55	6	4	31-6	"	" " " " " "	
S 56	6	4	32-9	"	" " " " " "	
S 57	6	4	33-9	"	" " " " " "	
S 58	4	4	11-9	1-6	LONGIT. " " " "	
S 59	4	4	17-9	"	" " " " " "	
S 60	20	9	29-6	SHOWN	" " " " " "	
S 61	4	5	7-6	1-0	CURB - STIRRUPS - HAUNCHED SECT. AT PIERS	B
S 62	2	5	7-9	1-0	" " " " " "	B
S 63	4	5	8-0	1-0	" " " " " "	B
S 64	4	5	8-3	1-0	" " " " " "	B
S 65	2	5	8-9	1-0	" " " " " "	B
S 66	4	5	9-0	1-0	" " " " " "	B
S 67	4	5	9-3	1-0	" " " " " "	B
S 68	2	5	8-9	1-0	" " " " " "	B
S 69	2	5	7-9	1-0	" " " " " "	B
S 70	2	5	7-9	1-0	" " " " " "	B
S 71	2	5	7-6	1-0	" " " " " "	B

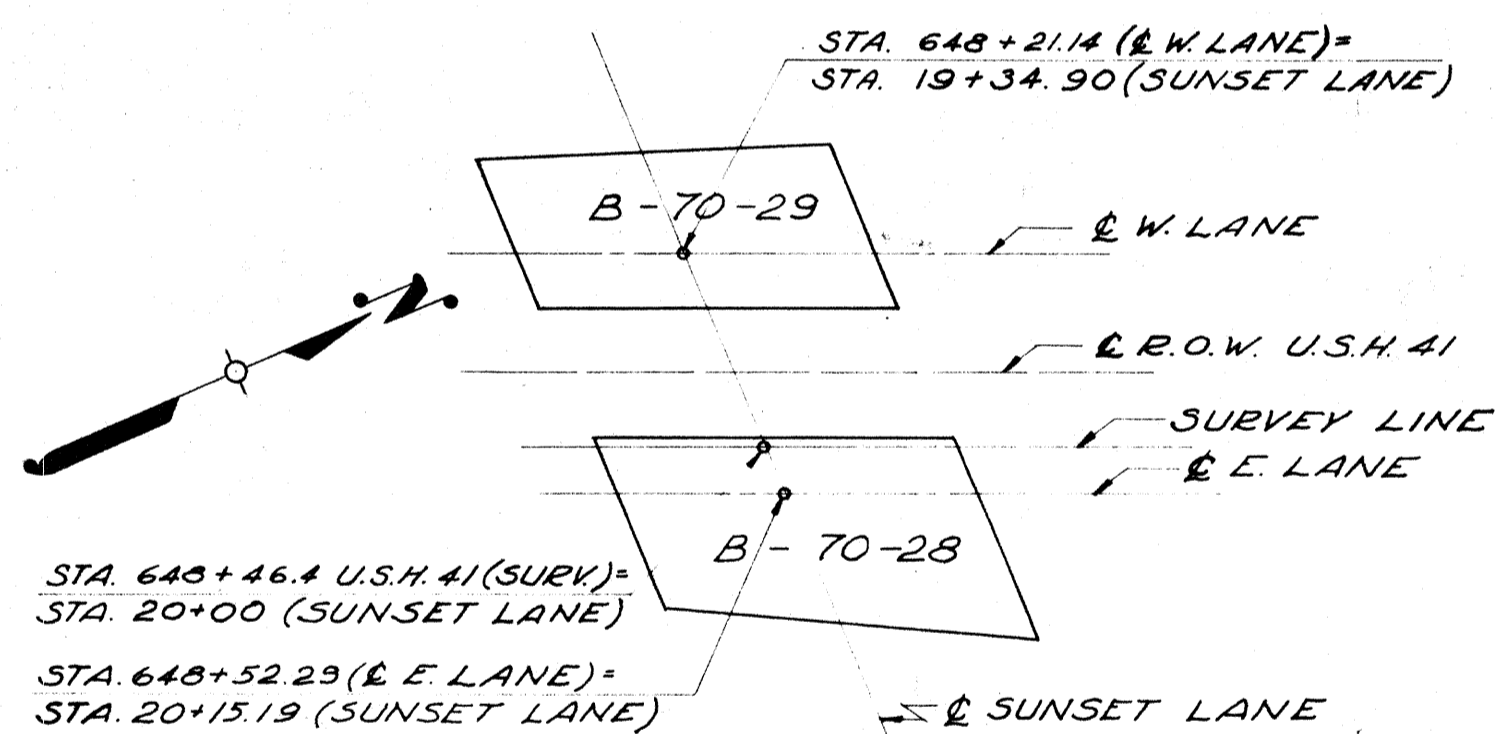
GENERAL NOTES

- DRAWINGS SHALL NOT BE SCALED.
- EXCAVATED VOLUME NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH GRANULAR BACKFILL TO THE ORIGINAL GROUND LINE. PAYMENT WILL BE MADE ONLY FOR THE MATERIAL ACTUALLY PLACED WITHIN THE LIMITS SPECIFIED IN THE STANDARD SPECIFICATIONS.
- THE USE OF STRUCTURAL GRADE BAR STEEL REINFORCEMENT IS PROHIBITED.
- BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED. BAR STEEL REINFORCEMENT IN FTBS SHALL BE EMBEDDED 3" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.
- EXCAVATION QUANTITIES FOR PIERS AND ABUTMENTS FIGURED FROM EXISTING GROUND.
- ALL CONCRETE MASONRY SHALL BE GRADE "AA" EXCEPT RAIL AND RAIL POSTS SHALL BE GRADE "X".
- ALL TRANSVERSE BARS IN SLAB SHALL BE PLACED PARALLEL TO CENTERLINE OF PIERS AND ABUTMENTS. SPACING CALLED FOR APPLIES ALONG CENTERLINE OF ROADWAY.
- BEVEL EXPOSED EDGES OF CONC. 1" UNLESS OTHERWISE SHOWN OR NOTED.

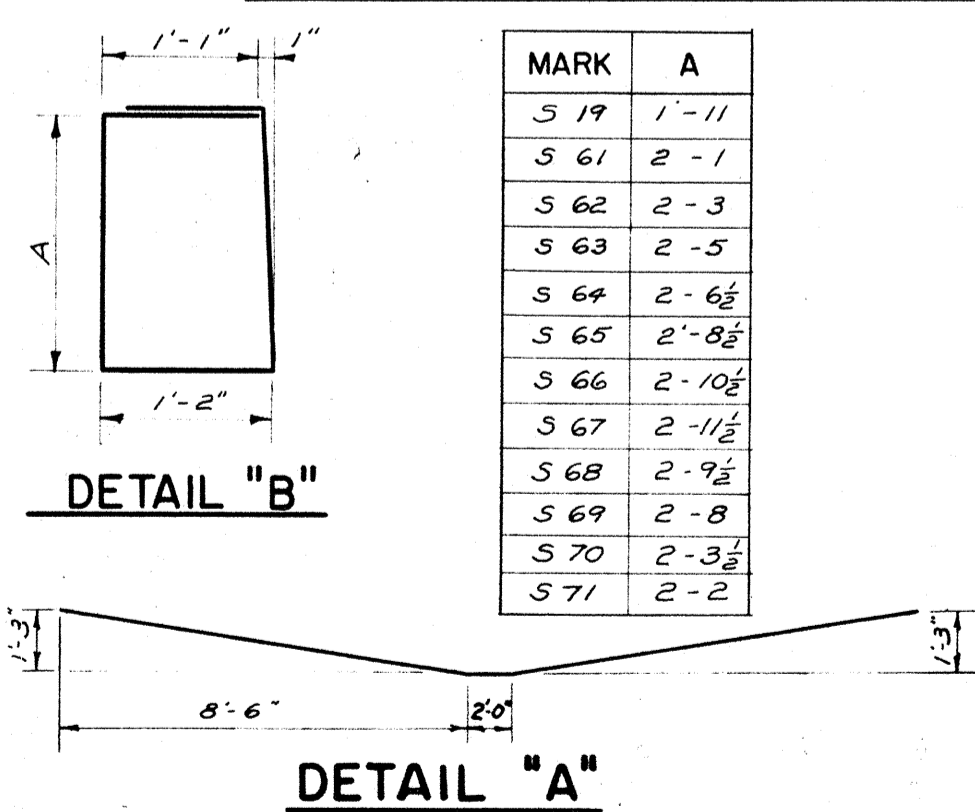


SHOWING ALLOWANCES TO BE MADE (BY CAMBERING OF SLAB FALSEWORK) FOR D.L. DEFLECTION, CREEP, & PLASTIC FLOW. NUMBERS IN PARENTHESES INCLUDE THE EFFECT OF THE VERTICAL CURVE ON THESE ALLOWANCES BUT DO NOT INCLUDE ALLOWANCE FOR FALSEWORK SETTLEMENT.

D.L. DEFLECTION AND FORMING DIAGRAM



LOCATION B-70-28 & B-70-29



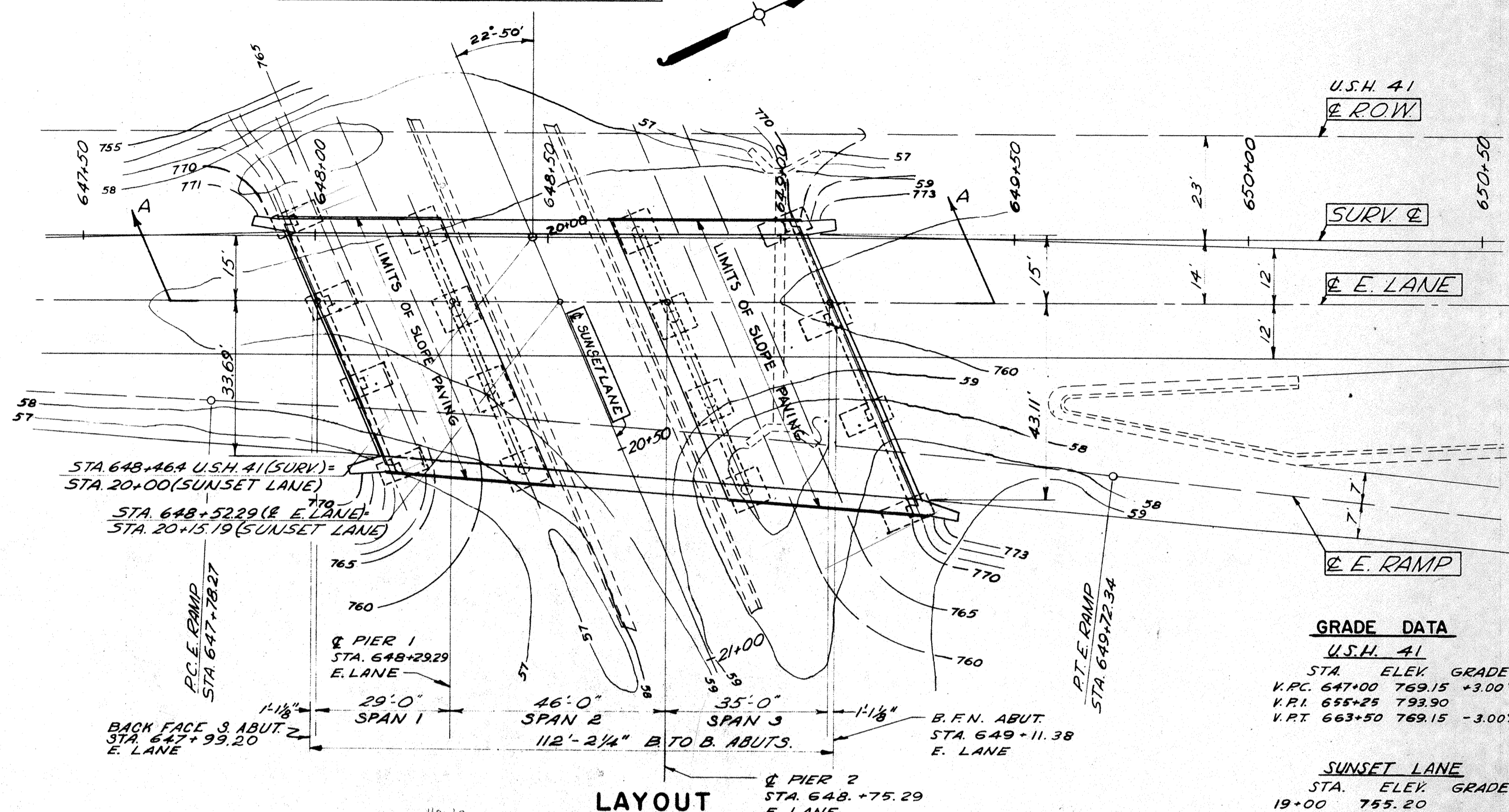
* BEND IN FIELD AS NECESSARY

DETAIL "C"

DETAIL "D"

* DESIGN STRESS: $f_c = 1400 \text{ psi}$

BENCH MARK
B.M. # 2 - STA. 647+80 - 105' L.T.
SPIKE IN POLE - ELEV. 756.03



LAYOUT

GRADE DATA

U.S.H. 41
STA. ELEV. GRADE
V.P.C. 647+00 769.15 +3.00%
V.P.I. 655+25 793.90
V.P.T. 663+50 769.15 -3.00%

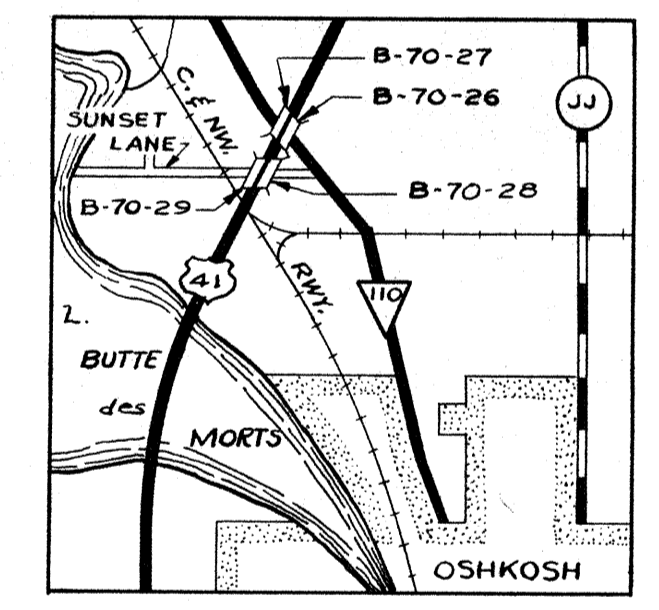
SUNSET LANE

STA. ELEV. GRADE
19+00 755.20
20+50 755.65 +0.30%

RAMP DATA

TANGENT
P.I. = STA. 646+25 OFFSET OF P.I. FROM E. LANE = 12' RT. & BETWEEN E. LANE & E. RAMP = 2'-44" - 53"

CURVE
P.I. = 648+75.35 OFFSET OF P.I. FROM E. LANE = 26.40' RT.
 $\Delta = 3^\circ - 52' - 53''$
 $D = 2^\circ - 00'$
 $T = 97.08'$
 $L.C. = 194.07'$
 $S.E. = 0.04 \text{ FT/FT}$



LOCALITY MAP

TOTAL ESTIMATED QUANTITIES

BID ITEMS	UNIT	SUPER-STRUCTURE	SOUTH ABUT.	PIER 1	PIER 2	NORTH ABUT.	TOTAL
EXCAVATION FOR STRUCTURES	C.Y.	—	165	170	165	165	665
CONC. MASONRY - GRADE "AA"	C.Y.	396.8*	55.1	55.9	59.0	62.9	629.7
CONC. MASONRY - GRADE "X"	C.Y.	4.3	—	—	—	—	4.3
BAR STEEL REINFORCEMENT	LB.	64,090	6360	6510	6810	7210	90,980
SHEET ZINC	LB.	284	—	—	—	—	284
SLOPE PAVING	S.Y.	—	225	—	—	290	515
GRANULAR BACKFILL	C.Y.	—	145	145	140	165	595
NON-BID ITEMS							
EXPANSION JOINT FILLER	LUMP SUM	12 1/2	—	—	—	—	12 1/2
1/4" PREFORMED BIT. FIBER TYPE FILLER	S.F.	—	—	118	127	—	245

LIST OF DRAWINGS

- LAYOUT X17405
- SUPERSTRUCTURE X17406
- SUPERSTRUCTURE X17407
- PIER 1 & RAILING DETAIL X17408
- PIER 2 X17409
- SOUTH ABUTMENT X17410
- NORTH ABUTMENT X17411
- LOG OF SOUNDINGS X17412

* THE DESIGN OF THIS STRUCTURE IS IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, R.A.S. H.O. EDITION OF 1953.

MEAD AND HUNT INC.
ENGINEERS - ARCHITECTS
MADISON, WISCONSIN

STATE HIGHWAY COMMISSION OF WISCONSIN

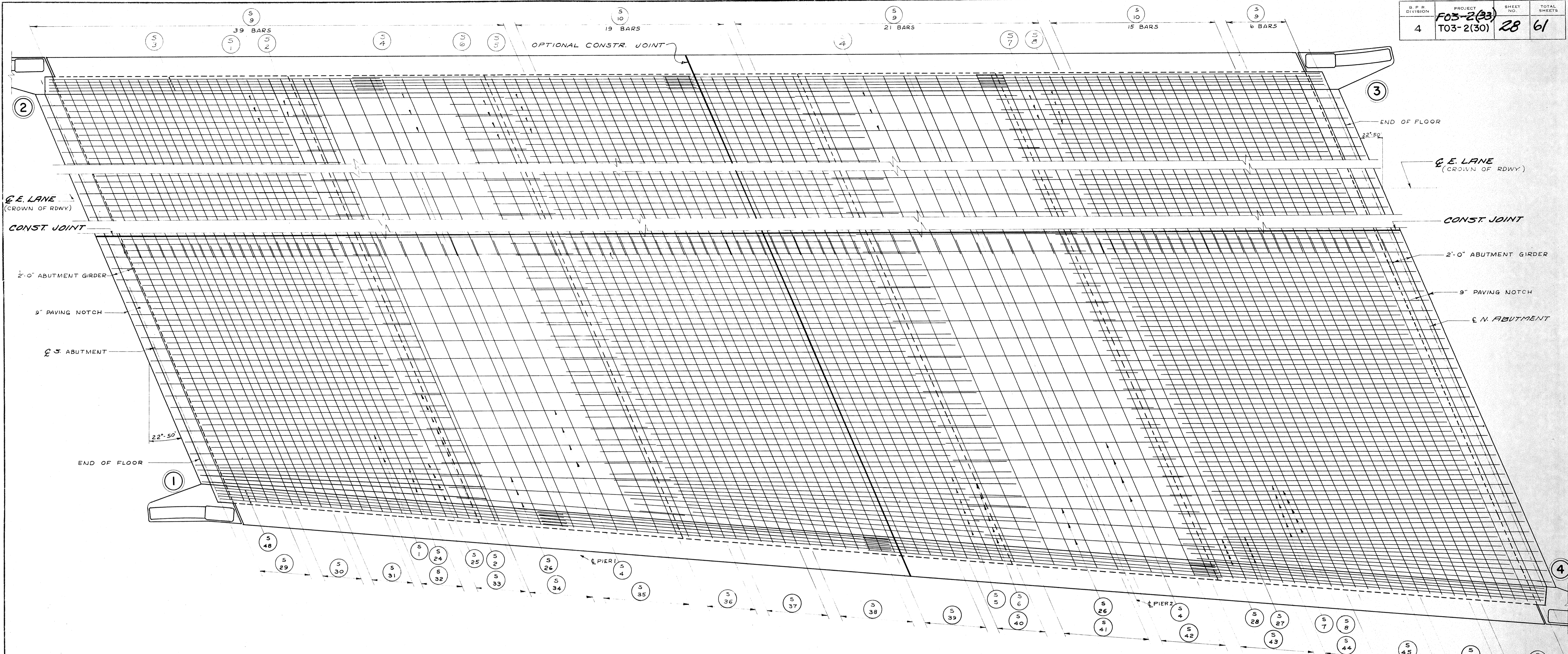
LAYOUT

CO. WINNEBAGO TNS. OSHKOSH STA. 648+52.29
SECTION 3 TOWN 18 N RANGE 16 E
DATE 9-23-57 DRWN. D.J. CKD. H.B. SPEC. 1957* LOAD 120-165

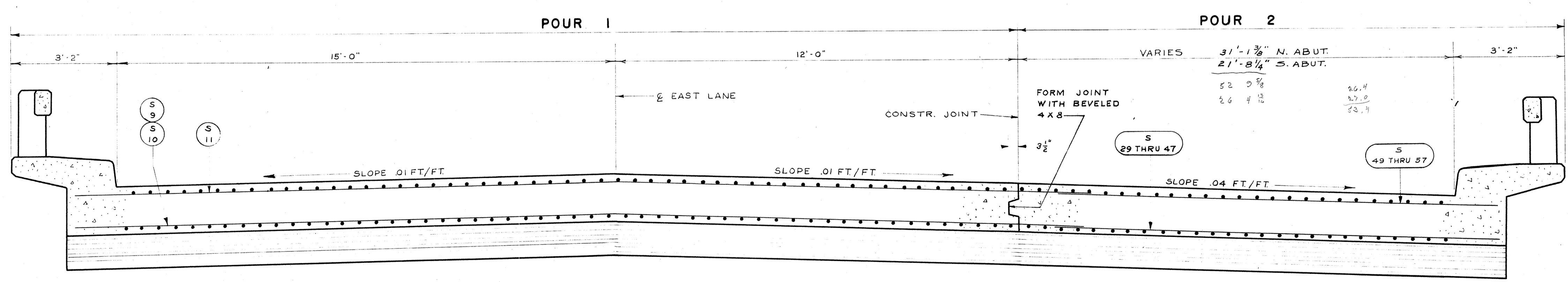
CORRECT: *H.B. Schultz*
BRIDGE ENGINEER

APPROVED: *E.L. Rattigan*
STATE HIGHWAY ENGINEER

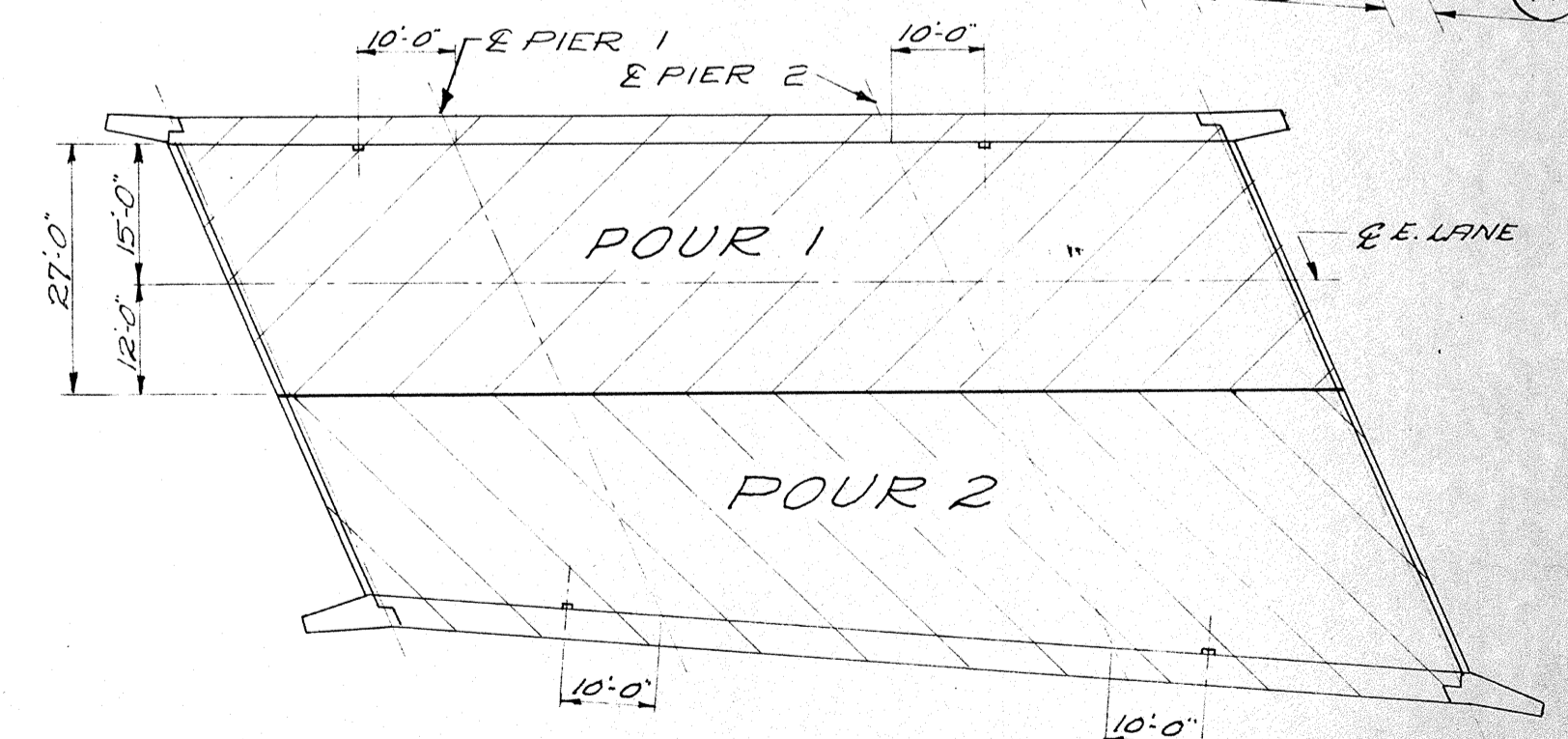
STRUCTURE B-70-28 SHEET 1 OF 8



PLAN
SHOWING REINFORCEMENT IN BOTTOM OF SLAB



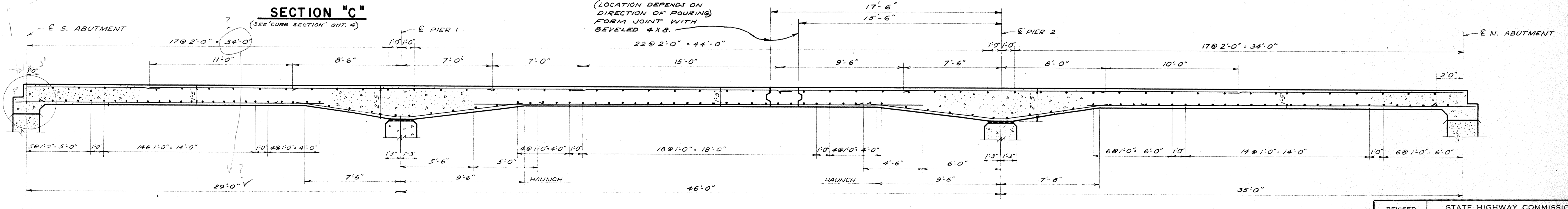
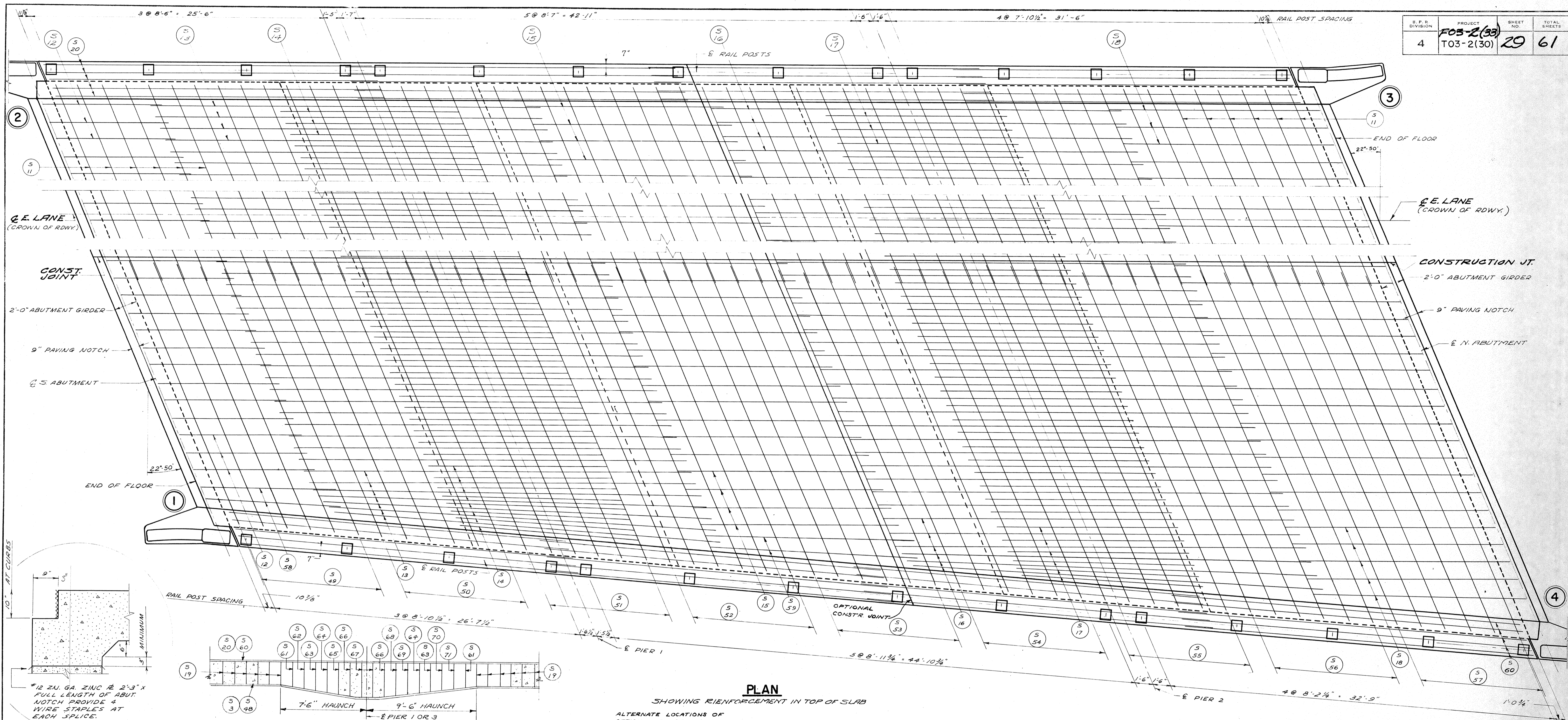
TRANSVERSE SECTION THRU ROADWAY



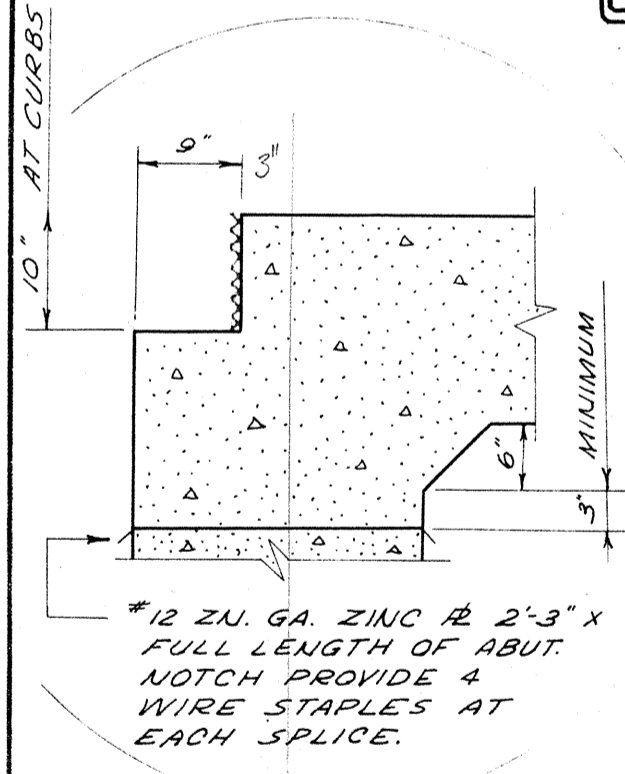
POURING DIAGRAM
(ALSO SHOWING FLOOR DRAIN LOCATIONS)

REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN		
	SUPERSTRUCTURE		
	DESIGN SPEC AASHO '53	LOADING H20-S16	CONST. SPEC. 1957
	DATE 9-23-57	DESIGN FCA	DRAWN C.V.L. CKD HEB
STRUCTURE	B - 70 - 28	SHEET	2 OF 8

B. P. R. DIVISION	PROJECT	SHEET NO.	TOTAL SHEETS
4	F03-2(30)	29	61

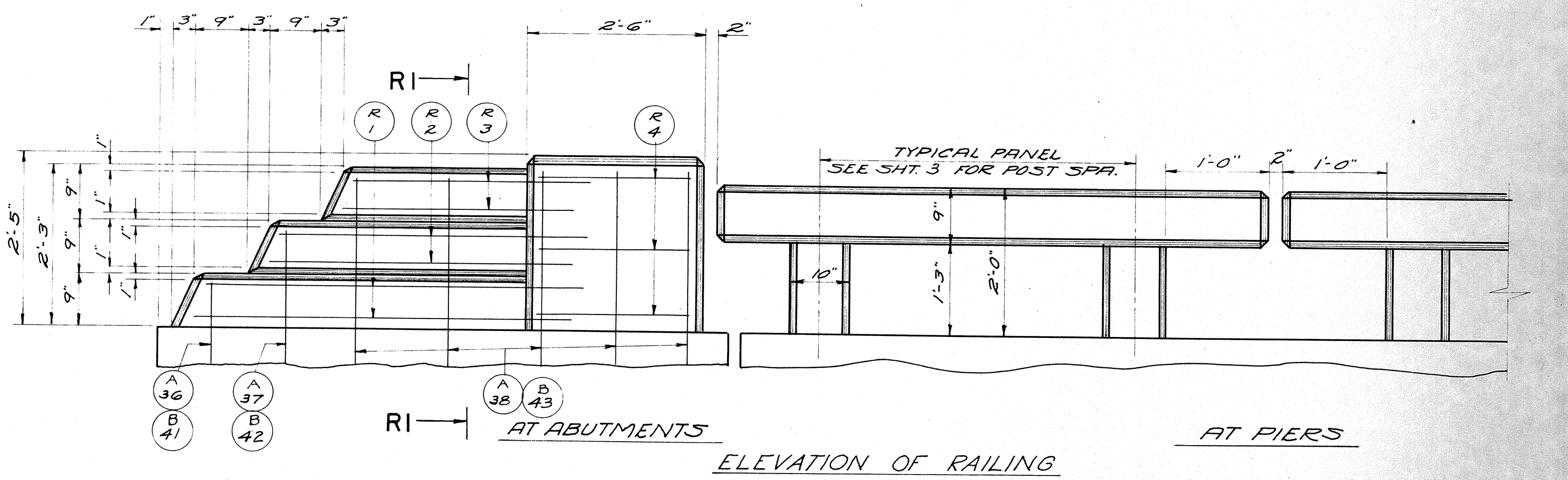
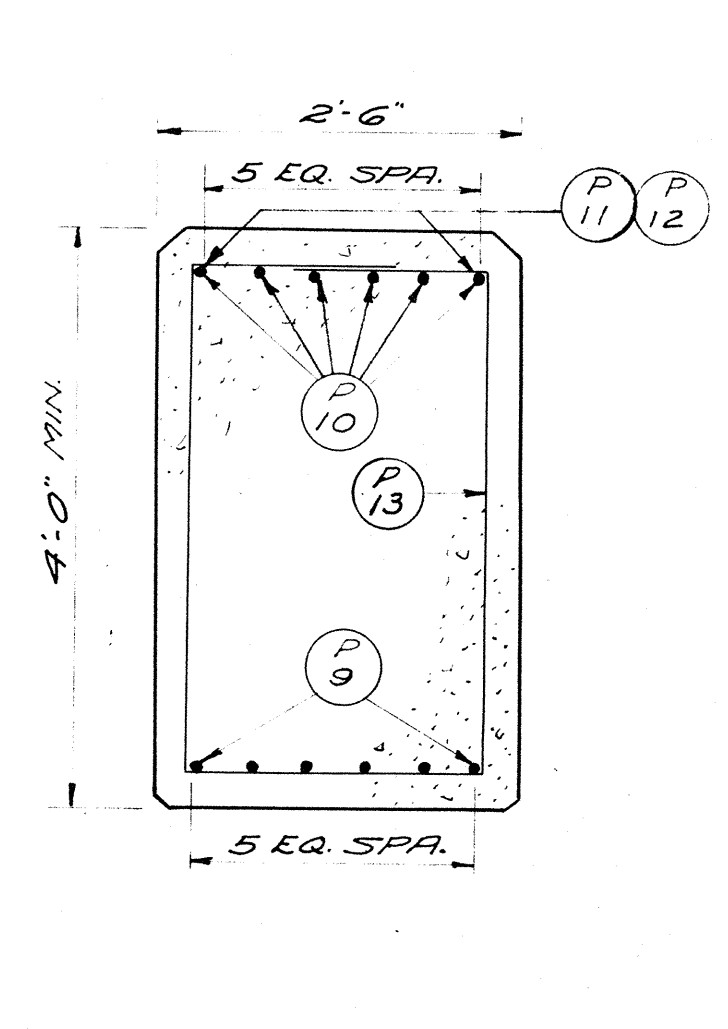
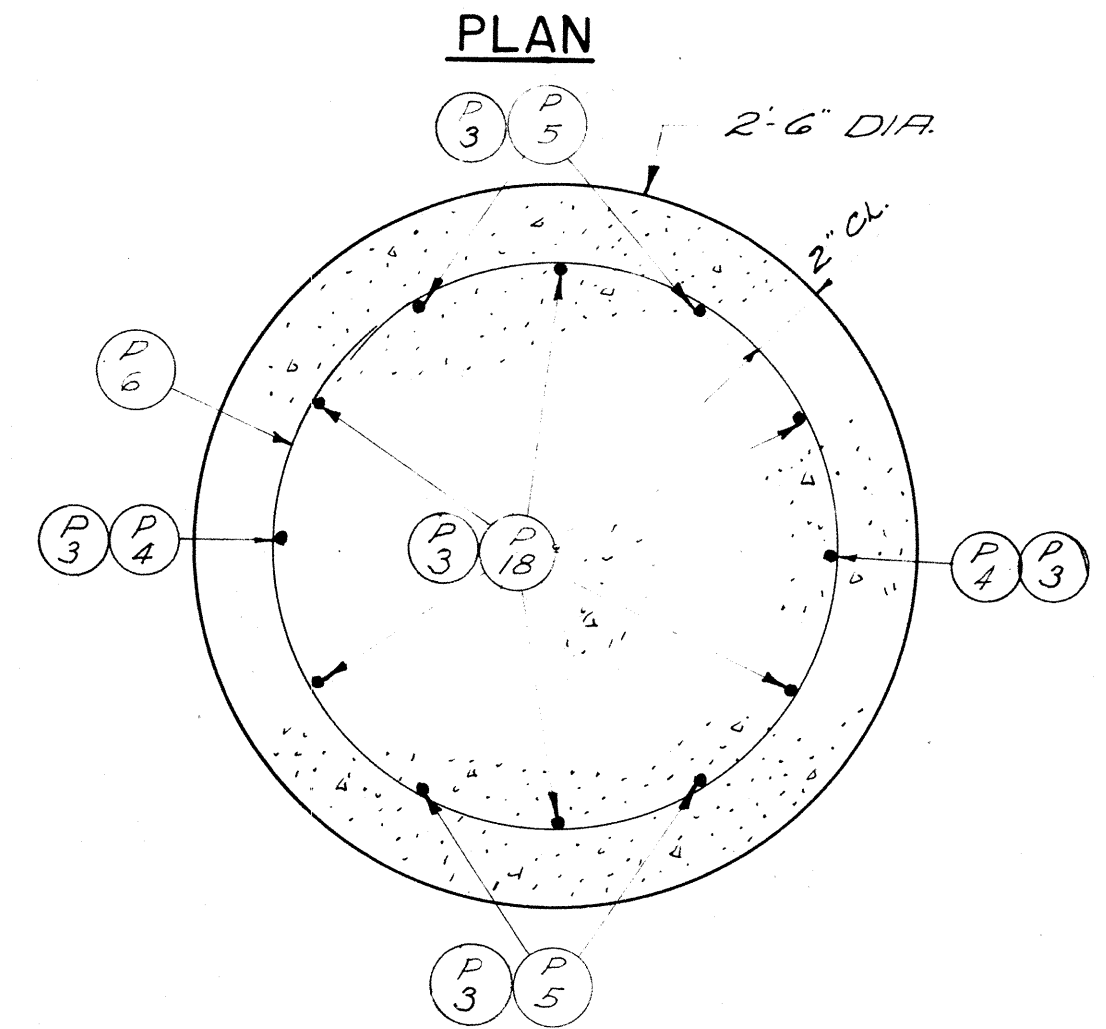
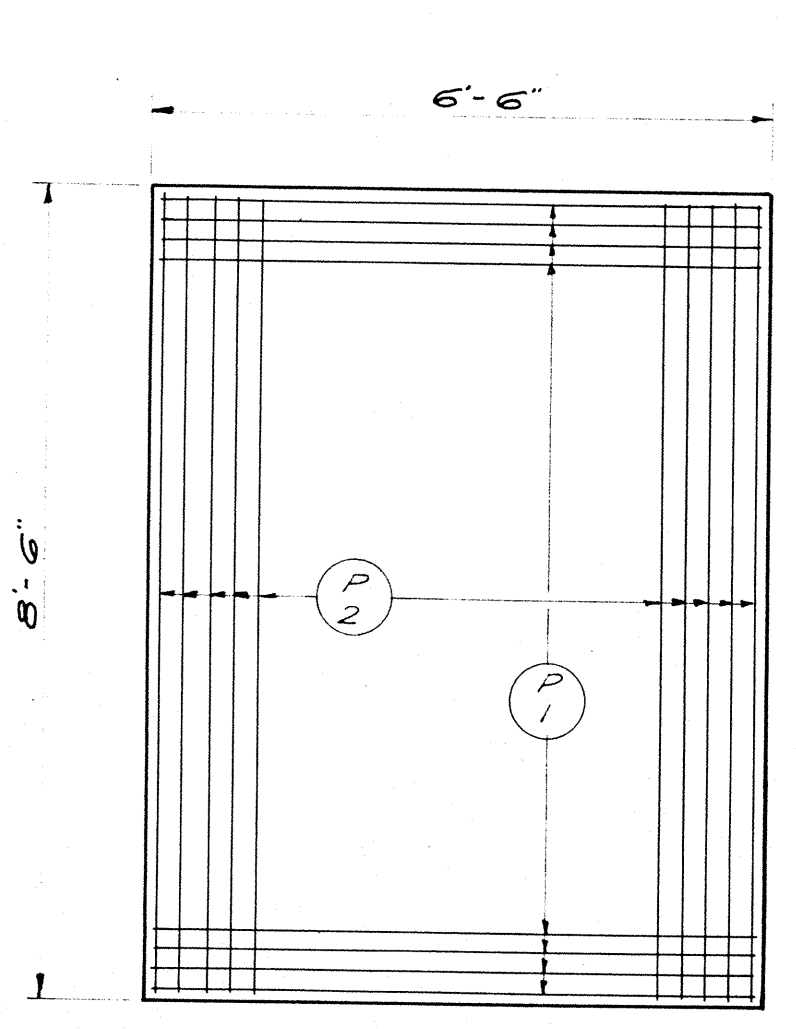
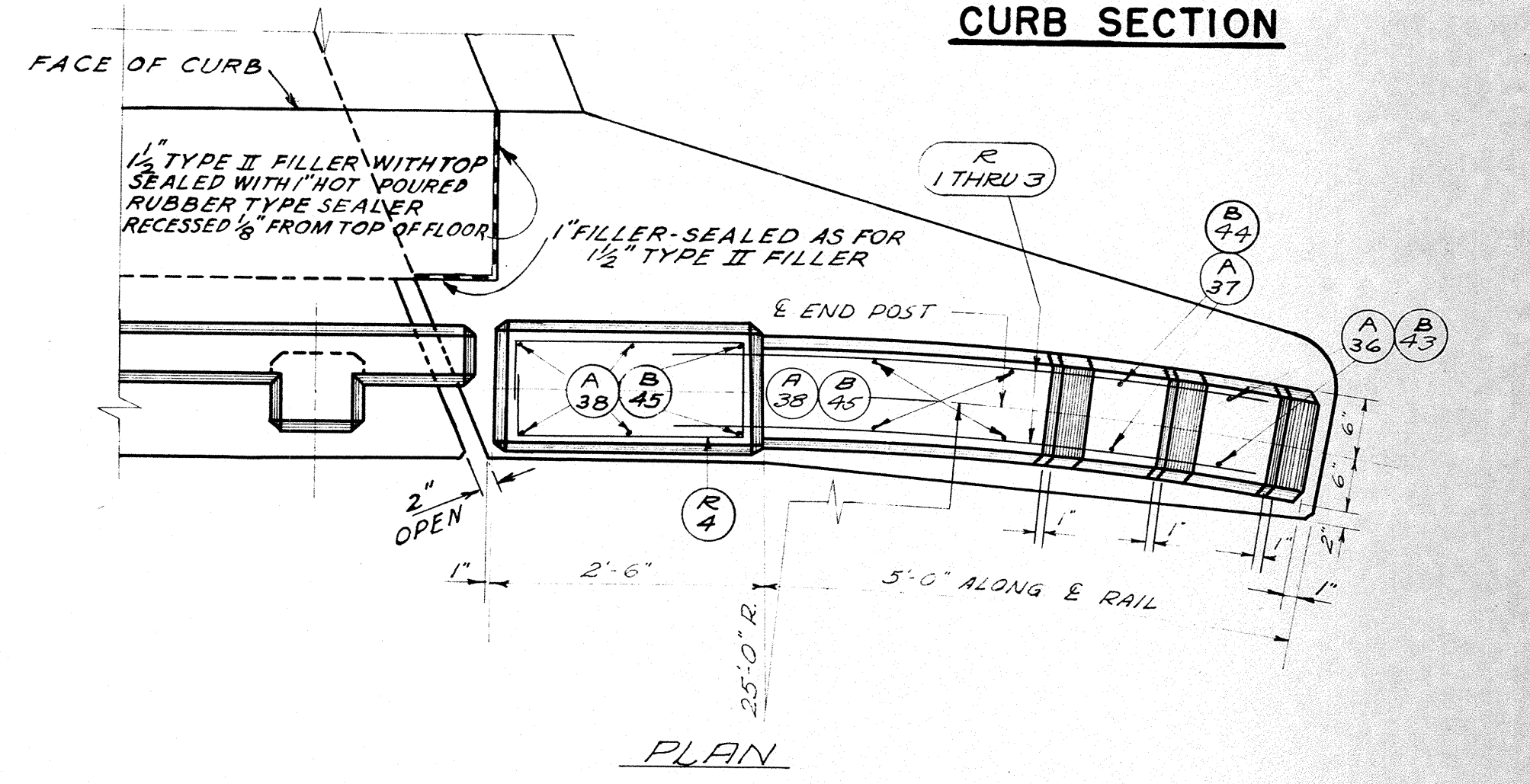
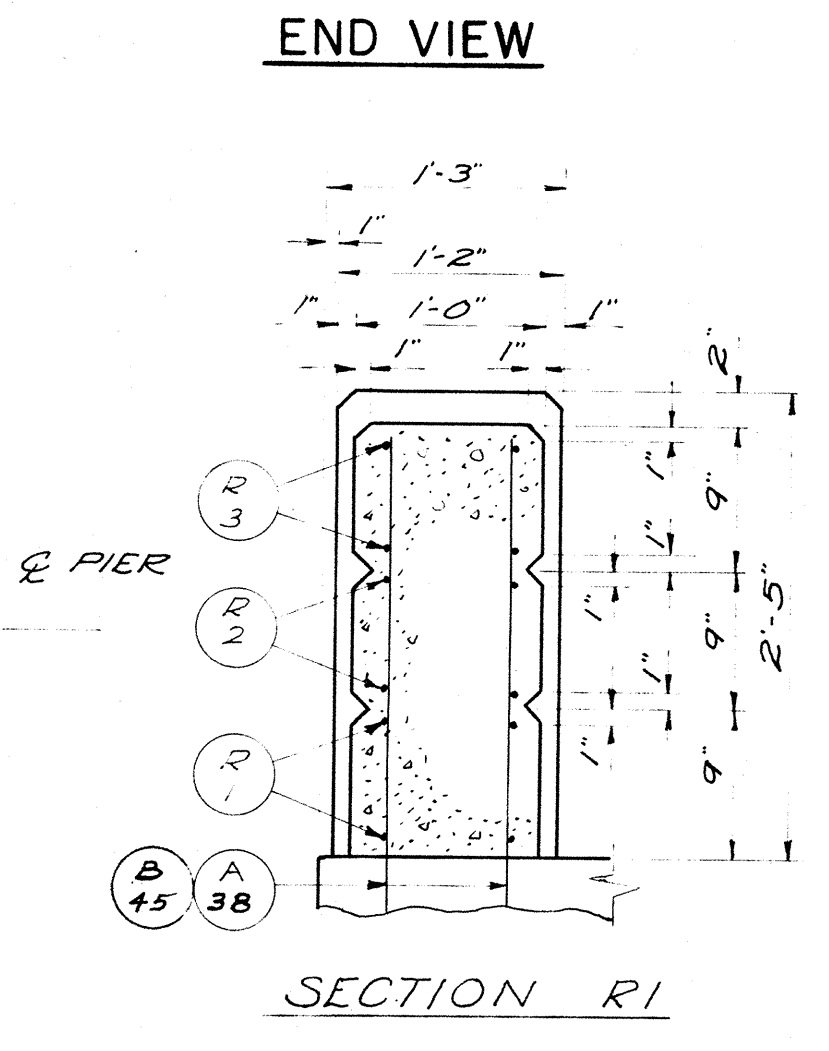
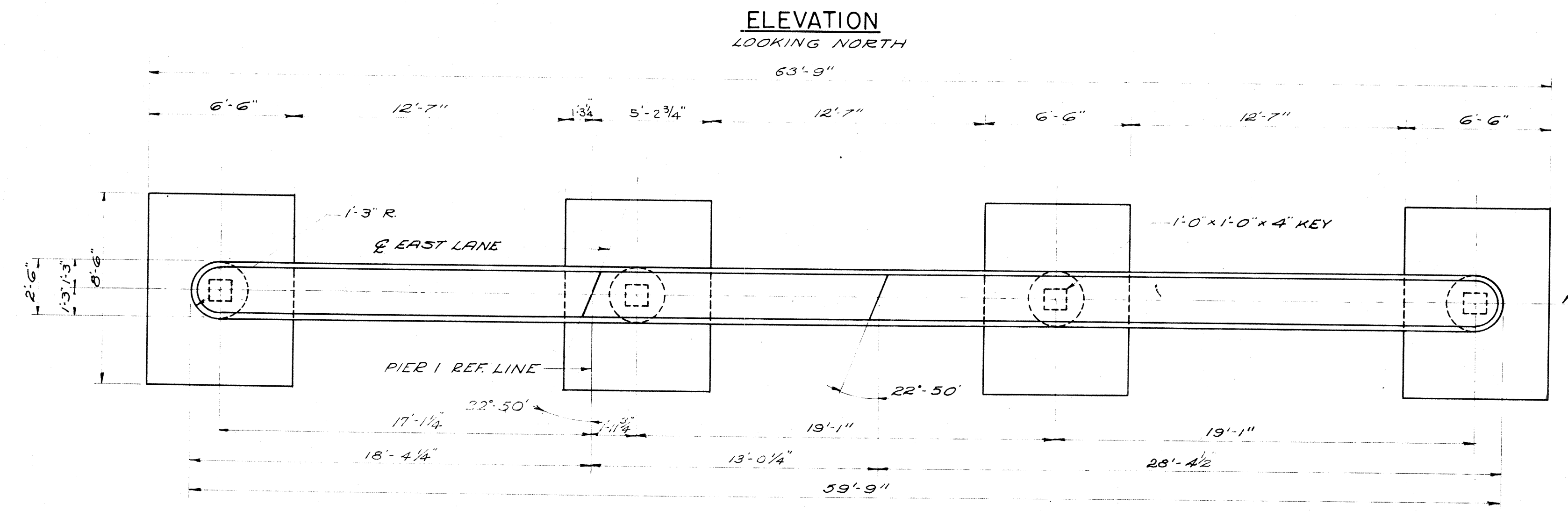
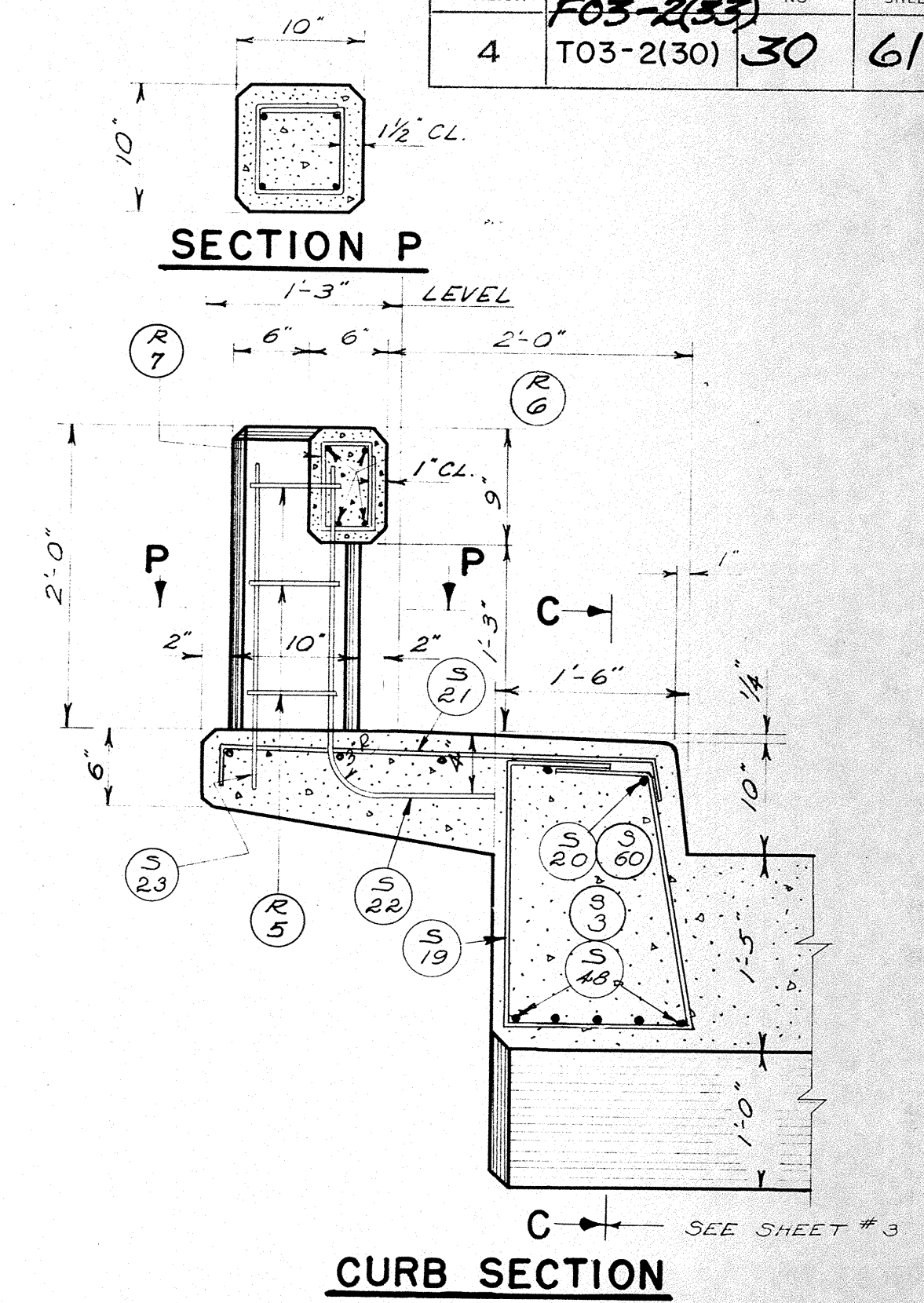
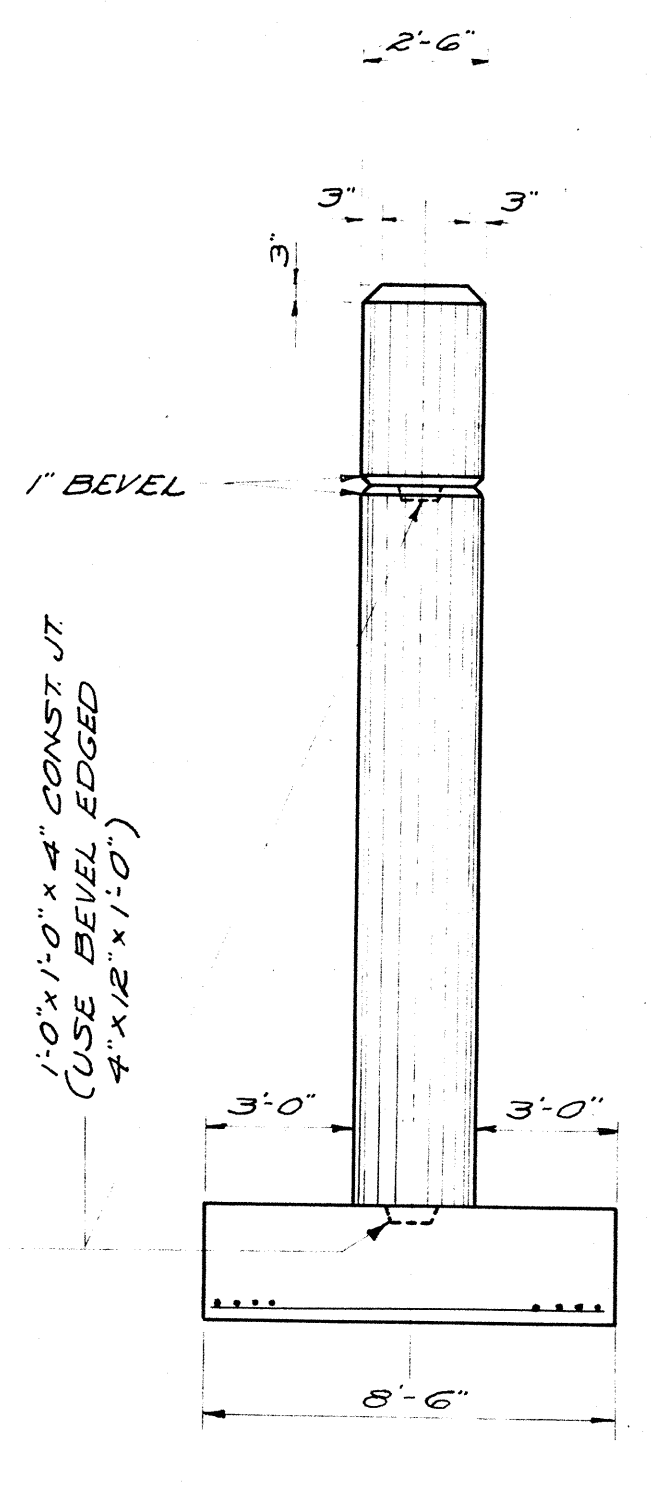
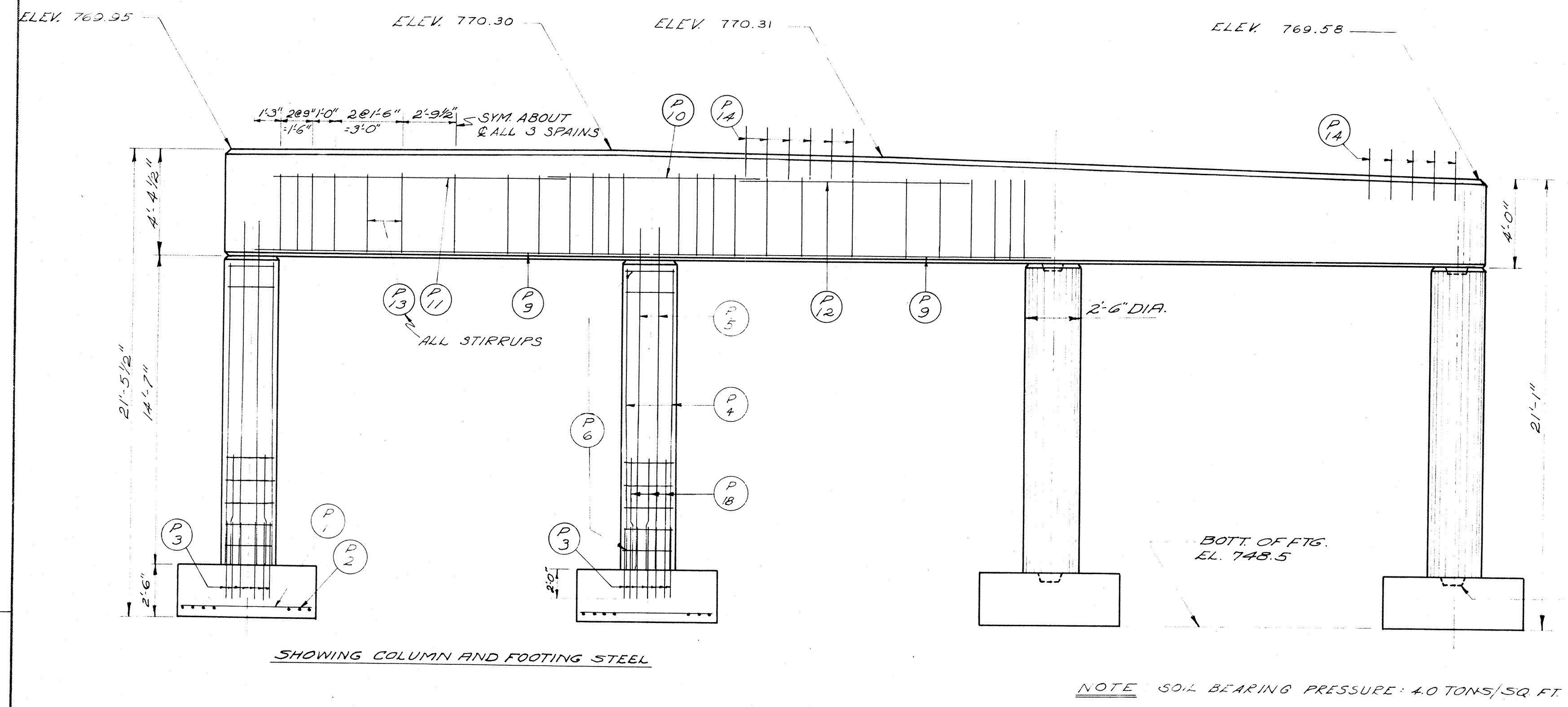


LONGITUDINAL SECTION



ALTERNATE LOCATIONS OF OPTIONAL CONST. JOINT. (LOCATION DEPENDS ON DIRECTION OF POURING) FORM JOINT WITH BEVELED F&B.

REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN		
	SUPERSTRUCTURE		
	DESIGN SPEC. AA3HO 53	LOADING H20-516	CONST. SPEC. 1957
	DATE 9-23-57	DESIGN T.M.G.	DRAWN C.L. CKD
STRUCTURE	B - 70 - 28	SHEET	3 OF 8



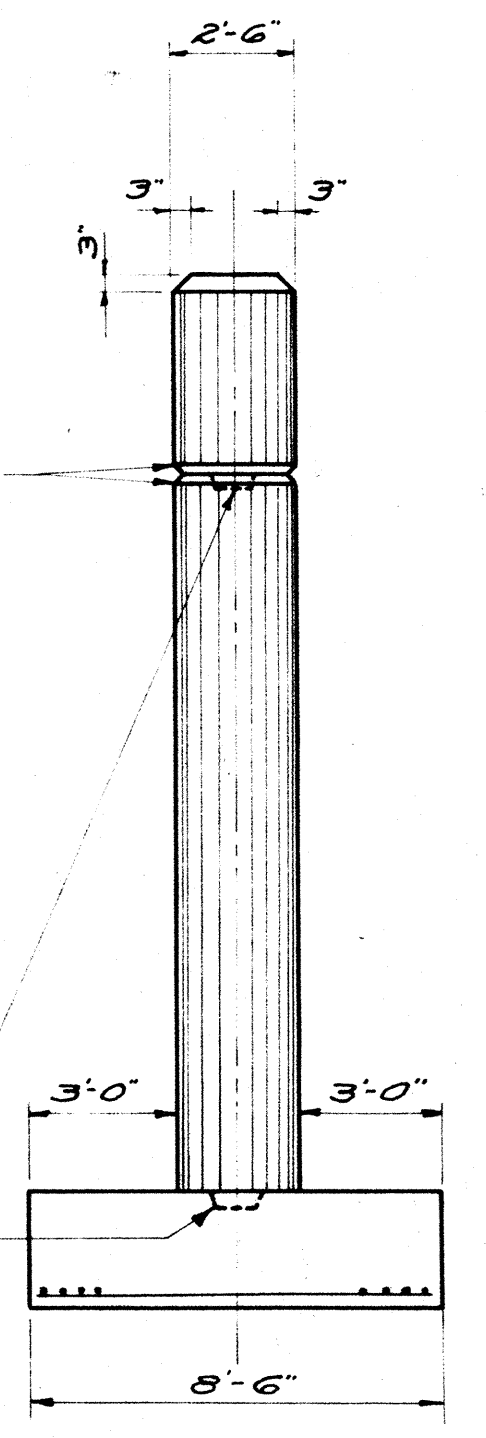
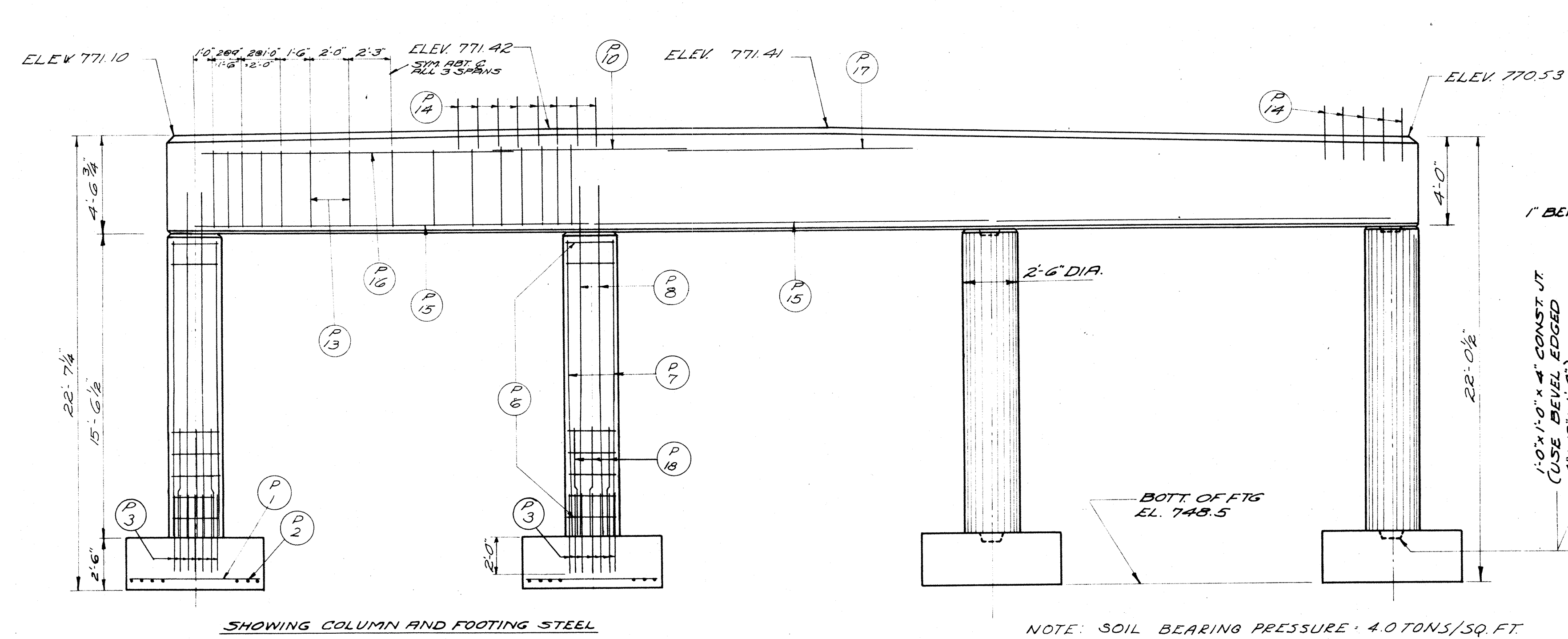
FOOTING PLAN
TYPICAL

SECTION THRU COLUMN

SECTION THRU GIRDER

RAILING AND END POST DETAILS

REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN
	PIER I & RAILING DETAIL
DESIGN SPEC. AAASHO '53	LOADING H20-S16
D. T. 9-23-57	DESIGN F.C.A. DRAWN BY H.B.
STRUCTURE	B-70-28
	SHEET 4 OF 8



BILL OF BARS
DIMENSIONS IN BENDING ARE OUT TO OUT
TWO PIERS

(13,320#)

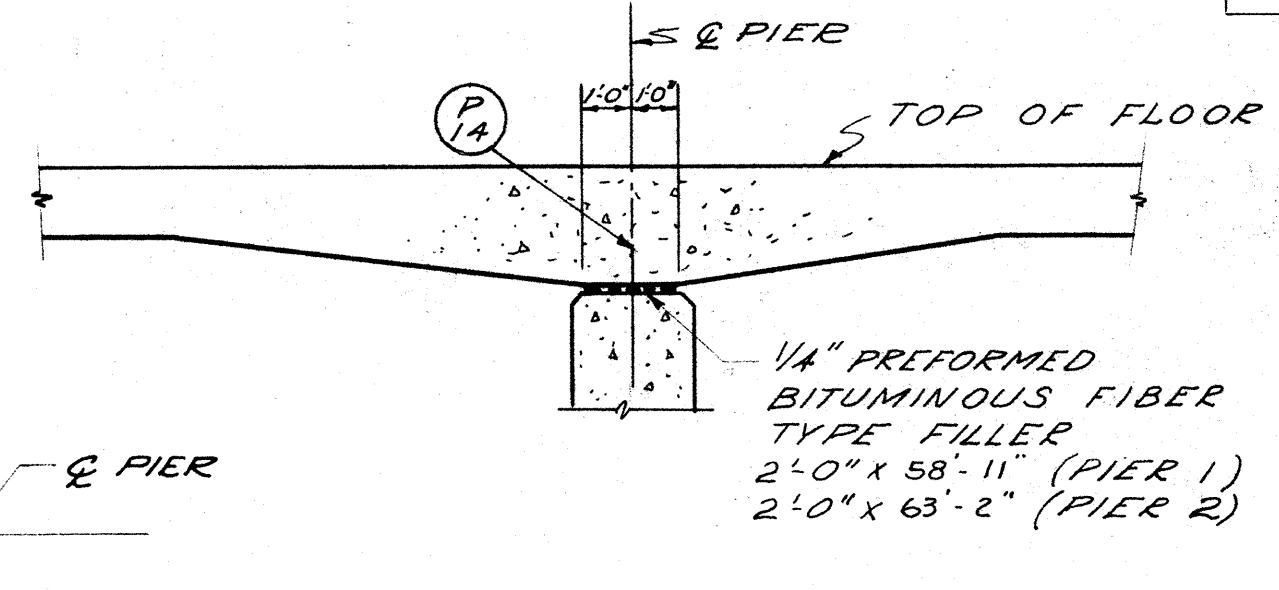
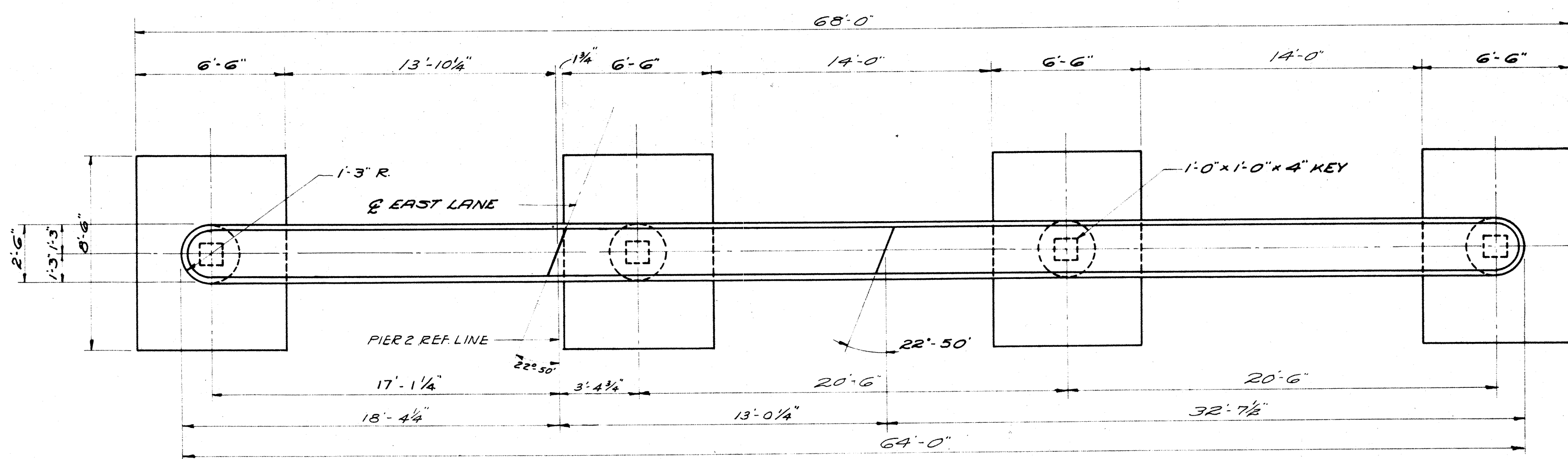
	MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
FTGS.	P 1	104	7	6'-0"	6"	FOOTING	
	P 2	160	5	8'-0"	5"		
	P 3	96	3	4'-0"	SHOWN	& COLUMN	
COLUMNS	P 4	8	8	14'-6"	SHOWN	COLUMNS - PIER 1	
	P 5	16	8	16'-9"		& GIRDER - PIER 1	
	P 6	124	4	7'-9"	1'-0"	COL TIES 15 PIER 1, 3 EA COL	A
	P 7	8	8	15'-3"	SHOWN	COLUMNS - PIER 2	
	P 8	16	8	17'-6"		& GIRDER - PIER 2	
	P 18	48	8	5'-0"		- BOTH PIERS	
GIRDER	P 9	18	10	19'-0"	SHOWN	GIRDER - BOTTOM - PIER 1	
	P 10	24	11	10'-0"		- TOP - CTR OVER COL.	
	P 11	4	4	14'-0"		- STIRRUP TIES - PIER 1	
	P 12	2	4	11'-0"		- STIRRUP TIES - PIER 2	
	P 13	84	6	13'-3"		- STIRRUPS 39 PIER 1, 45 PIER 2	B
	P 14	122	5	2'-6"	1'-0"	& SLAB 51 PIER 1, 63 PIER 2	
	P 15	18	10	20'-3"	SHOWN	- BOTTOM - PIER 2	
	P 16	4	4	16'-0"		- TOP - STIRRUP TIES - PIER 2	
P 17	2	4	12'-6"		- STIRRUP TIES - PIER 2		

SHOWING COLUMN AND FOOTING STEEL

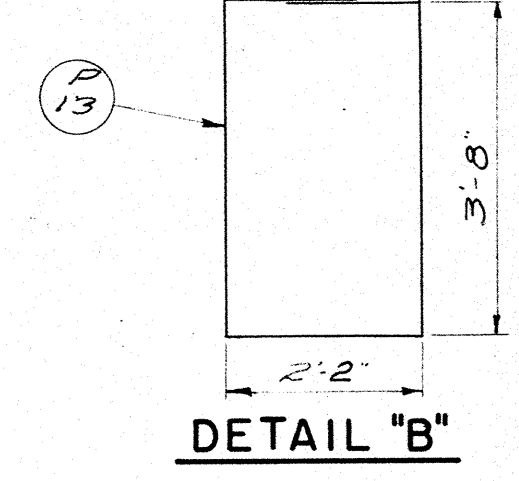
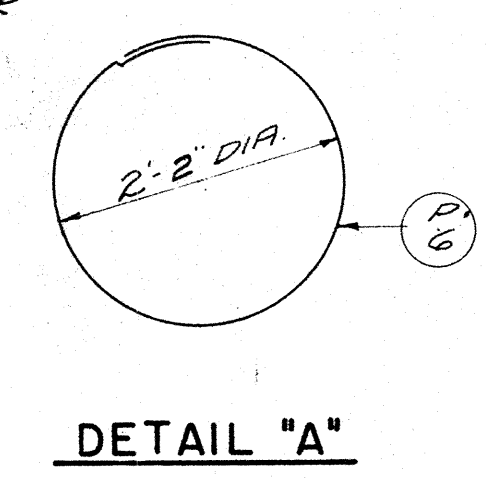
NOTE: SOIL BEARING PRESSURE 4.0 TONS/SQ. FT.

END VIEW

ELEVATION
LOOKING NORTH



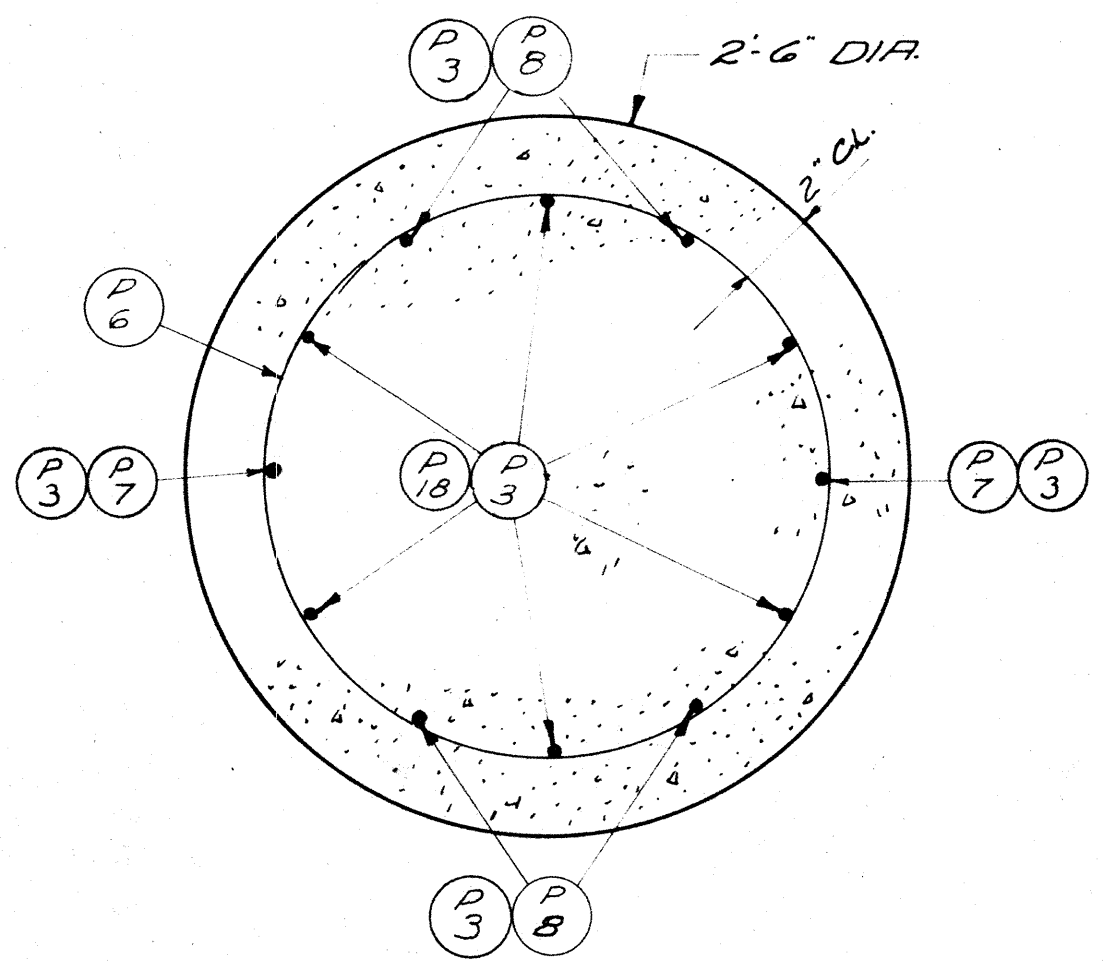
DETAIL AT PIERS



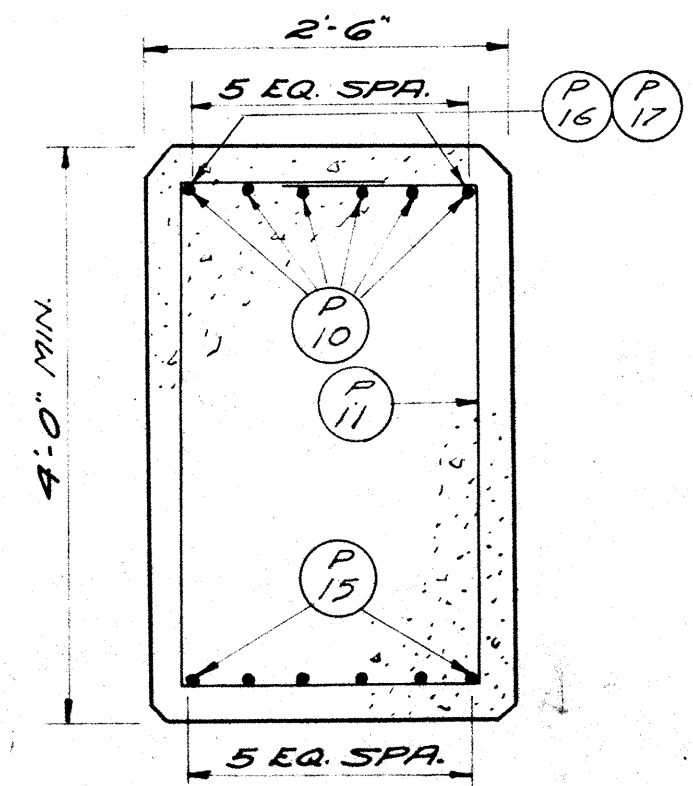
DETAIL "A"

DETAIL "B"

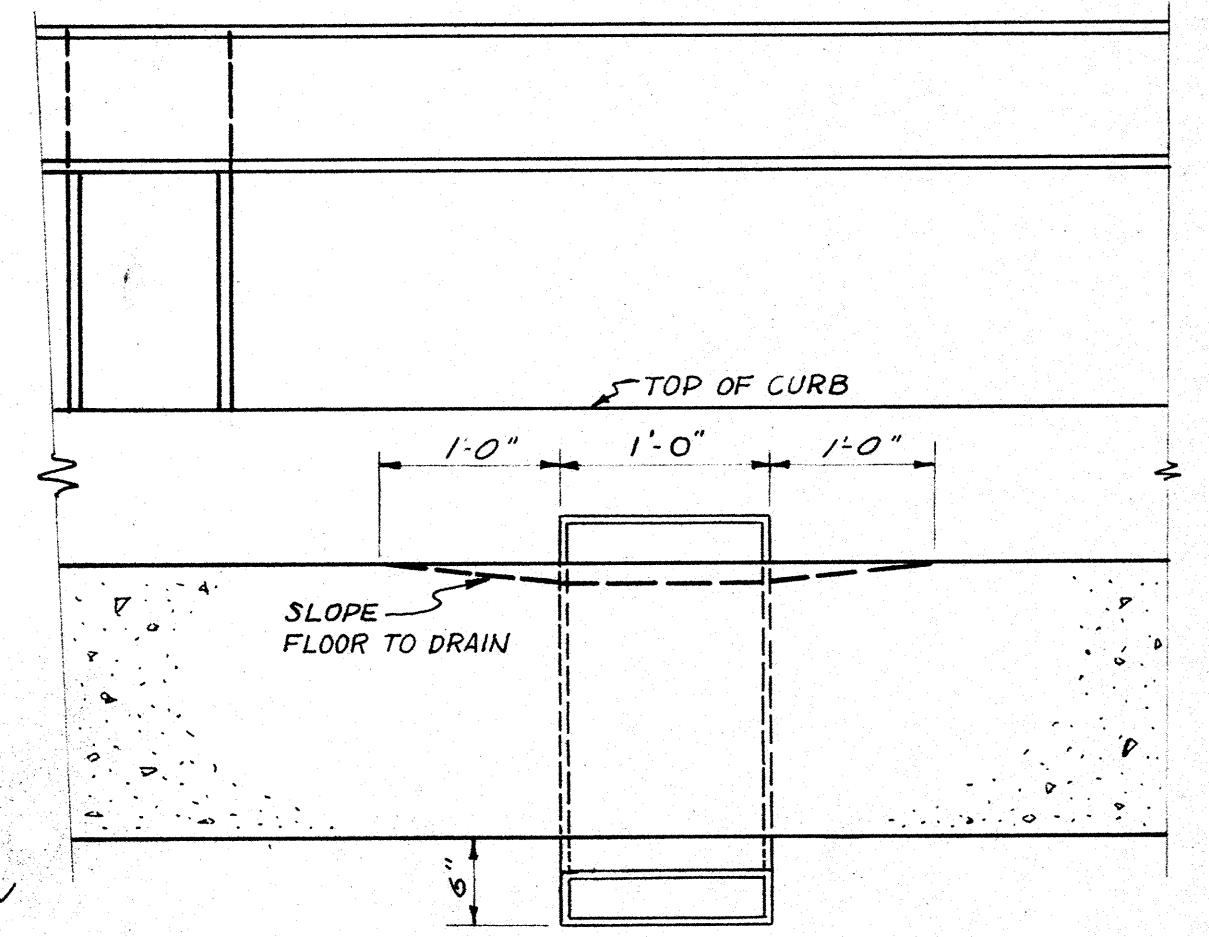
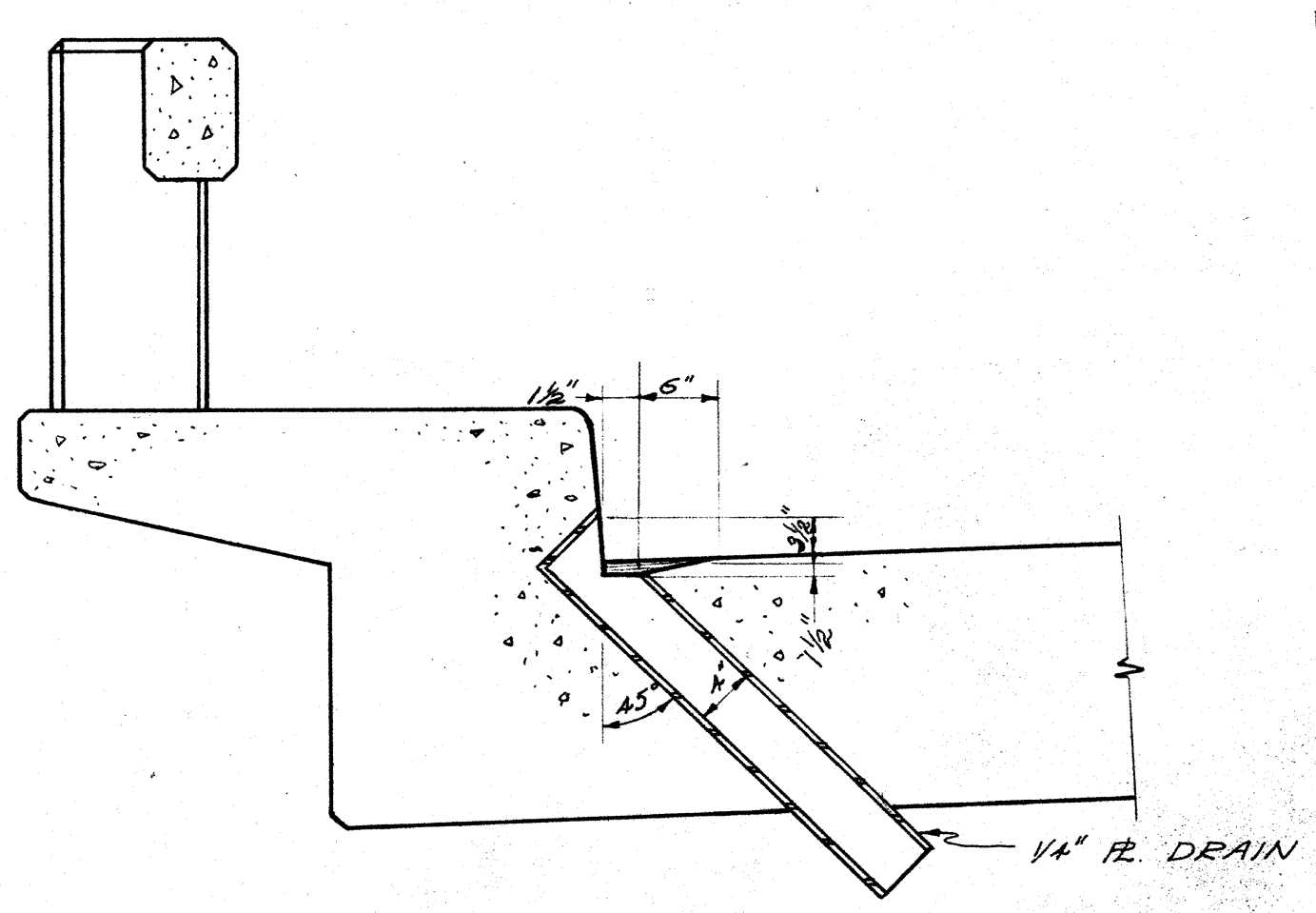
PLAN



SECTION THRU COLUMN

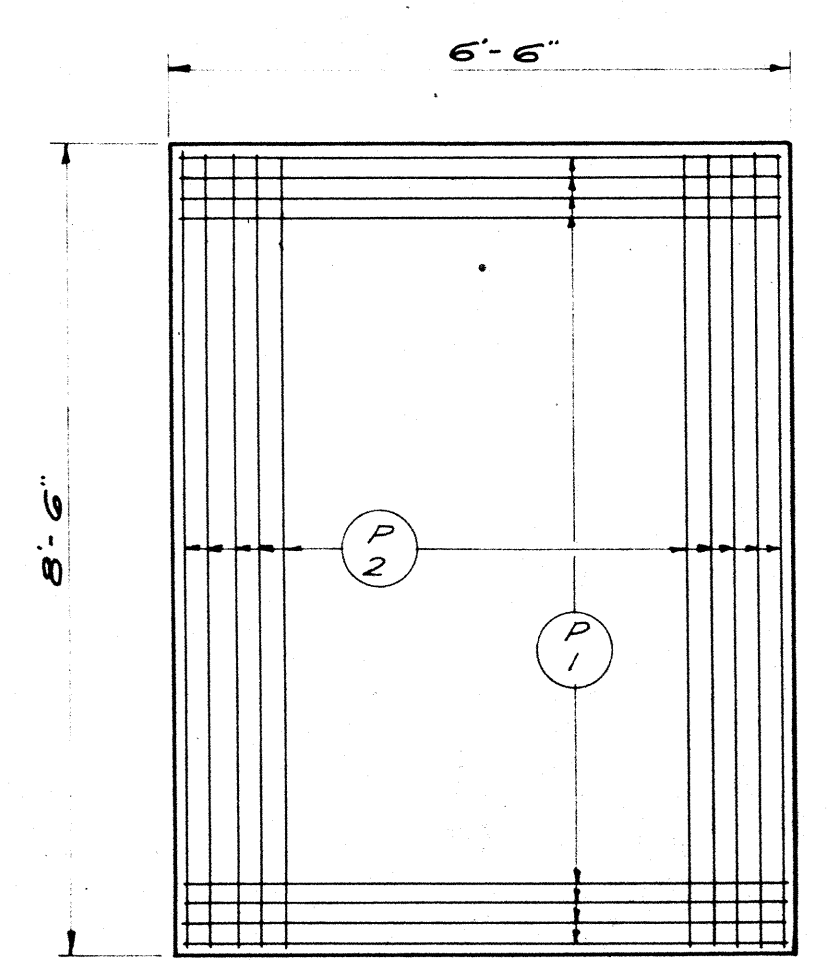


SECTION THRU GIRDER



DRAIN DETAILS

NOTE: FLOOR DRAINS TO BE CONSIDERED AS INCIDENTAL TO CONCRETE MASONRY-GRADE 'A-A1'. NO SEPARATE PAYMENT WILL BE MADE FOR THESE ITEMS.



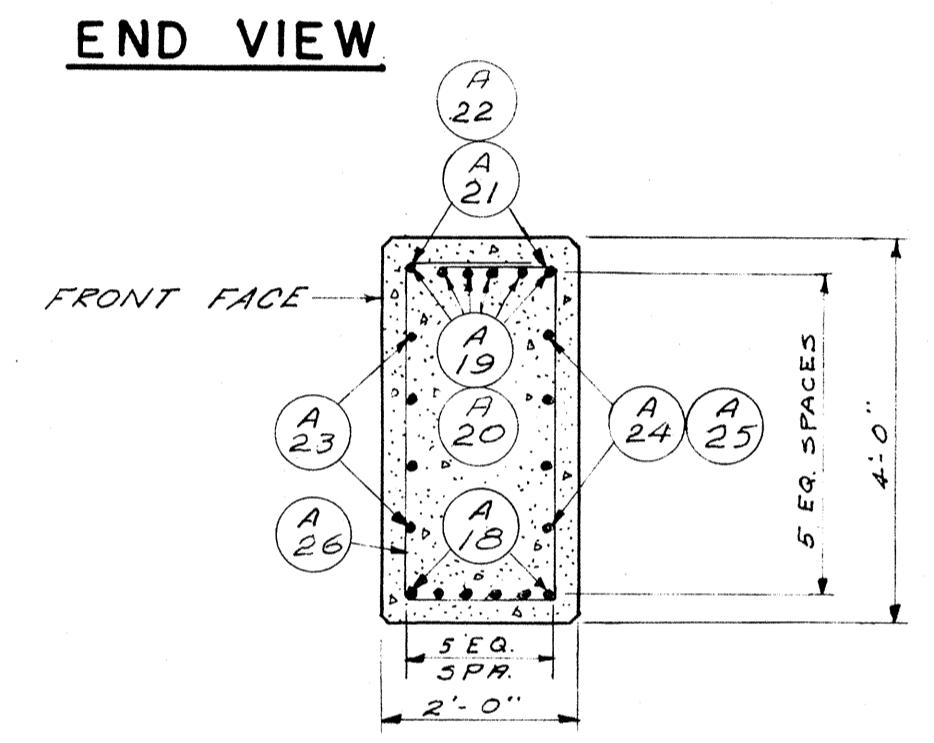
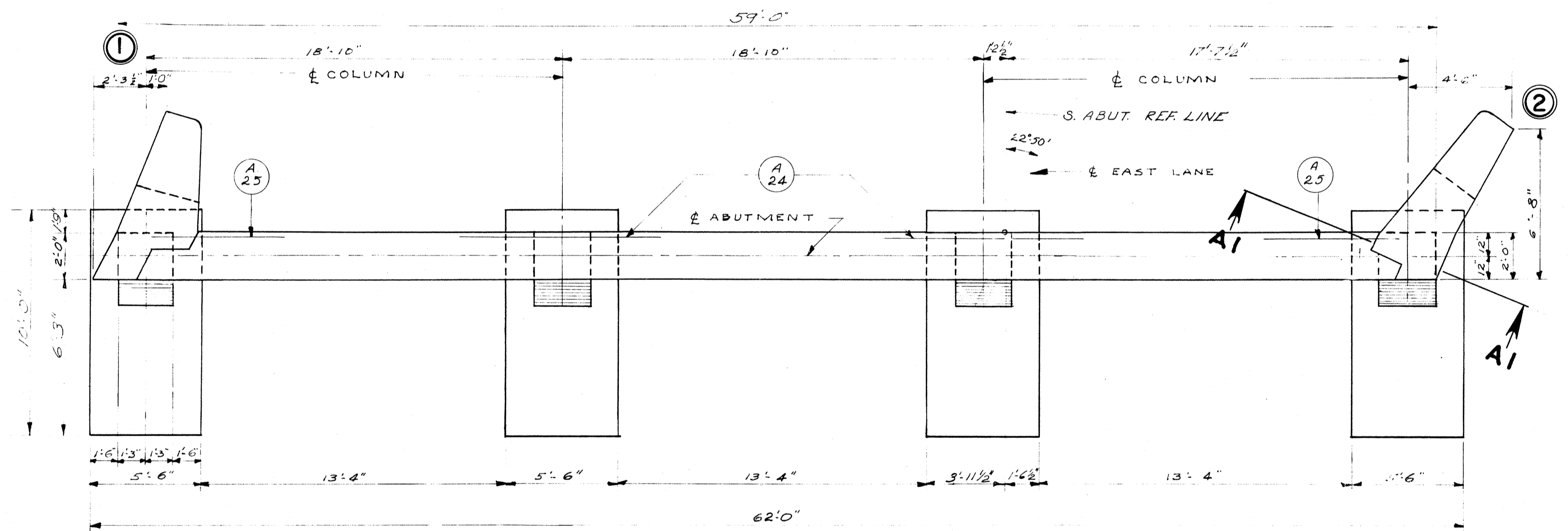
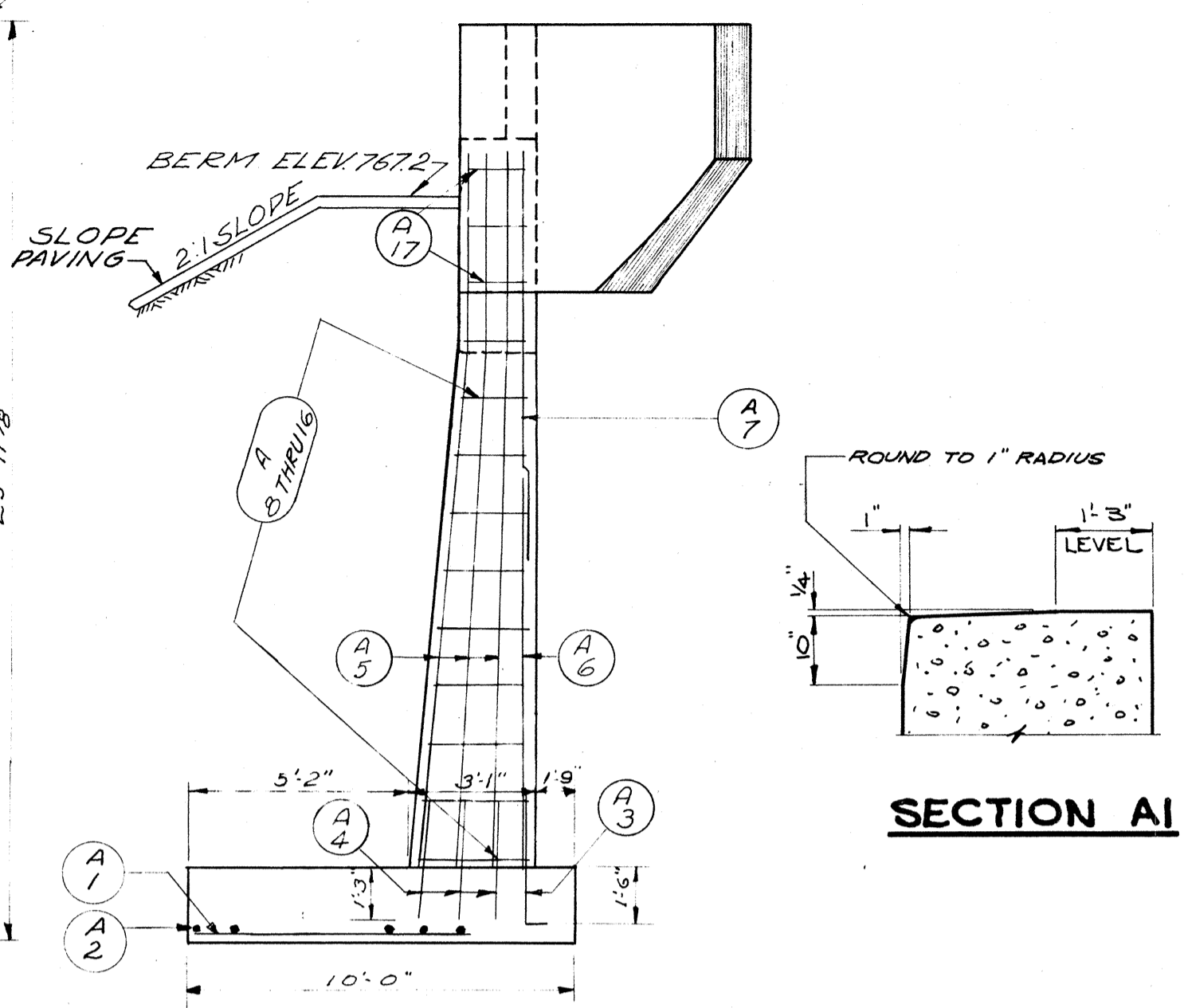
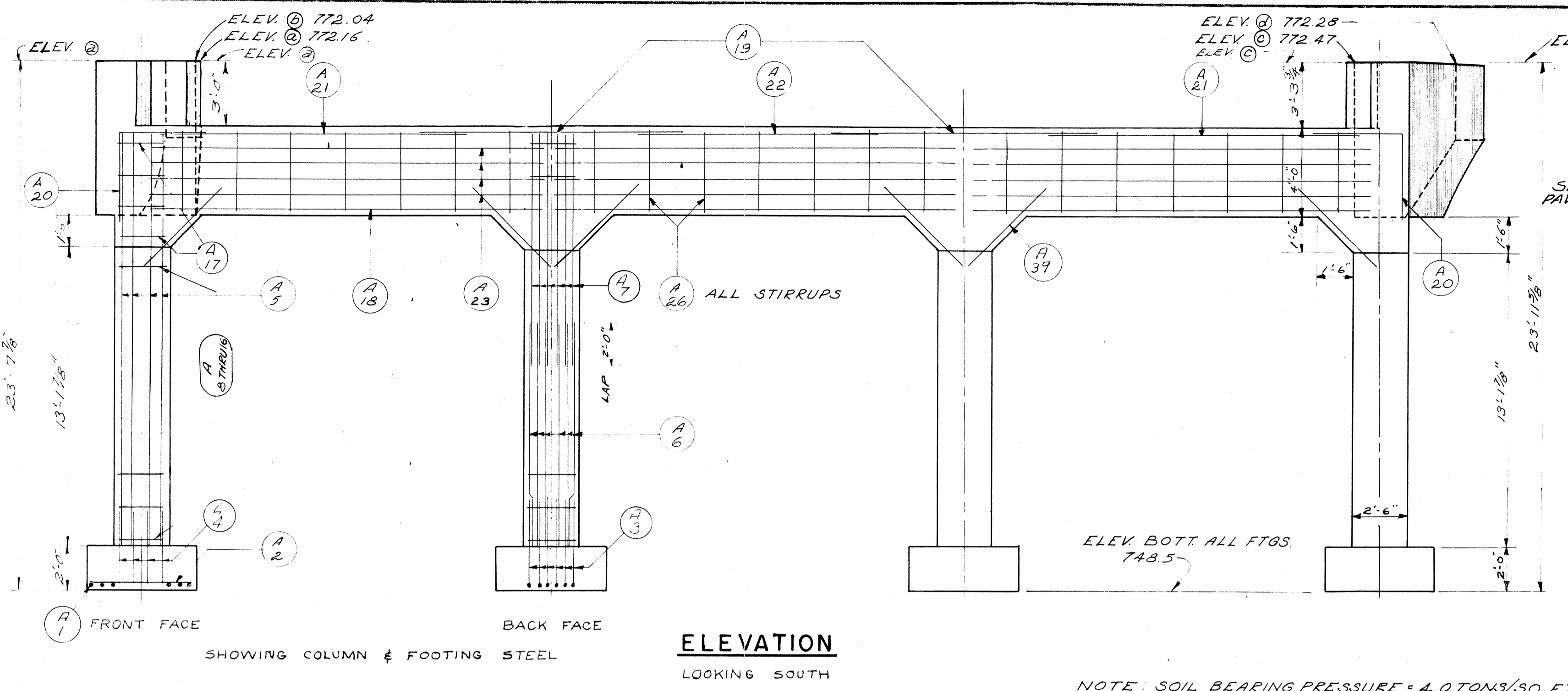
FOOTING PLAN
TYPICAL

REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN		
	PIER 2		
	DESIGN SPEC. AASHO 53	LOADING H20-316	CONST. SPEC. 1957
	DATE 9-23-57	DESIGN F.C.K.	DRAWN J.W.K. H.B.
STRUCTURE	B-70-28	SHEET	5 OF 8

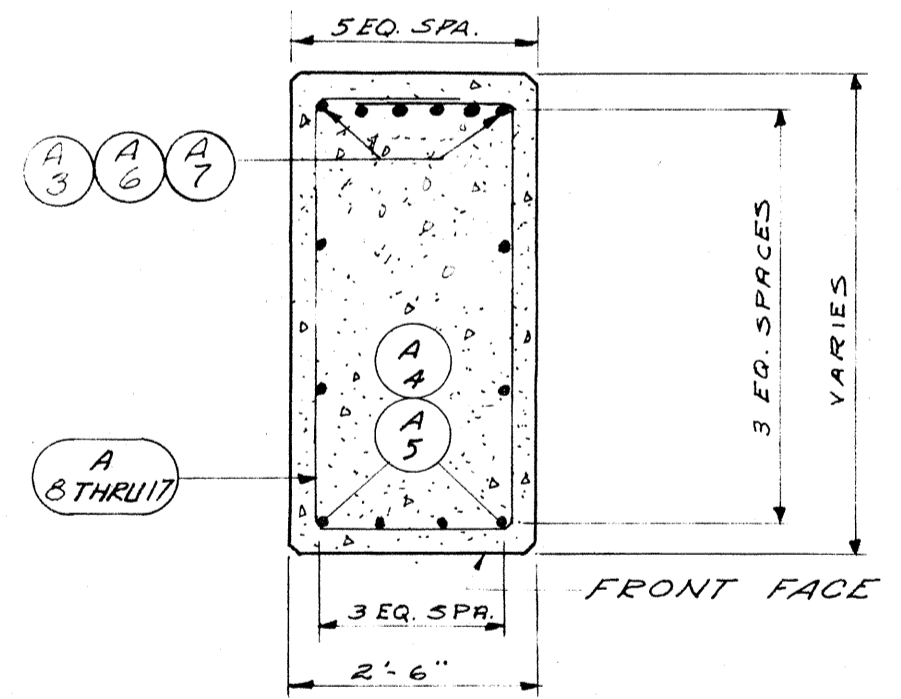
BILL OF BARS
DIMENSIONS IN BENDING ARE OUT TO OUT

SOUTH ABUTMENT (6,360#)

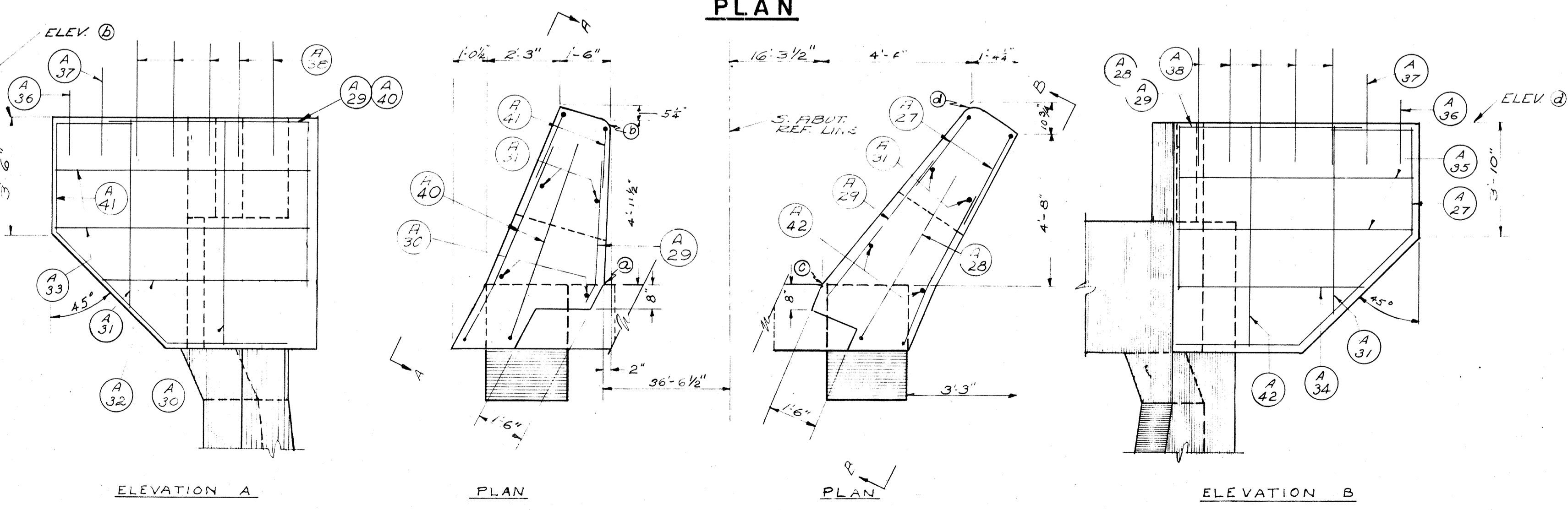
POUR	MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
FTGS.	A 1	52	8	7-6	5	FTG - TOP - BOTTL - LONGIT.	
	A 2	32	4	5-0	1-0	" " " " - TRANSV.	
	A 3	24	9	4-6	SHOWN	" " " " - BACK FACE	D
	A 4	32	5	2-6	"	" " " " - FRONT FACE & SIDES	
COLUMNS, GIRDERS & WINGS	A 5	32	5	18-3	SHOWN	COLS - VERT - FRONT FACE & SIDES	
	A 6	24	9	10-0	"	" " " " - BACK FACE	
	A 7	24	8	10-6	"	" " " " - " " " "	
	A 8	4	4	10-9	1-6	" " " " - TIES	
	A 9	4	4	10-6	1-6	" " " " - " " " "	A
	A 10	4	4	10-3	1-6	" " " " - " " " "	A
	A 11	4	4	10-0	1-6	" " " " - " " " "	A
	A 12	4	4	9-9	1-6	" " " " - " " " "	A
	A 13	4	4	9-6	1-6	" " " " - " " " "	A
	A 14	4	4	9-3	1-6	" " " " - " " " "	A
	A 15	4	4	9-0	1-6	" " " " - " " " "	A
	A 16	4	4	8-9	1-6	" " " " - " " " "	A
	A 17	4	4	8-9	1-6	" " " " - " " " "	A
	A 18	18	8	18-9	SHOWN	GIRDER - BOTTOM	
	A 19	12	9	10-0	"	GIRDER - TOP - INT. COLS.	
	A 20	12	5	8-0	"	" " " " - TOP EXT. COLS.	B
	A 21	4	4	13-0	"	" " " " - STIRRUP SUPPORT	
	A 22	2	4	10-6	"	" " " " - " " " "	
	A 23	12	6	18-9	"	" " " " - FRONT FACE	
	A 24	8	6	10-0	"	" " " " - BACK " " - INT. COLS.	
	A 25	8	5	6-0	"	" " " " - " " " " - EXT. "	
	A 26	21	4	11-9	2-6	" " " " - STIRRUPS	A
	A 27	2	4	15-9	SHOWN	WING 2	C
	A 28	2	4	10-0	"	" " " " - " " " "	B
	A 29	2	4	5-0	"	WINGS 1 & 2	
	A 30	4	4	6-9	"	WINGS 1 & 2 - VERTICAL	
	A 31	4	4	5-9	"	" " " " - " " " "	
	A 32	2	4	9-3	"	WING 1 " " - HOR.	
	A 33	4	4	7-9	"	" " " " - " " " "	
	A 34	2	4	4-6	"	" " " " - " " " "	
	A 35	4	4	7-0	"	" " " " - " " " "	
	A 36	4	5	2-0	"	END POSTS - VERT.	
	A 37	4	5	2-9	"	" " " " - " " " "	
	A 38	20	5	3-6	"	" " " " - " " " "	
A 39	24	5	5-0	6"	GIRDER - FILLETS		
A 40	2	4	11-0	SHOWN	WING 1	B	
A 41	2	4	14-6	"	WING 1	C	
A 42	2	4	7-0	"	" " " " - " " " "		



SECTION THRU ABUTMENT GIRDER

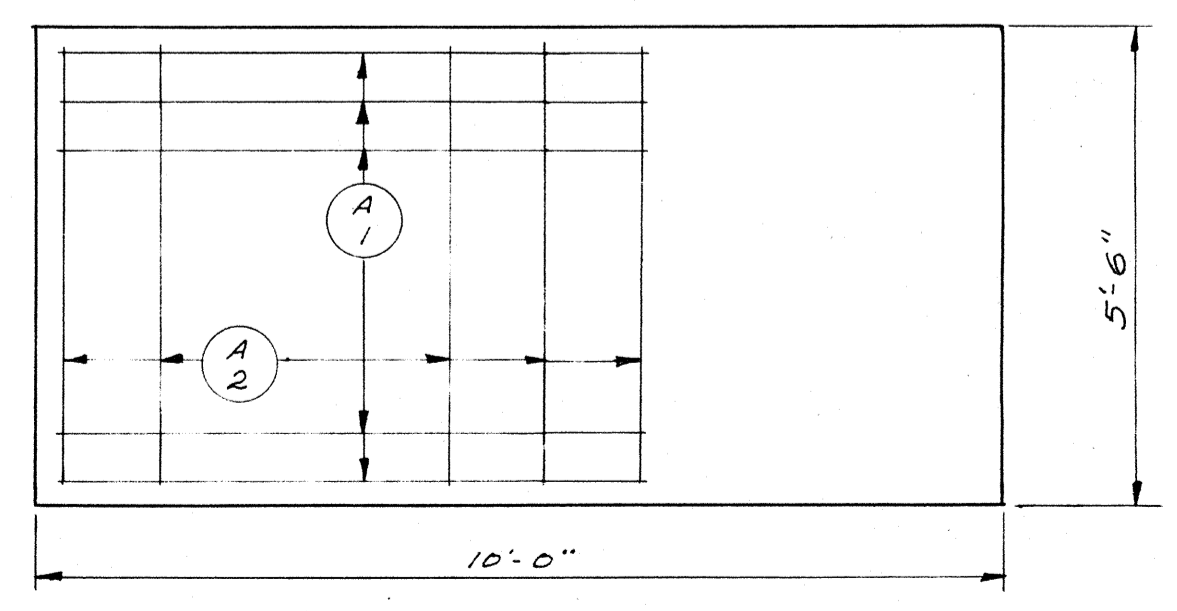


SECTION THRU ABUTMENT COLUMN

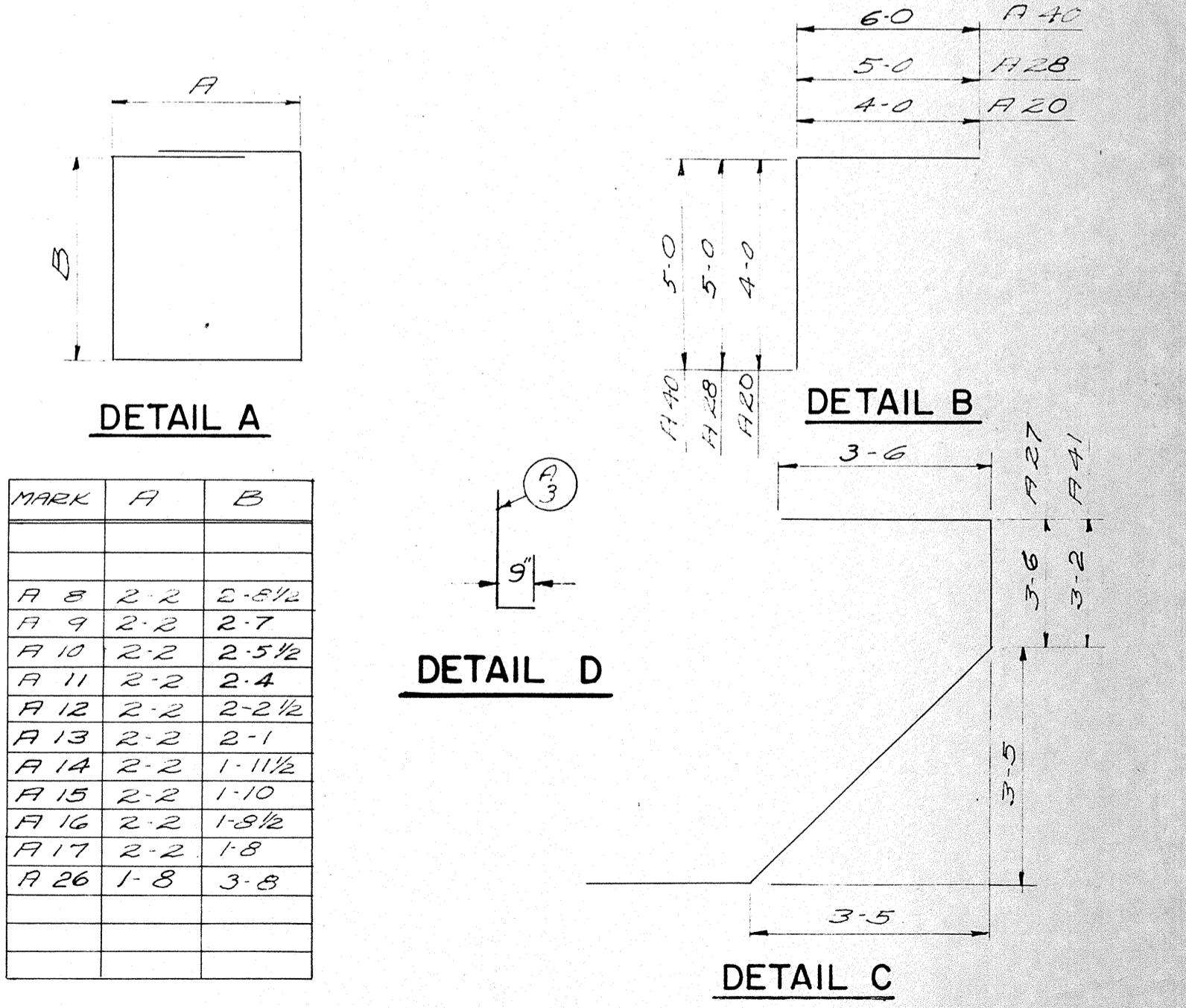


WING 1

WING 2



ABUTMENT FOOTING PLAN



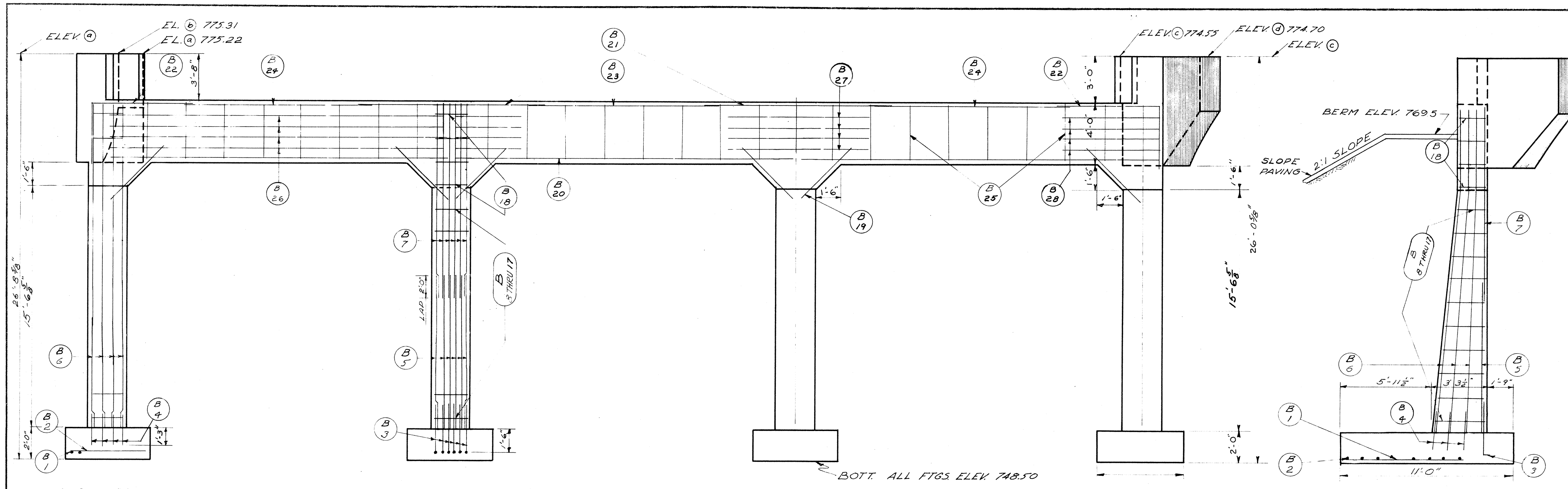
MARK	A	B
A 8	2-2	2-5 1/2
A 9	2-2	2-7
A 10	2-2	2-5 1/2
A 11	2-2	2-4
A 12	2-2	2-2 1/2
A 13	2-2	2-1
A 14	2-2	1-1 1/2
A 15	2-2	1-10
A 16	2-2	1-8 1/2
A 17	2-2	1-8
A 26	1-8	3-8

REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN
SOUTH ABUTMENT	
DESIGN SPEC AASHO 53	LOADING H20-516
DATE: 7/23/57	DESIGN: FCK
DRAWN: JMT	CKD: HJR
STRUCTURE	B - 70 - 28
SHEET	6 OF 8

BILL OF BARS
DIMENSIONS IN BENDING ARE OUT TO OUT

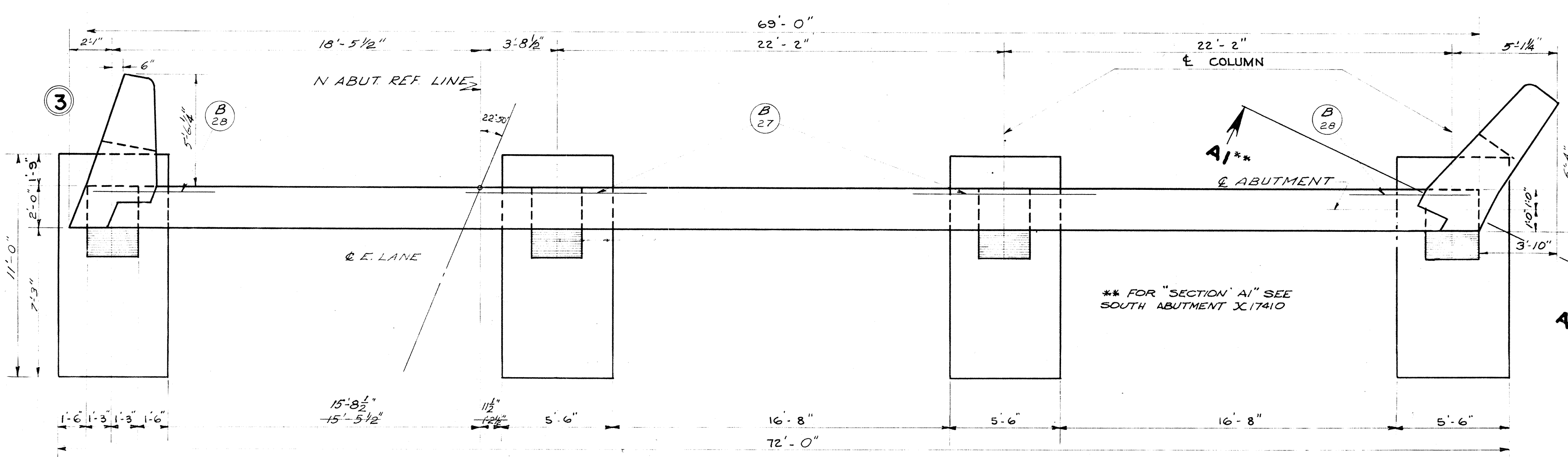
NORTH ABUTMENT (7210#)

FOUR	MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
FTGS.	B 1	52	8	7-3	5	FTS. - TOE - LONGIT.	
	B 2	32	4	5-0	1-0	" - TRANSV.	
	B 3	24	9	4-6	SHOWN	" - COL. - BACK FACE	A
	B 4	32	5	2-6	"	" - FRONT FACE & SIDES	
COLUMNS, GIRDERS & WINGS	B 5	24	9	10-0	SHOWN	COL. - BACK FACE	
	B 6	32	5	20-9	"	" - FRONT FACE & SIDES	*
	B 7	24	8	12-9	"	" - BACK FACE	
	B 8	4	2	11-3	1-6	" - TIES	B
	B 9	4	4	11-0	1-6	" - "	B
	B 10	4	4	10-9	1-6	" - "	B
	B 11	4	4	10-6	1-6	" - "	B
	B 12	4	4	10-3	1-6	" - "	B
	B 13	4	4	10-0	1-6	" - "	B
	B 14	4	4	9-9	1-6	" - "	B
	B 15	4	4	9-6	1-6	" - "	B
	B 16	4	4	9-3	1-6	" - "	B
	B 17	4	4	9-0	1-6	" - "	B
	B 18	16	4	8-9	1-6	" - "	B
	B 19	24	5	5-0	6	GIRDER - HAUNCHES	
	B 20	18	8	22-0	SHOWN	" - BOT.	
	B 21	12	9	12-0	"	" - TOP - INT. COLS.	
	B 22	12	5	10-0	"	" - EXT. COLS.	C
	B 23	2	4	12-0	"	" - STIRRUP SUPPORTS	
B 24	4	4	13-0	"	" - "		
B 25	24	4	11-9	2-6	" - STIRRUPS	B	
B 26	12	6	22-0	SHOWN	" - FRONT FACE		
B 27	8	6	12-0	"	" - BACK FACE - INT. COLS.		
B 28	8	5	6-0	"	" - EXT. COLS.		
B 29	4	4	12-0	"	WINGS 3 & 4	C	
B 30	2	4	5-6	"	" - "	*	
B 31	2	4	13-6	"	WING 3	D	
B 32	2	4	15-0	"	" - "	D	
B 33	4	4	7-6	"	" - 3 - HOR.		
B 34	2	4	5-6	"	" - "		
B 35	2	4	6-3	"	" - VERT.		
B 36	2	4	7-3	"	" - "		
B 37	4	4	7-0	"	" - 4 - HOR.		
B 38	2	4	5-3	"	" - "		
B 39	2	4	5-6	"	" - VERT.		
B 40	2	4	6-9	"	" - "		
B 41	4	5	2-0	"	" - END POST		
B 42	4	5	2-9	"	" - "		
B 43	20	5	3-6	"	" - "		

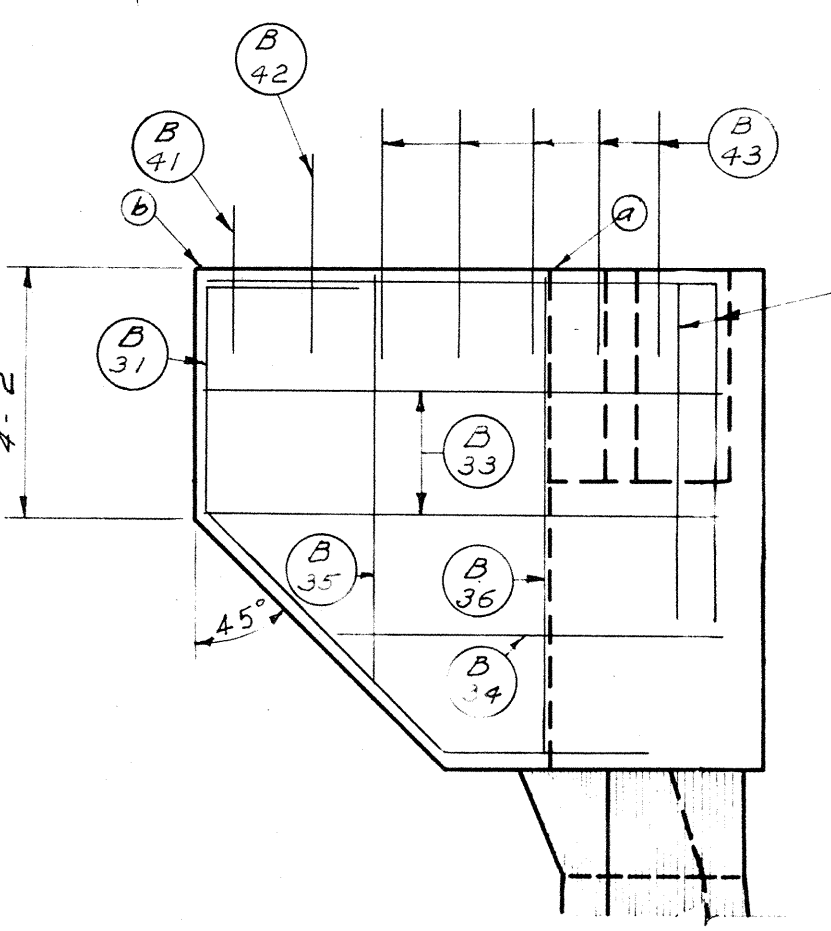


ELEVATION
LOOKING NORTH

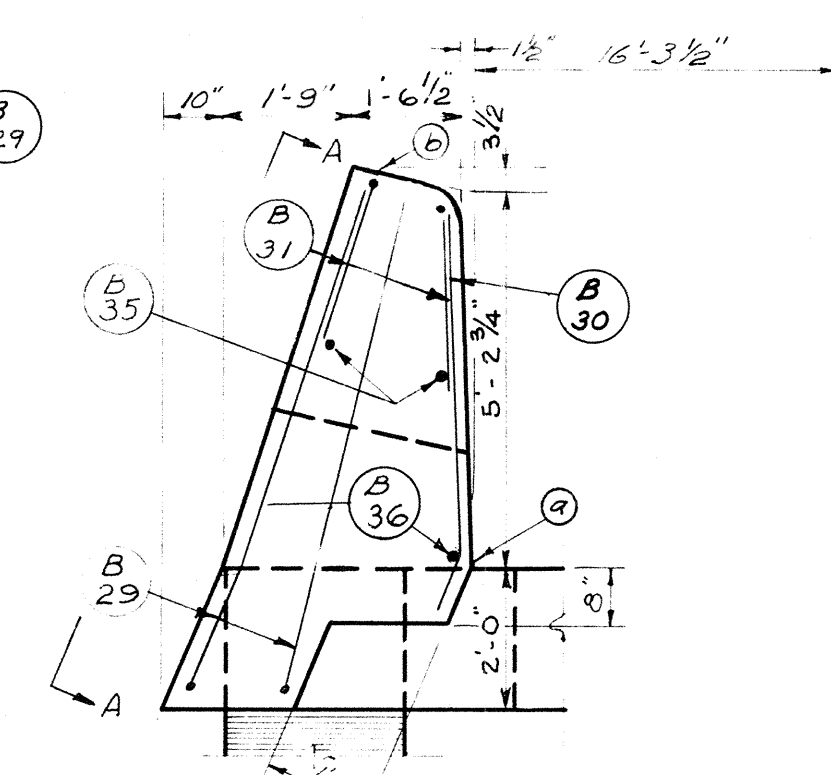
NOTE: SOIL BEARING PRESSURE = 4.0 TONS/SQ. FT.



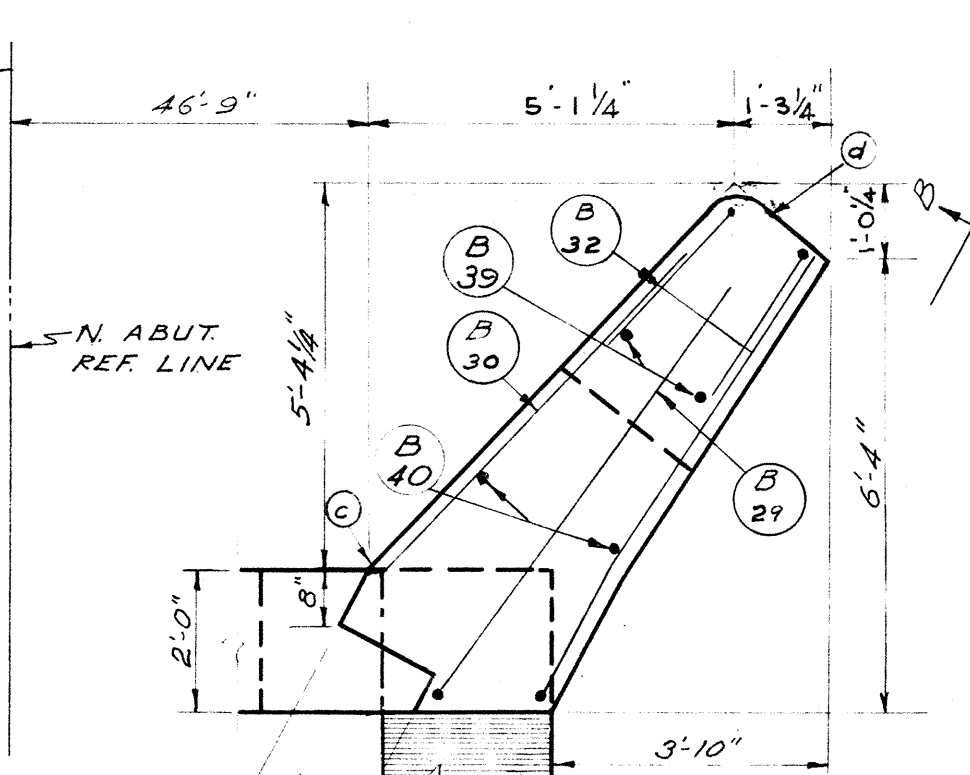
PLAN



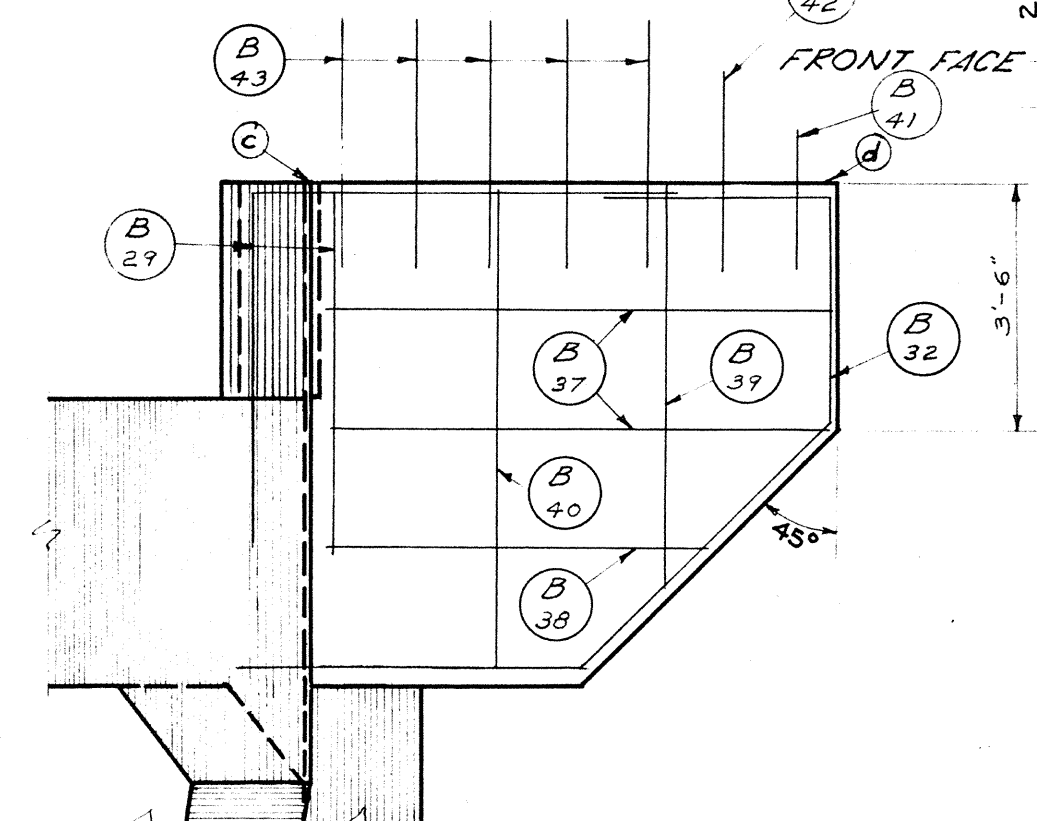
WING 3



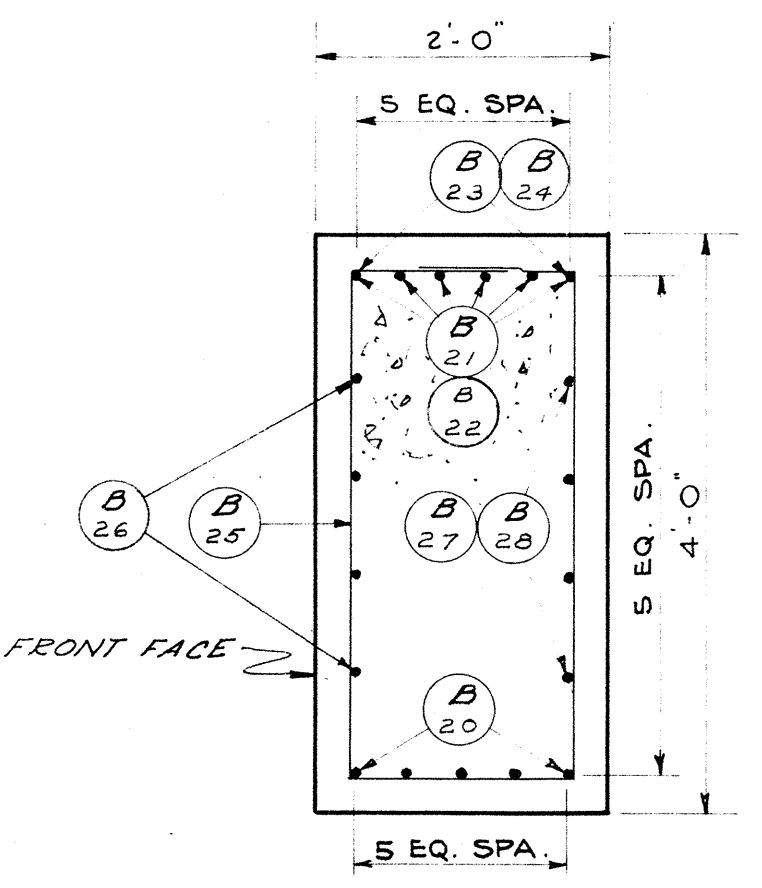
PLAN



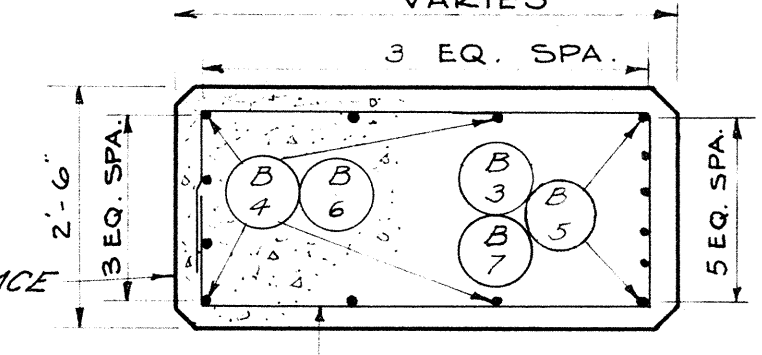
WING 4



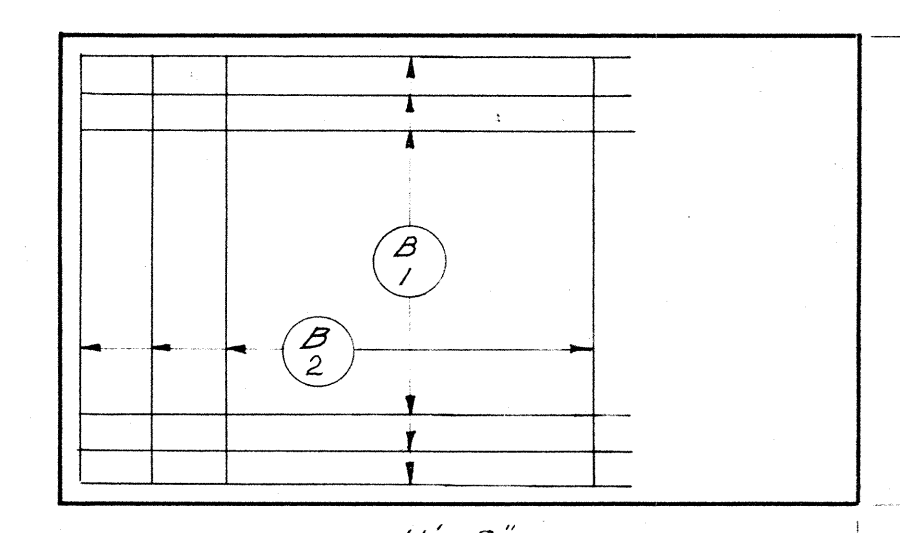
ELEVATION B



SECTION THRU ABUTMENT GIRDER



SECTION THRU COLUMN



ABUTMENT FOOTING PLAN

DETAIL A

DETAIL B

DETAIL C

DETAIL D

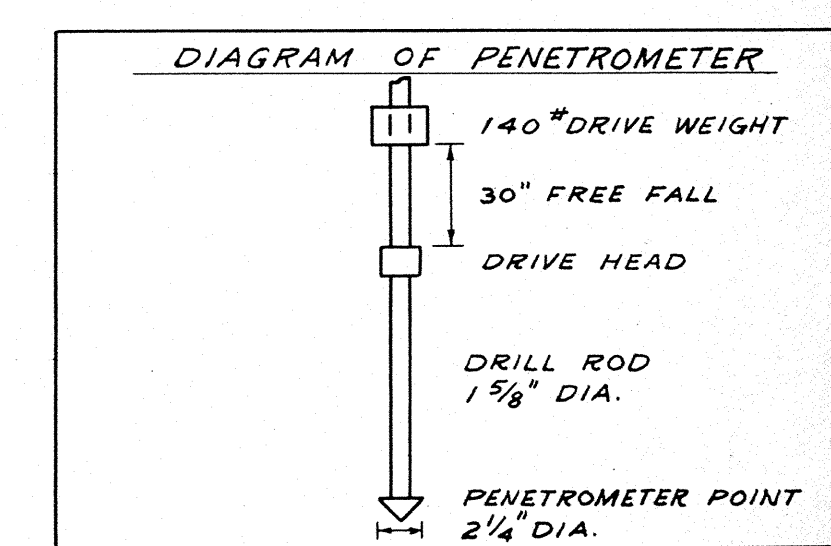
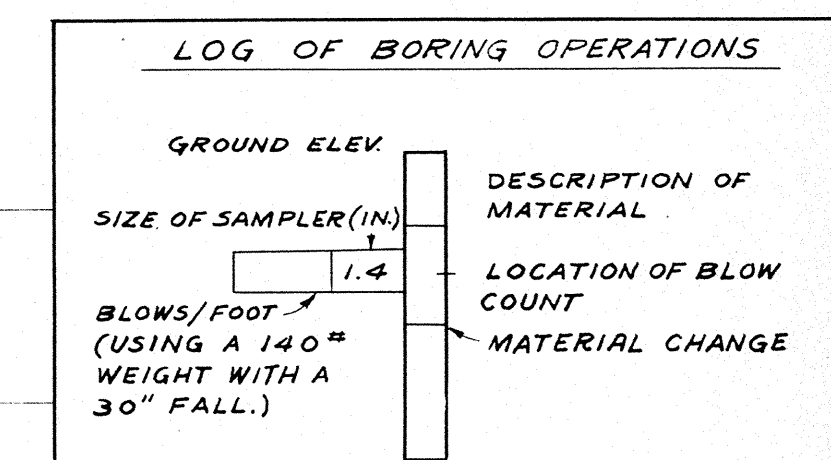
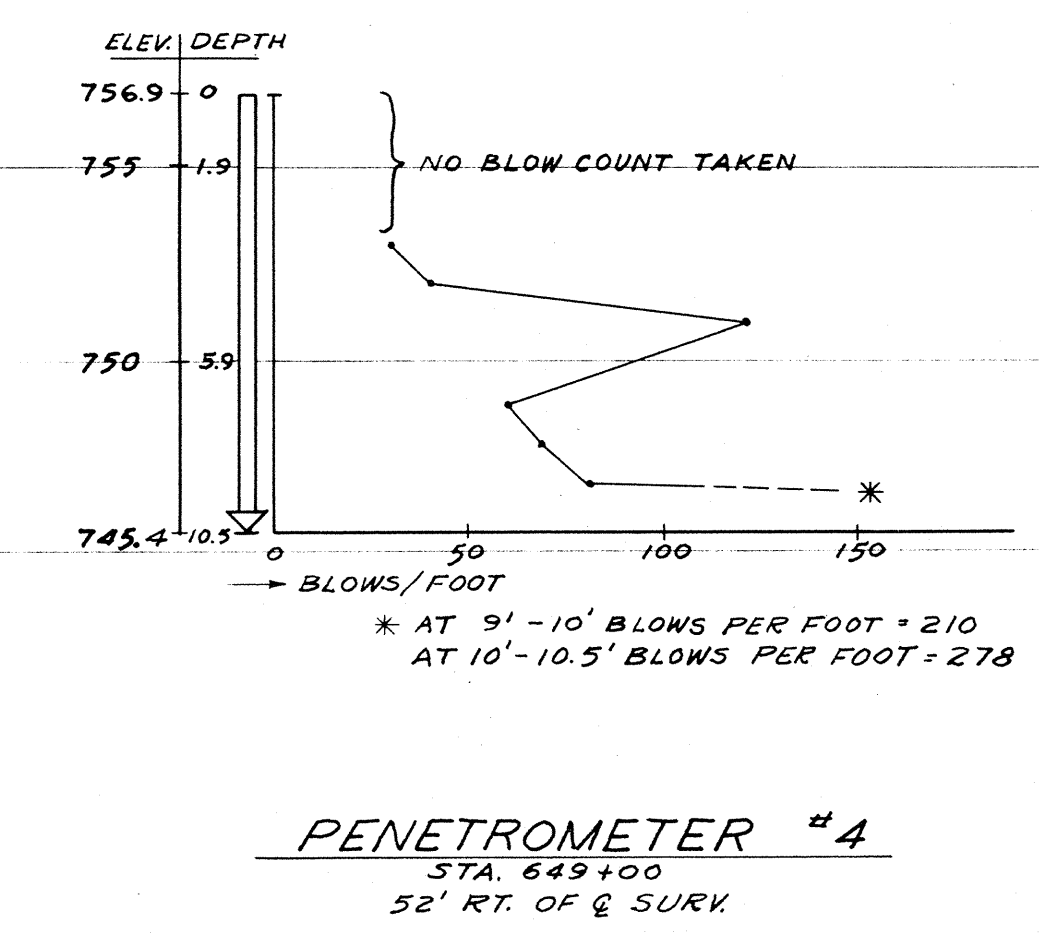
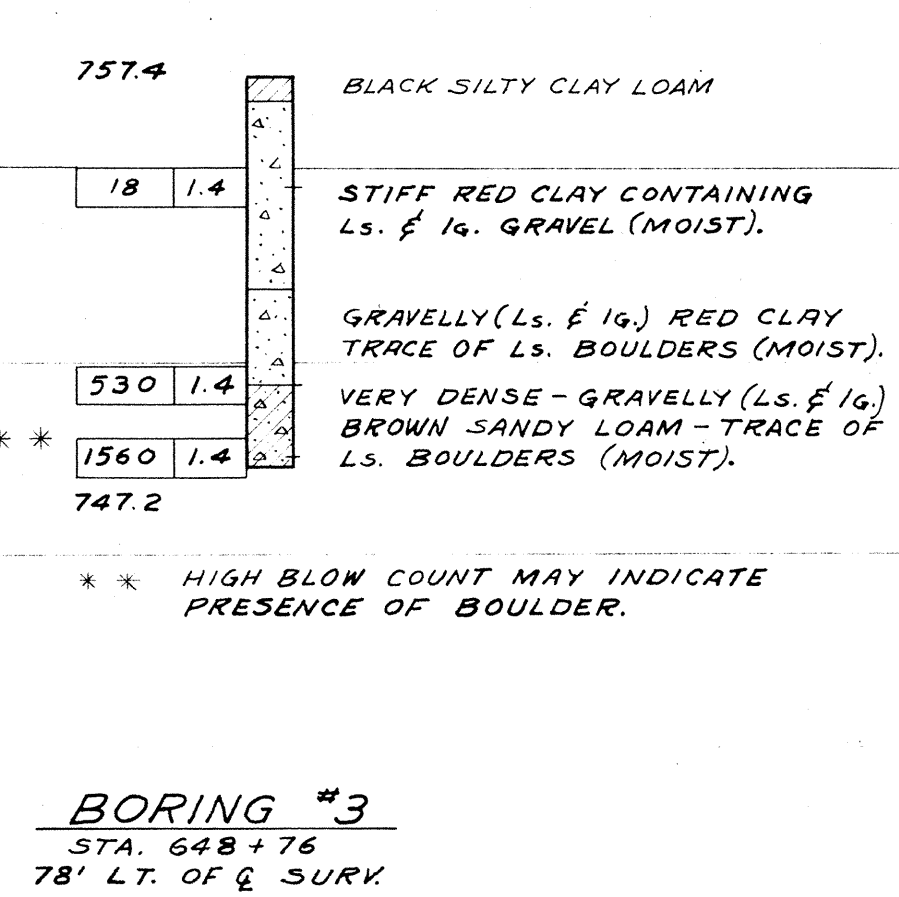
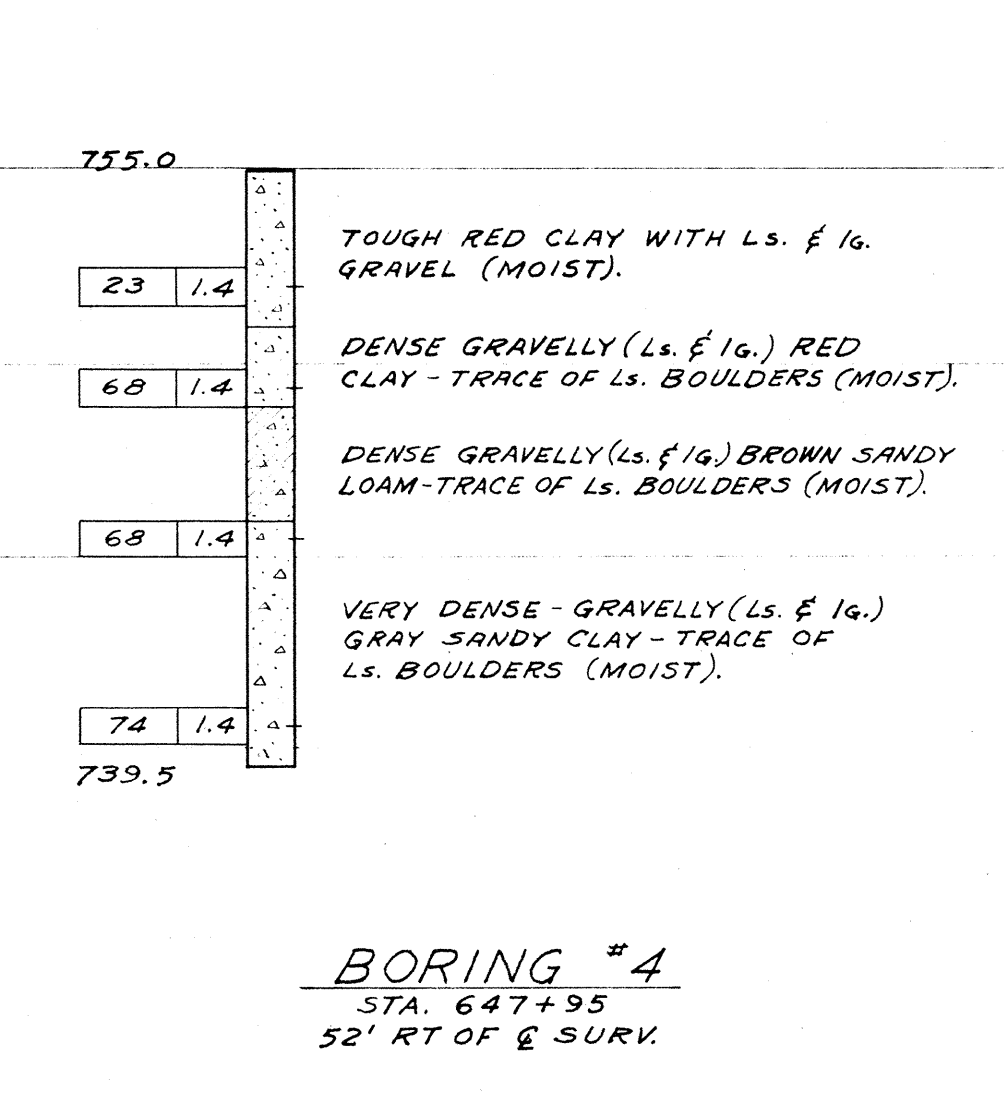
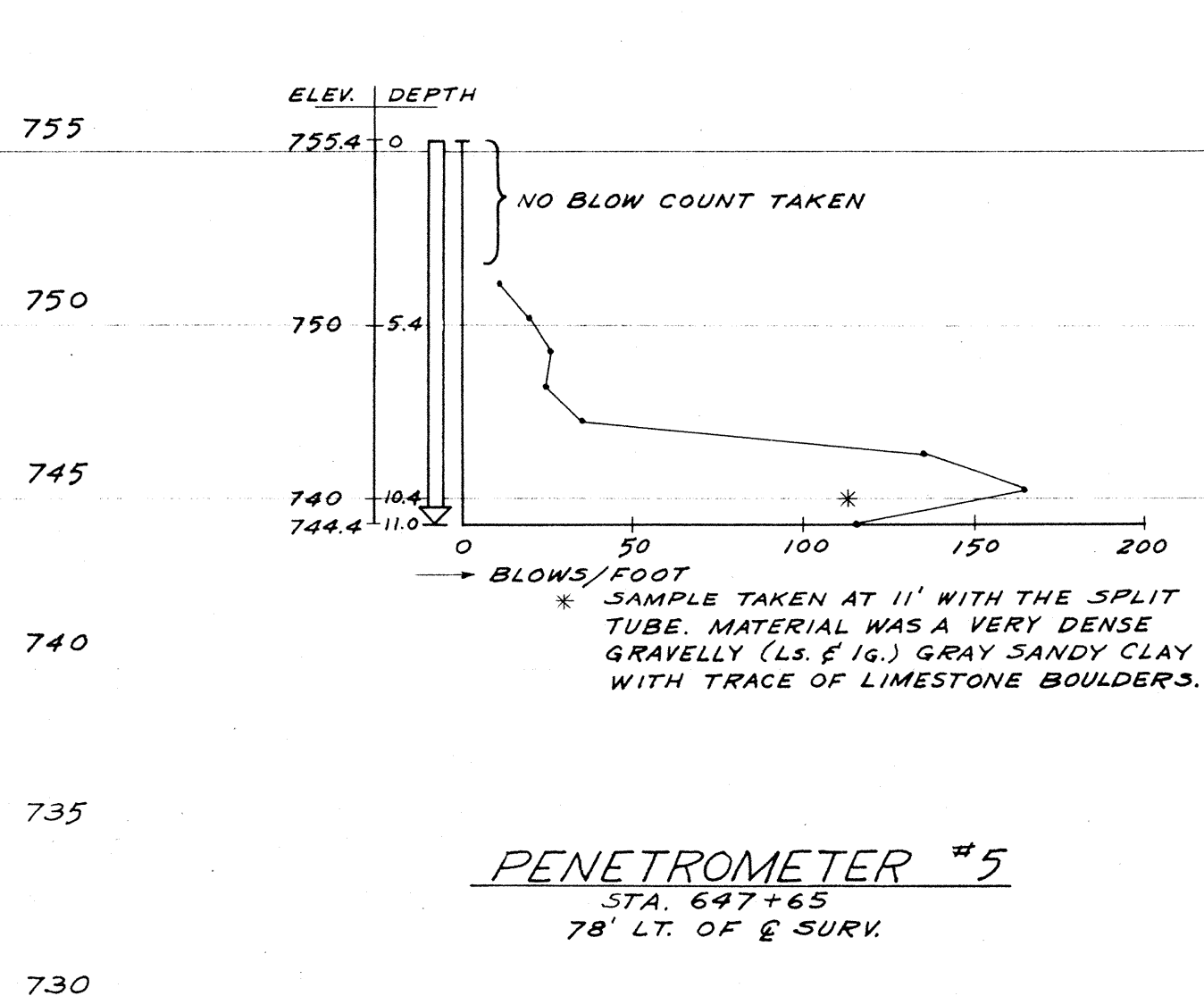
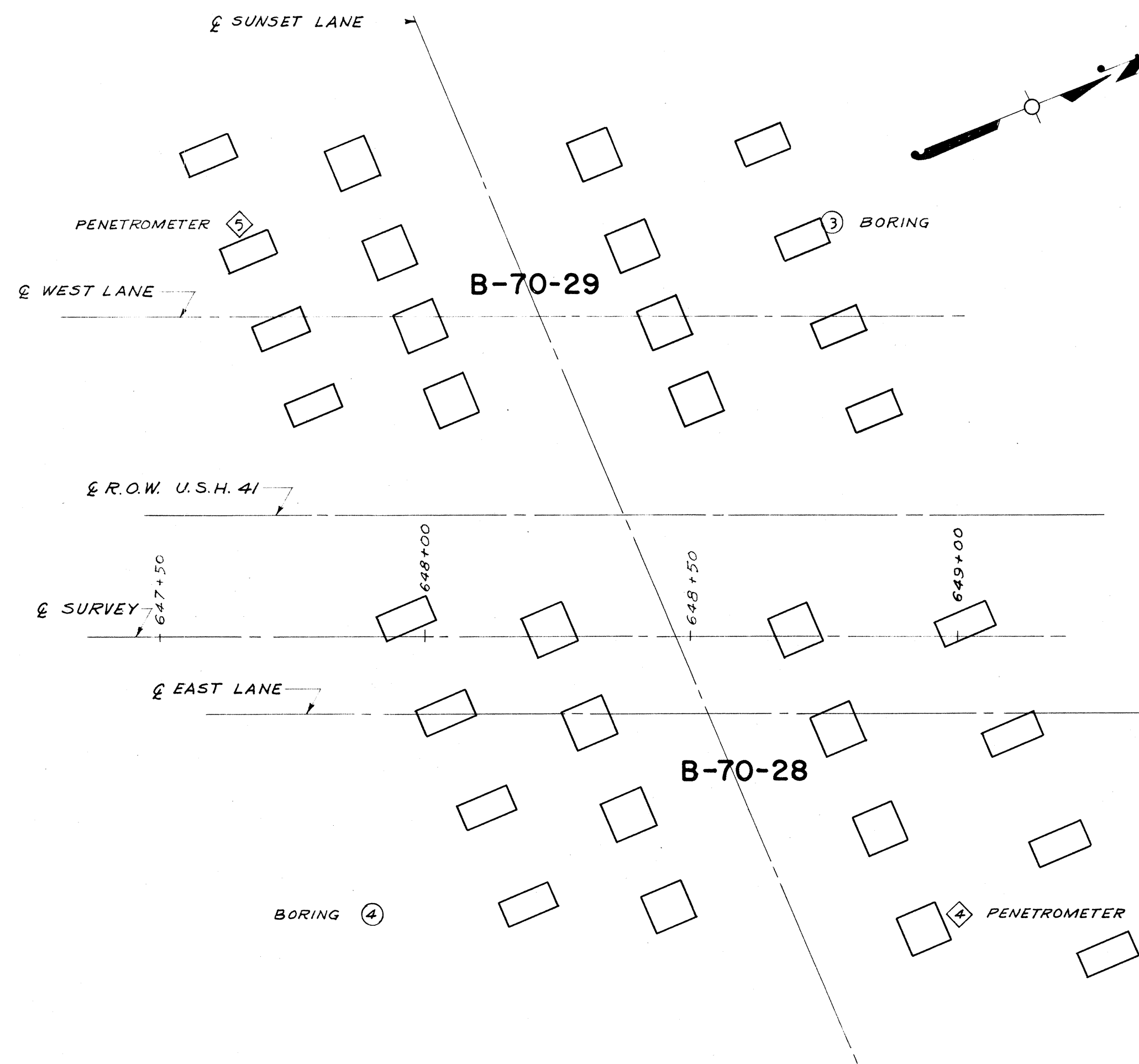
	A	B
B 8	2-11	2-2
B 9	2-9 1/2	2-2
B 10	2-8	2-2
B 11	2-6 1/2	2-2
B 12	2-5	2-2
B 13	2-3 1/2	2-2
B 14	2-2	2-2
B 15	2-0 1/2	2-2
B 16	1-11	2-2
B 17	1-9 1/2	2-2
B 18	1-8	2-2
B 25	3-8	1-8

REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN		
3-20-58	NORTH ABUTMENT		
3-4-66	DESIGN SPEC AASHO 53	LOADING AASHTO 516	CONST. SPEC. 1957
	DATE 9-23-57	DESIGN FCK	DRAWN DSJ CKD. NEB
STRUCTURE	B-70-28	SHEET	7 OF 8

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN

FOR THE DESIGN OF THE STRUCTURE FOUNDATION, TO OBTAIN RELATIVE DATA CONCERNING THE CHARACTER OF MATERIAL IN AND UPON WHICH THE FOUNDATION MIGHT BE BUILT, BORINGS AND/OR SOUNDINGS WERE MADE AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING WITH THE LOG OF SUCH EXPLORATION DATA AS INTERPRETED FOR SUCH DESIGN PURPOSE AS SHOWN. THE EXPLORATIONS WERE MADE BY ORDINARY AND CONVENTIONAL METHODS AND CARE DEEMED ADEQUATE FOR SUCH PURPOSE. HOWEVER, SINCE IT IS A MATTER OF COMMON KNOWLEDGE THAT THE EXACT CHARACTER OF ANY MATERIAL AND ITS REACTION IS DIFFICULT TO DETERMINE FROM SUCH SUBSURFACE EXPLORATION AND THAT THE KIND AND CHARACTER OF MATERIAL AT THE SITE WHERE THE FOUNDATIONS ARE BUILT MAY VARY SUBSTANTIALLY FROM THAT INDICATED BY THE LOG, THEY ARE MADE AVAILABLE TO THE BIDDERS SIMPLY FOR WHAT THEY ARE WORTH, WITHOUT ANY WARRANTY, EXPRESS OR IMPLIED, THAT THE MATERIAL TO BE ENCOUNTERED IN BUILDING THE FOUNDATION WILL CONFORM THEREWITH. IF THE LOG IS USED BY THE CONTRACTOR IN MAKING HIS BID, IT IS HEREBY EXPRESSLY STIPULATED THAT THE COMMISSION ACCEPTS NO RESPONSIBILITY FOR SAID USE.

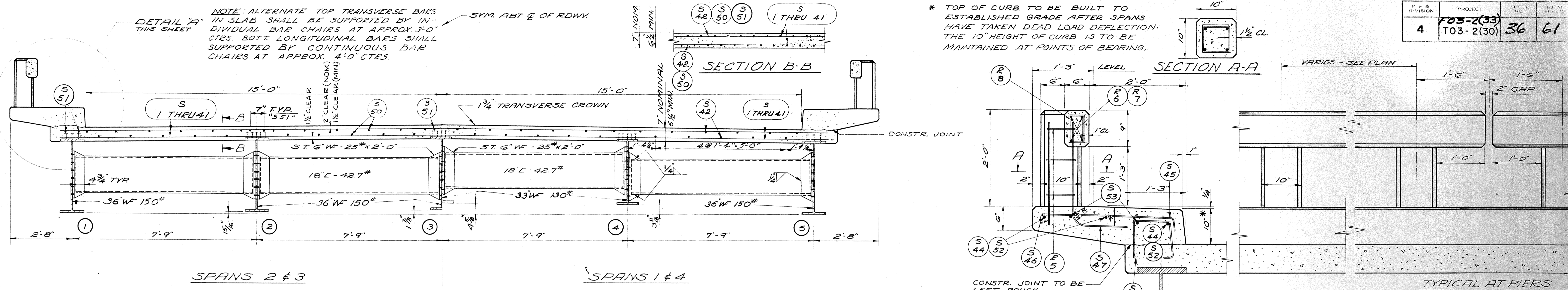
UNLESS OTHERWISE SPECIFIED THE BLOWS PER FOOT AT THE LOCATIONS INDICATED ARE BASED ON DRIVING A 2" O.D. x 1.4" I.D. SPLIT SPOON SAMPLER WITH A 140 LB. HAMMER HAVING A FREE FALL OF 30 INCHES. THE BLOW COUNT IS TAKEN IN UNDISTURBED SOIL IMMEDIATELY BELOW A CASED OR OPEN HOLE ELIMINATING SIDE FRICTION ON THE DRIVE PIPE.



REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN		
	LOG OF SOUNDINGS		
DESIGN SPEC	AASHO '53	LOADING H20-316	CONSTR. SPEC. 1957
DATE	9/23/57	DESIGN F.C.K.	DRAWN J.H.D. CKD. J.D.H.
STRUCTURE	B - 70 - 28/29		SHEET 8 OF 8

NOTE: ALTERNATE TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROX. 3'-0" CTRS. BOTT. LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROX. 4'-0" CTRS.

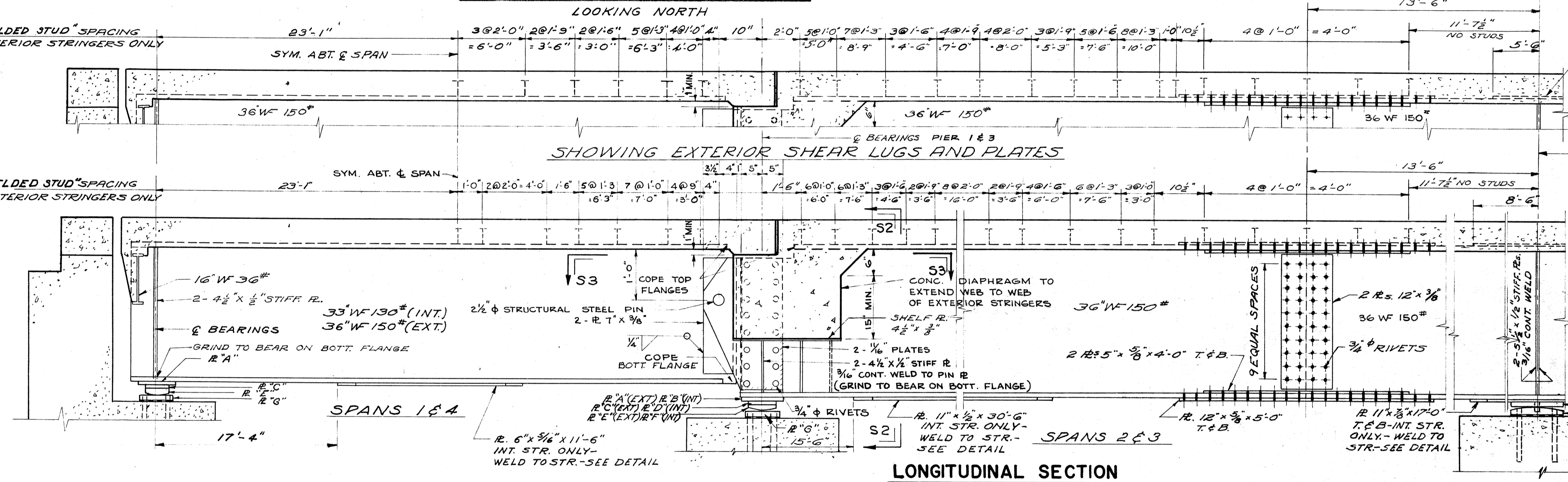
* TOP OF CURB TO BE BUILT TO ESTABLISHED GRADE AFTER SPANS HAVE TAKEN DEAD LOAD DEFLECTION. THE 10" HEIGHT OF CURB IS TO BE MAINTAINED AT POINTS OF BEARING.



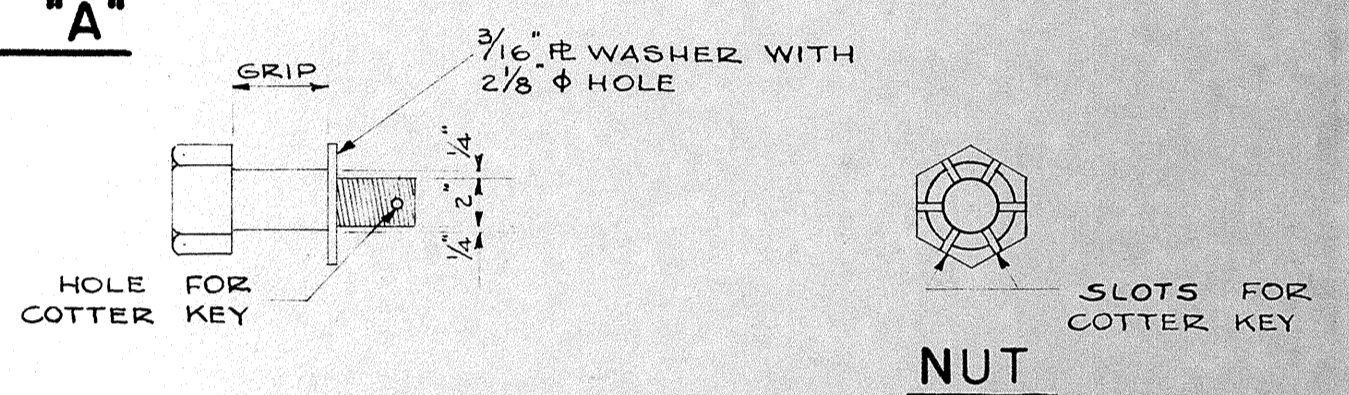
CROSS SECTION THRU ROADWAY

"WELDED STUD" SPACING EXTERIOR STRINGERS ONLY

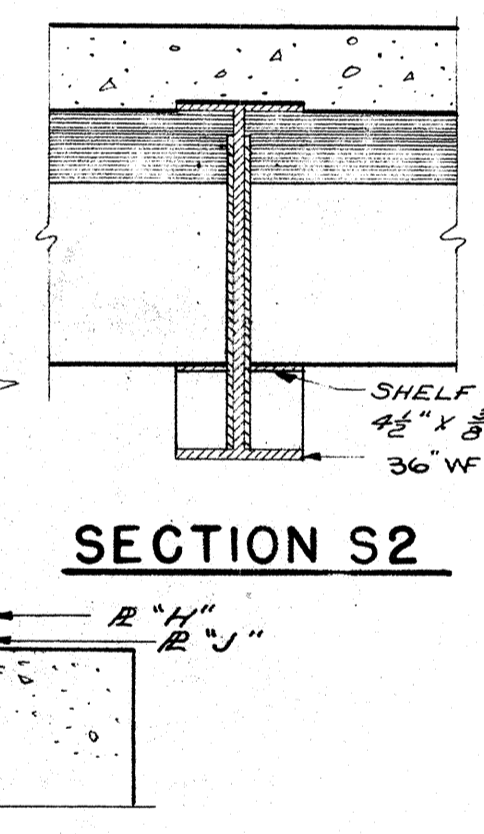
"WELDED STUD" SPACING INTERIOR STRINGERS ONLY



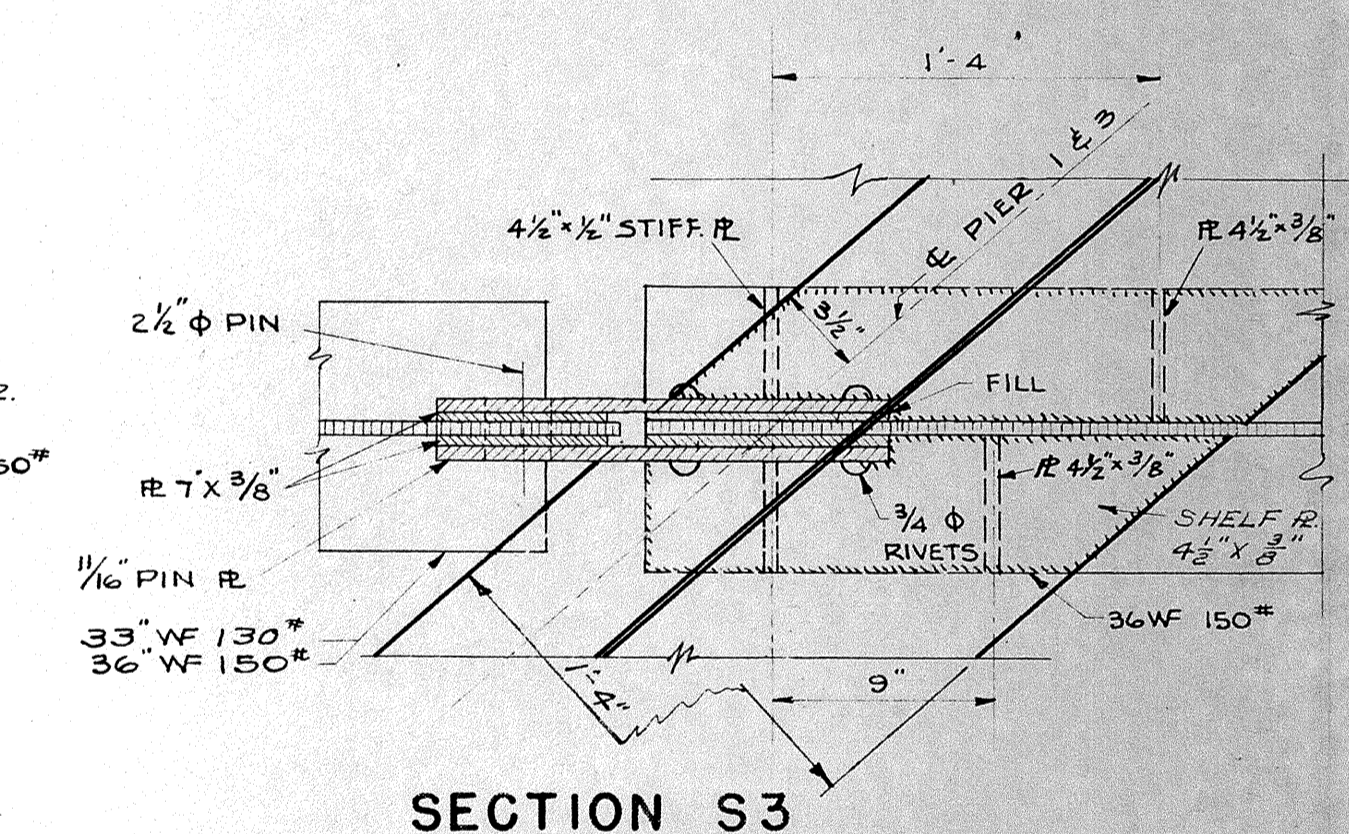
LONGITUDINAL SECTION



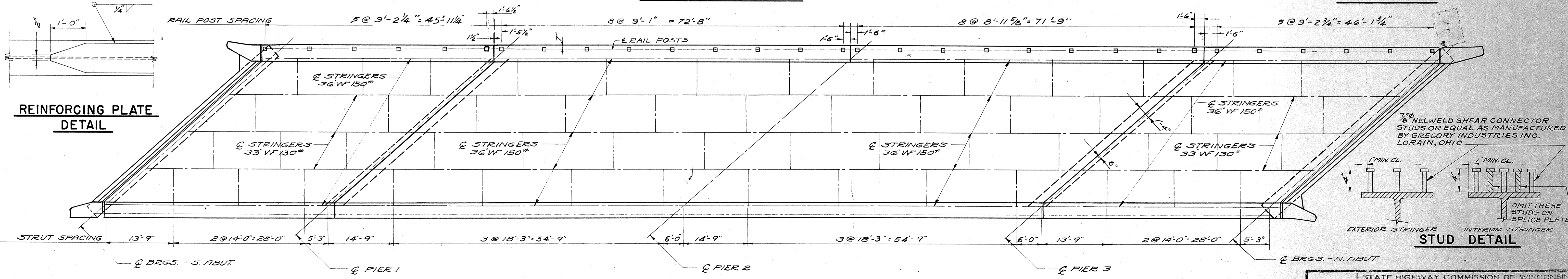
STRUCTURAL STEEL PIN DETAIL



SECTION S2



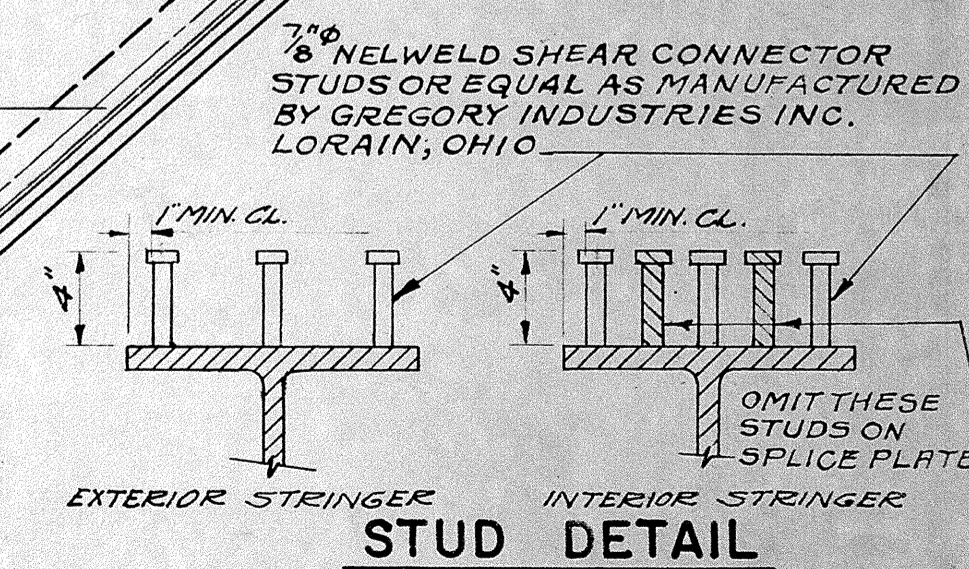
SECTION S3



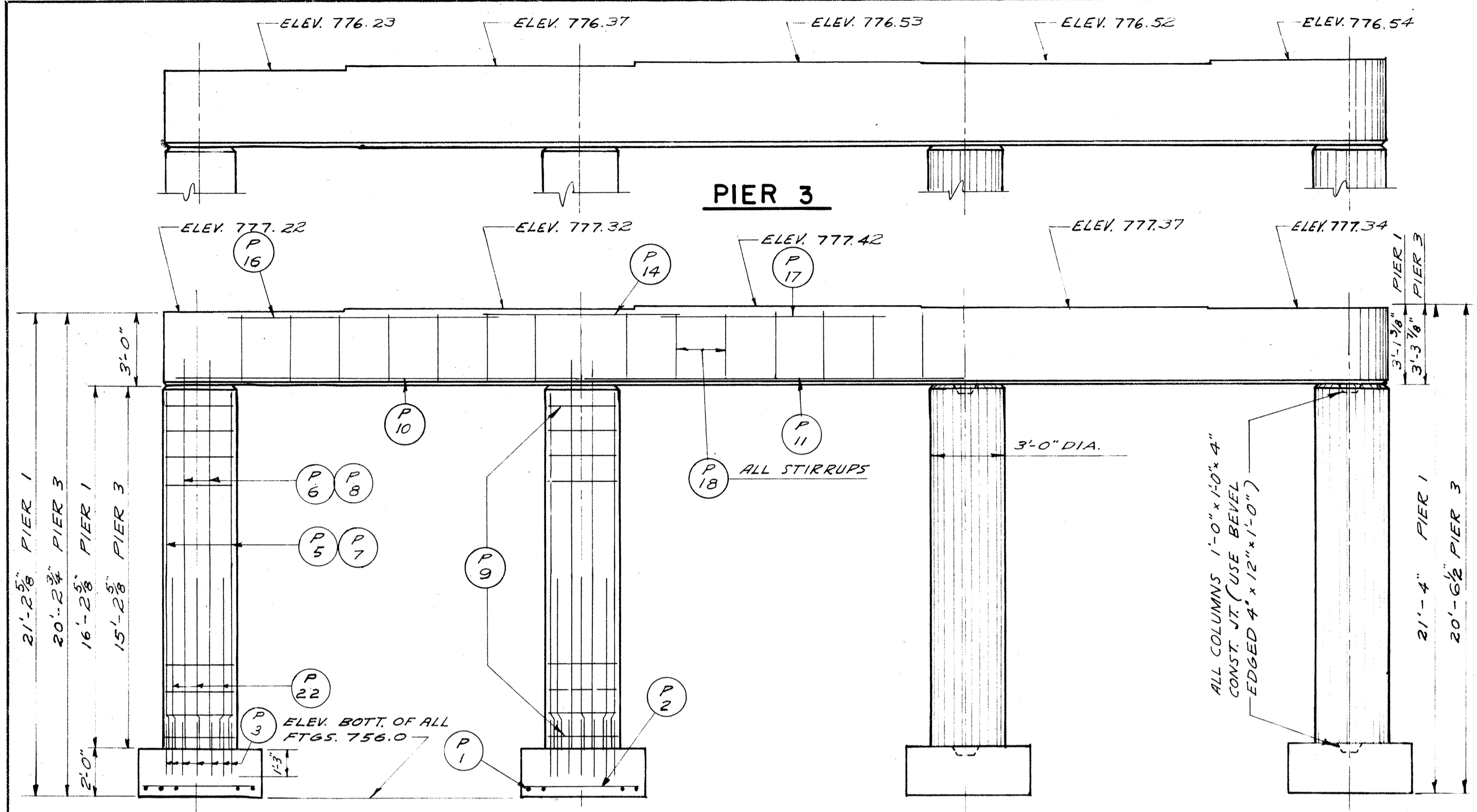
PLAN

SHOWING STRUCTURAL STEEL & RAIL POST SPACING

REINFORCING PLATE DETAIL

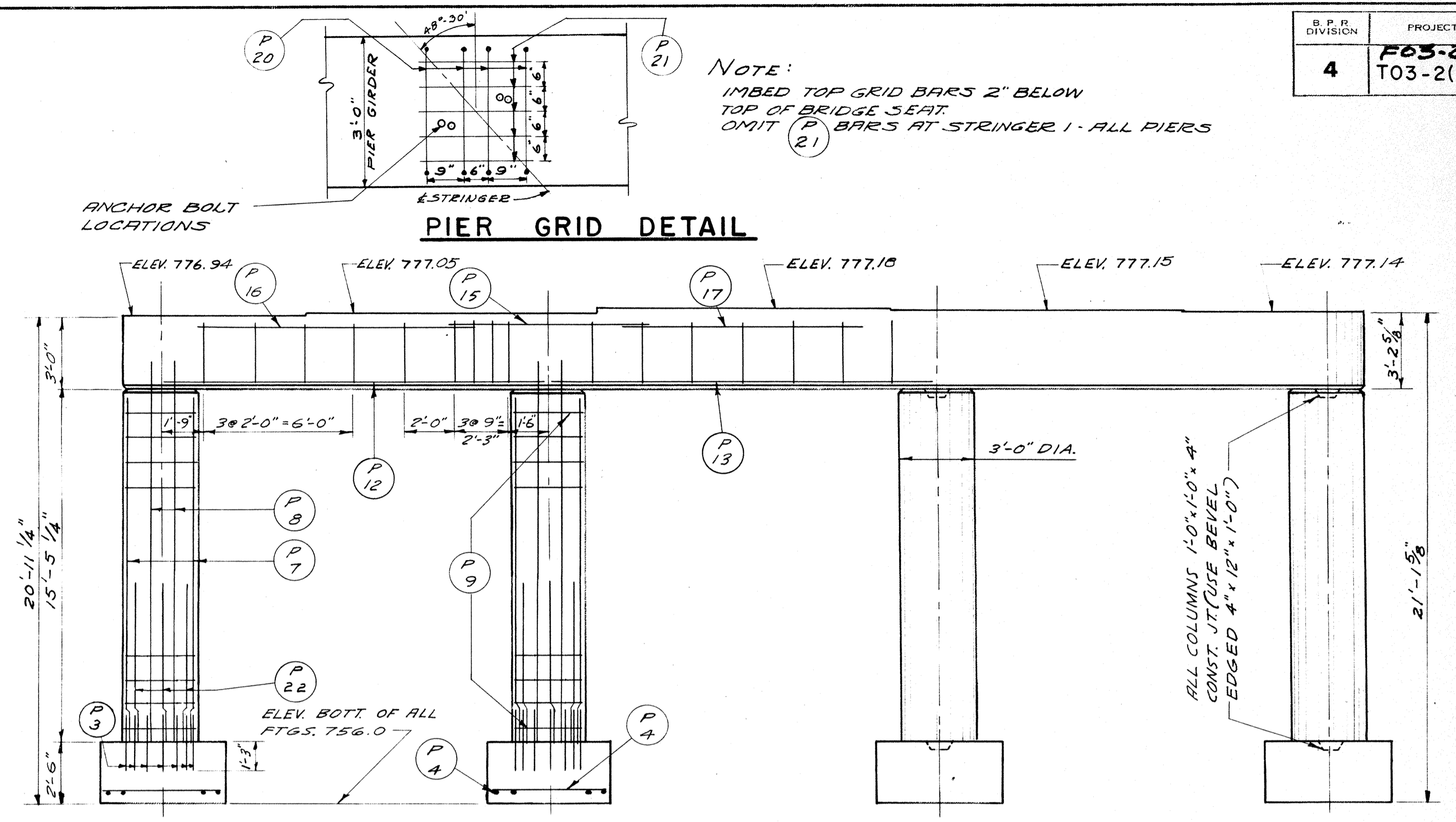


STUD DETAIL



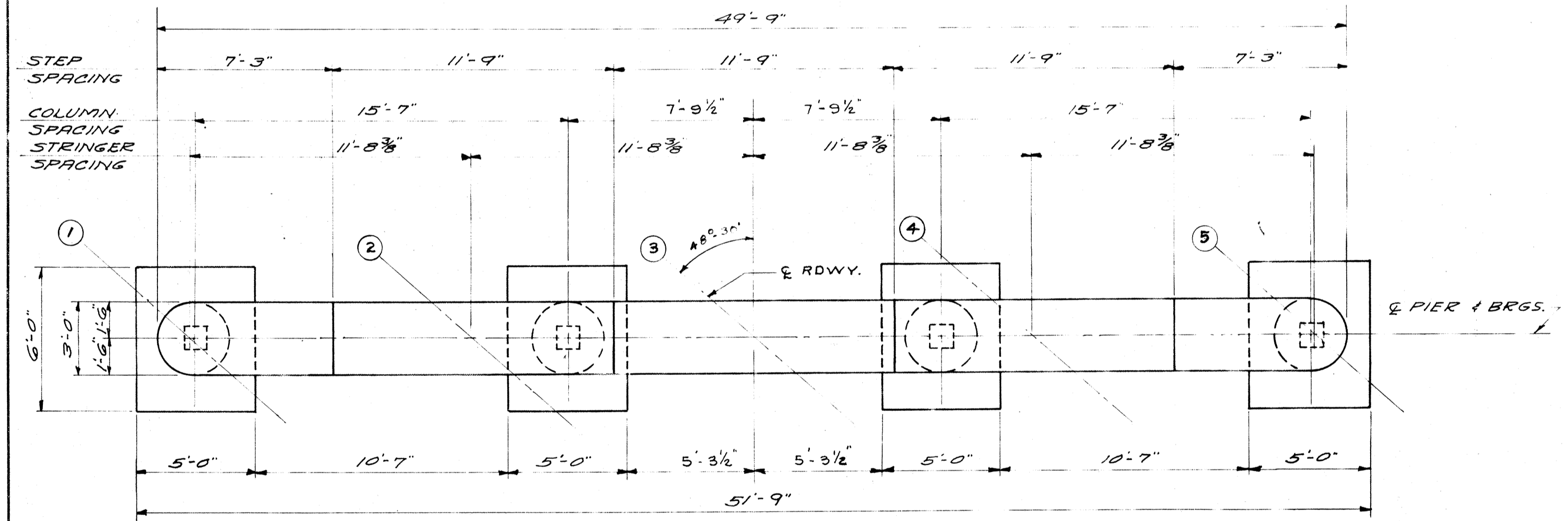
SHOWING COLUMN & FOOTING STEEL
ELEVATION - PIERS 1 & 3
LOOKING NORTH

END VIEW

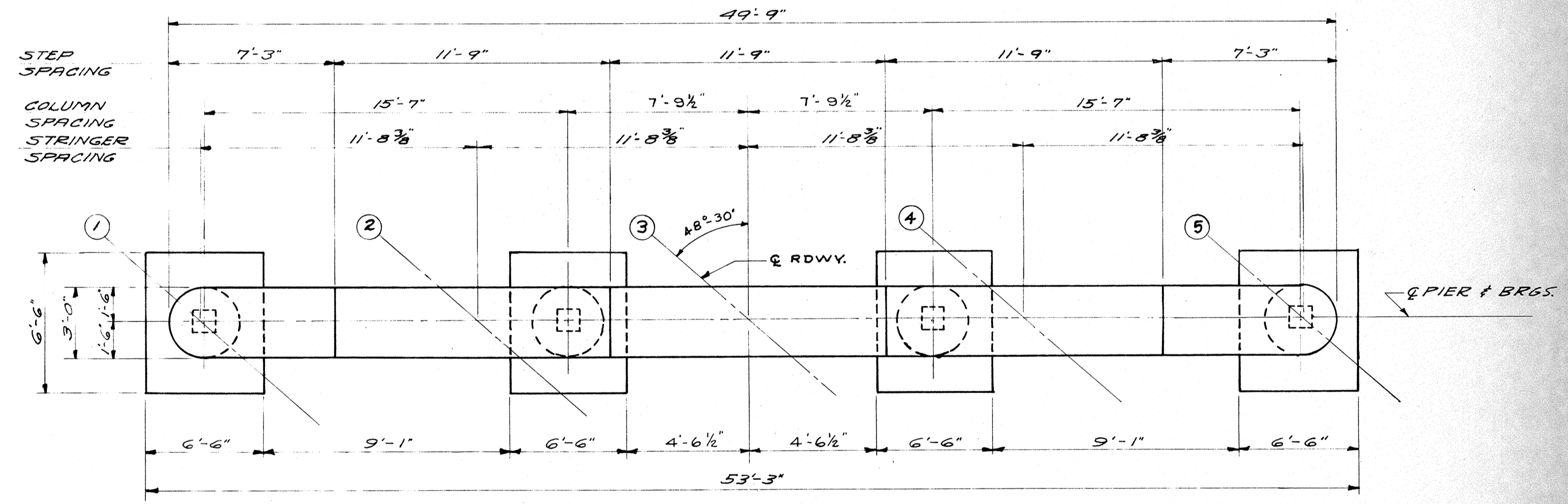


SHOWING COLUMN & FOOTING STEEL
ELEVATION - PIER 2
LOOKING NORTH

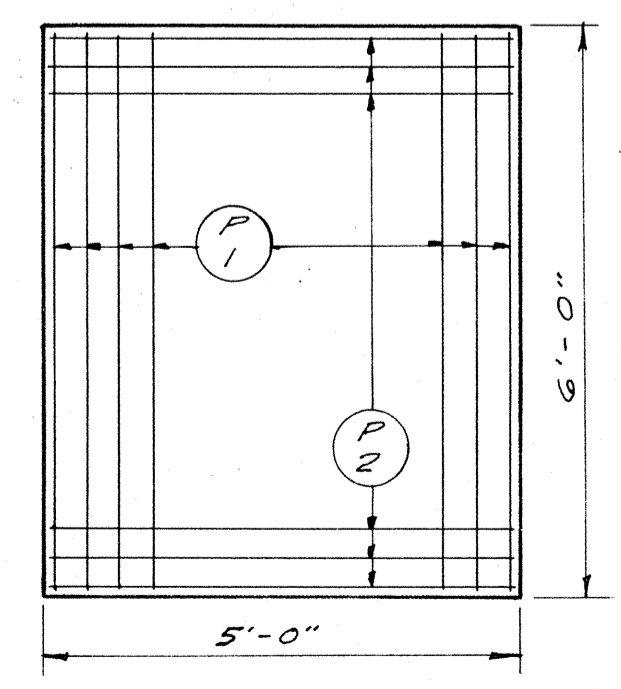
NOTE: SOIL BEARING PRESSURE = 4.4 TONS PER SQ. FT.



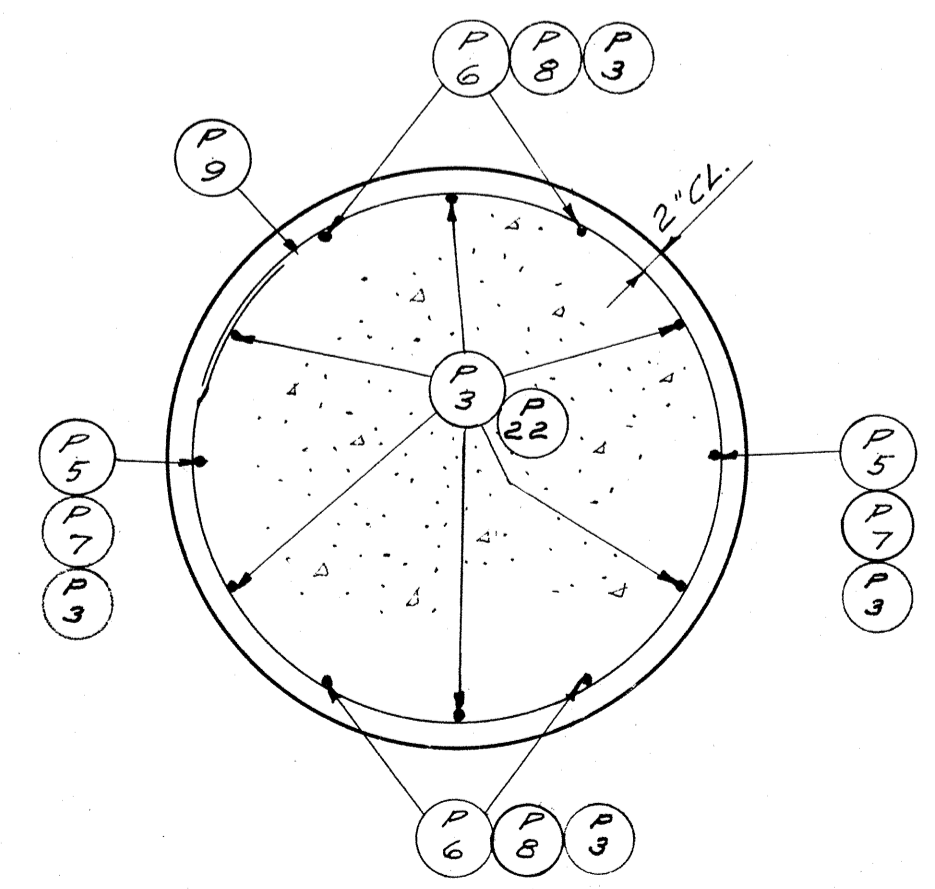
PLAN



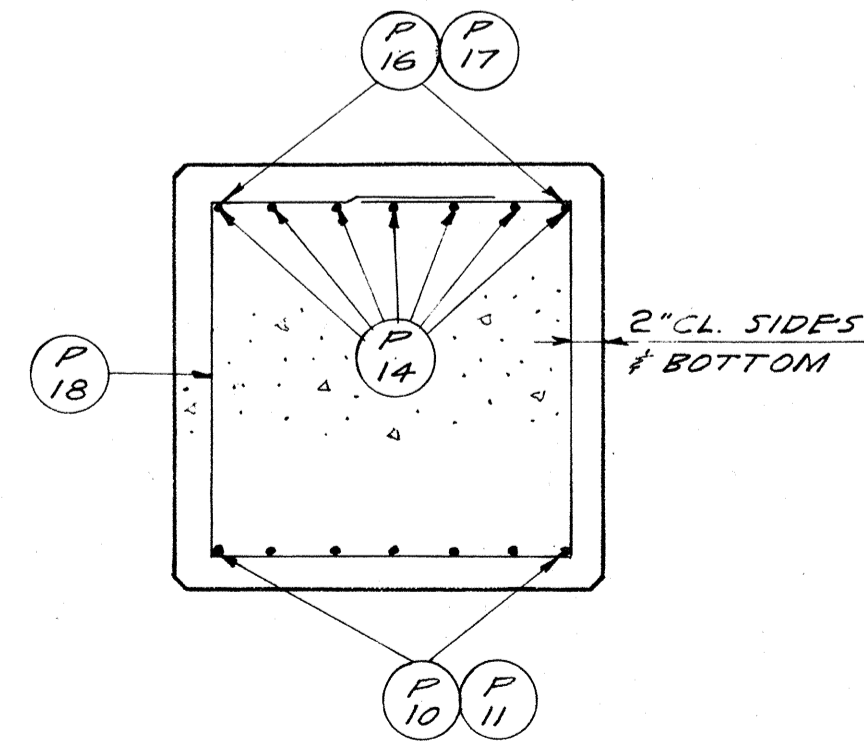
PLAN



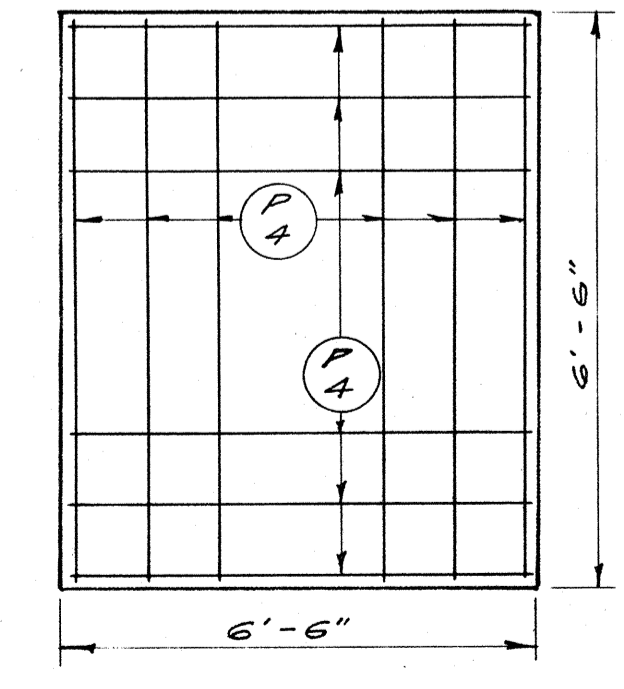
FOOTING PLAN
PIERS 1 & 3



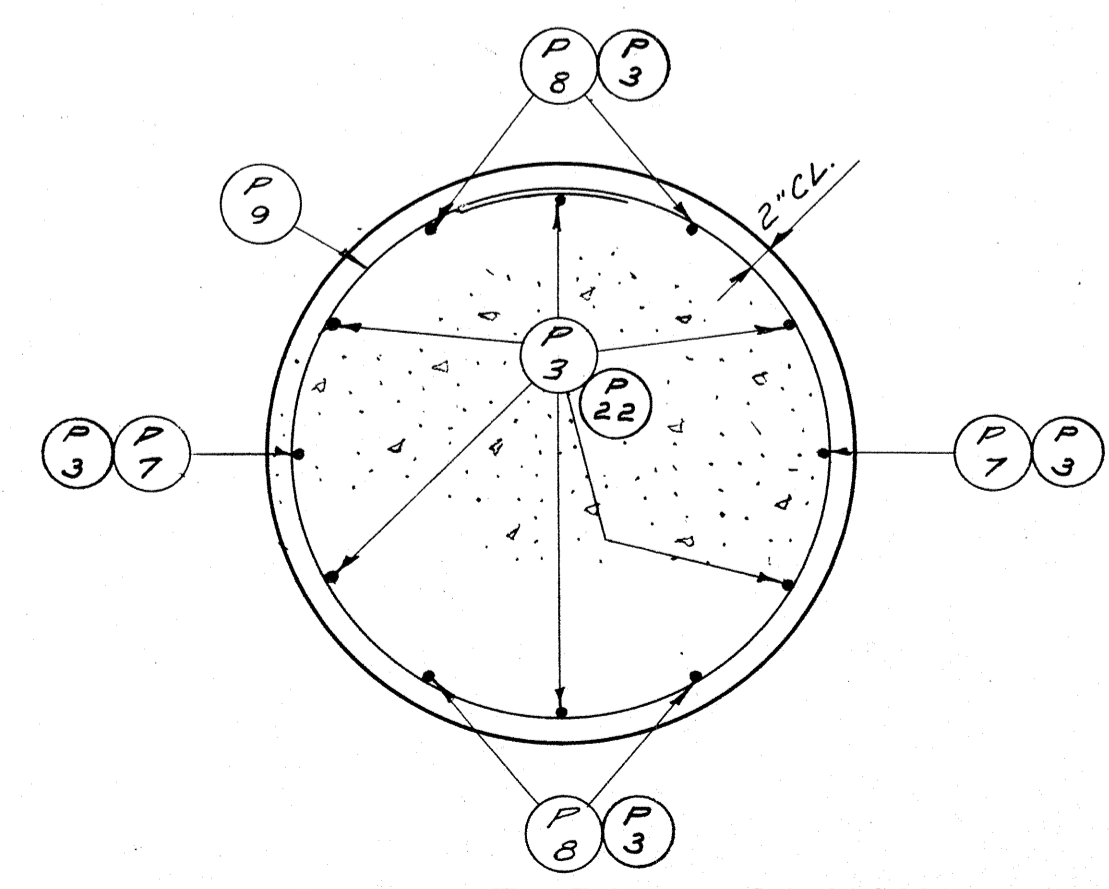
SECTION THRU COLUMN
PIERS 1 & 3



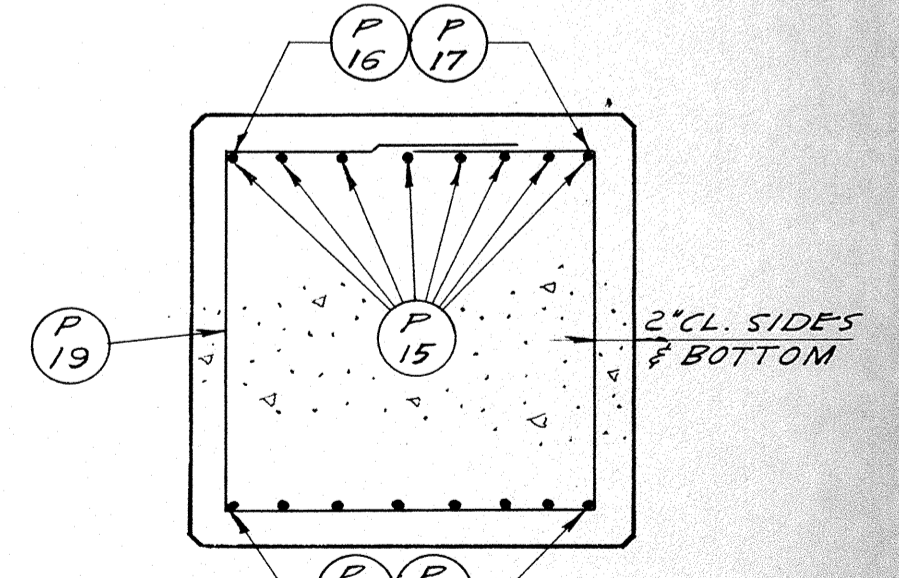
SECTION THRU GIRDER
PIERS 1 & 3



FOOTING PLAN
PIER 2

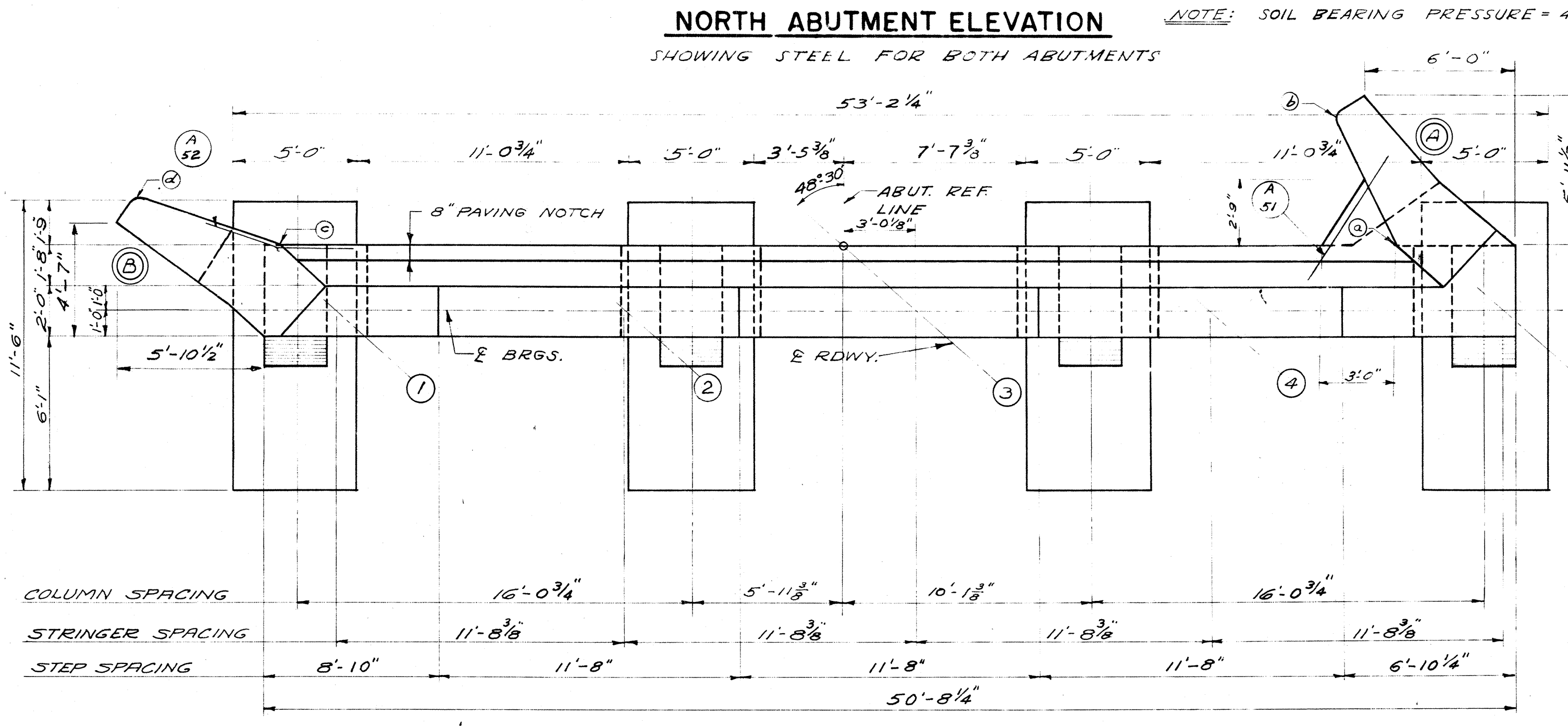
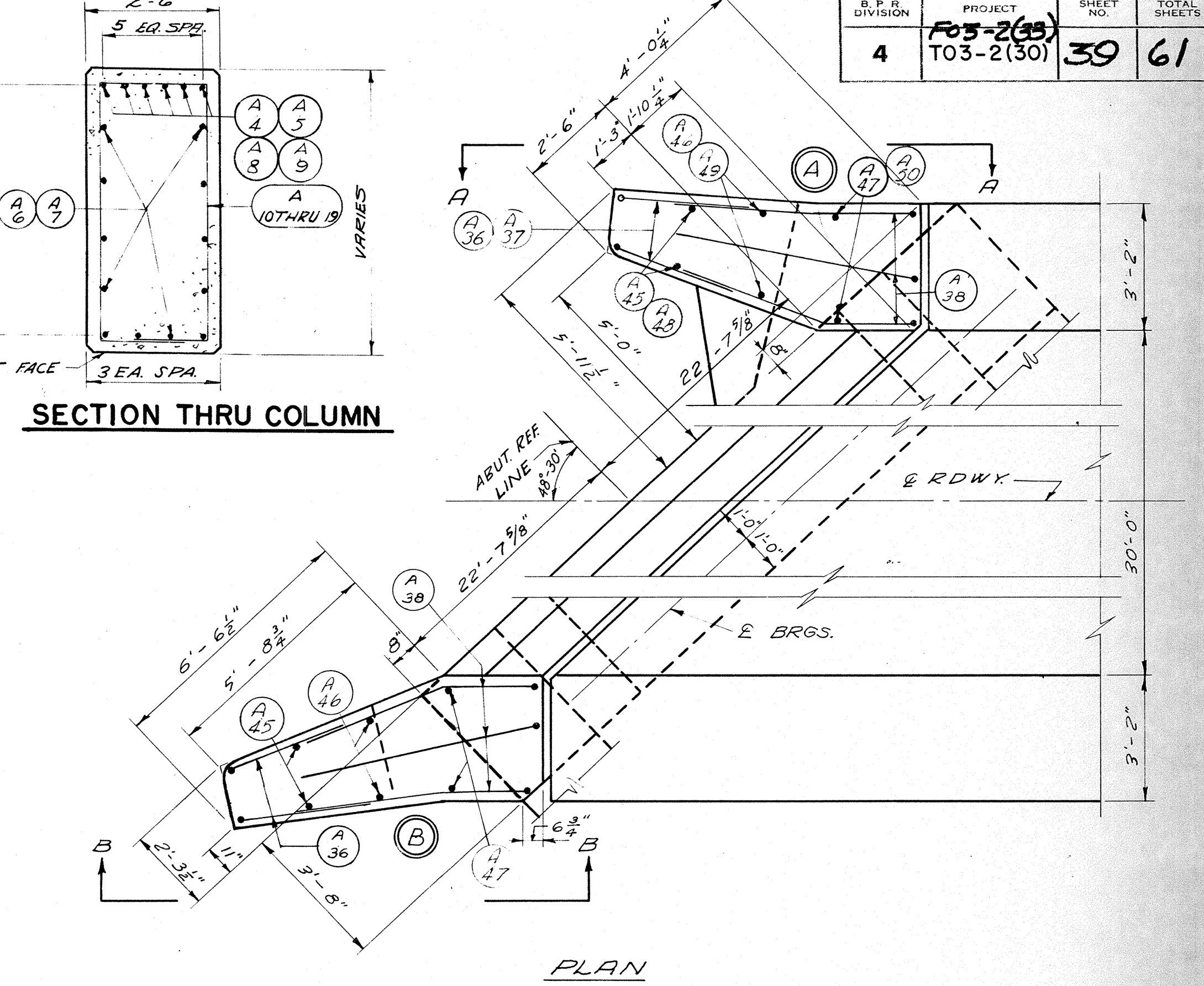
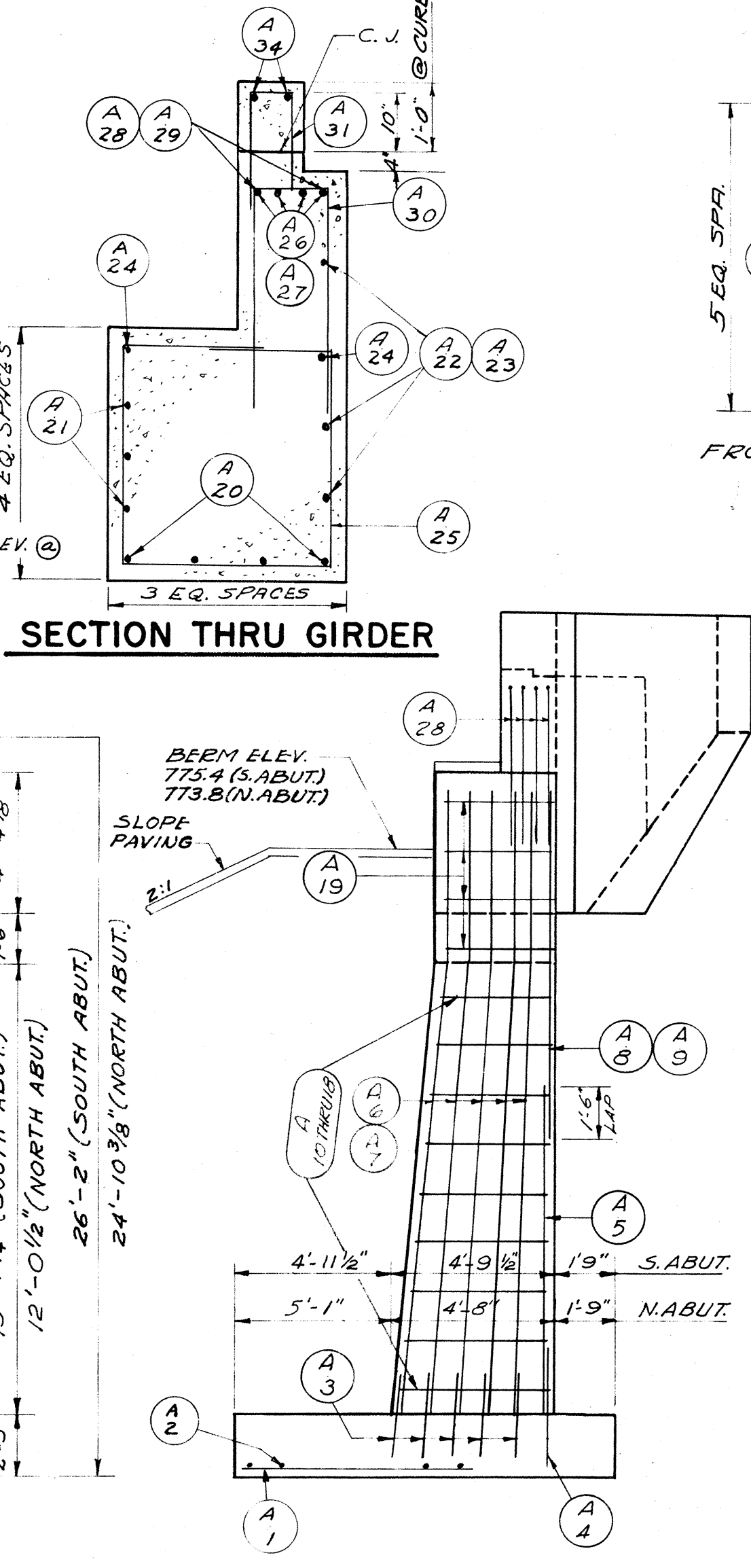
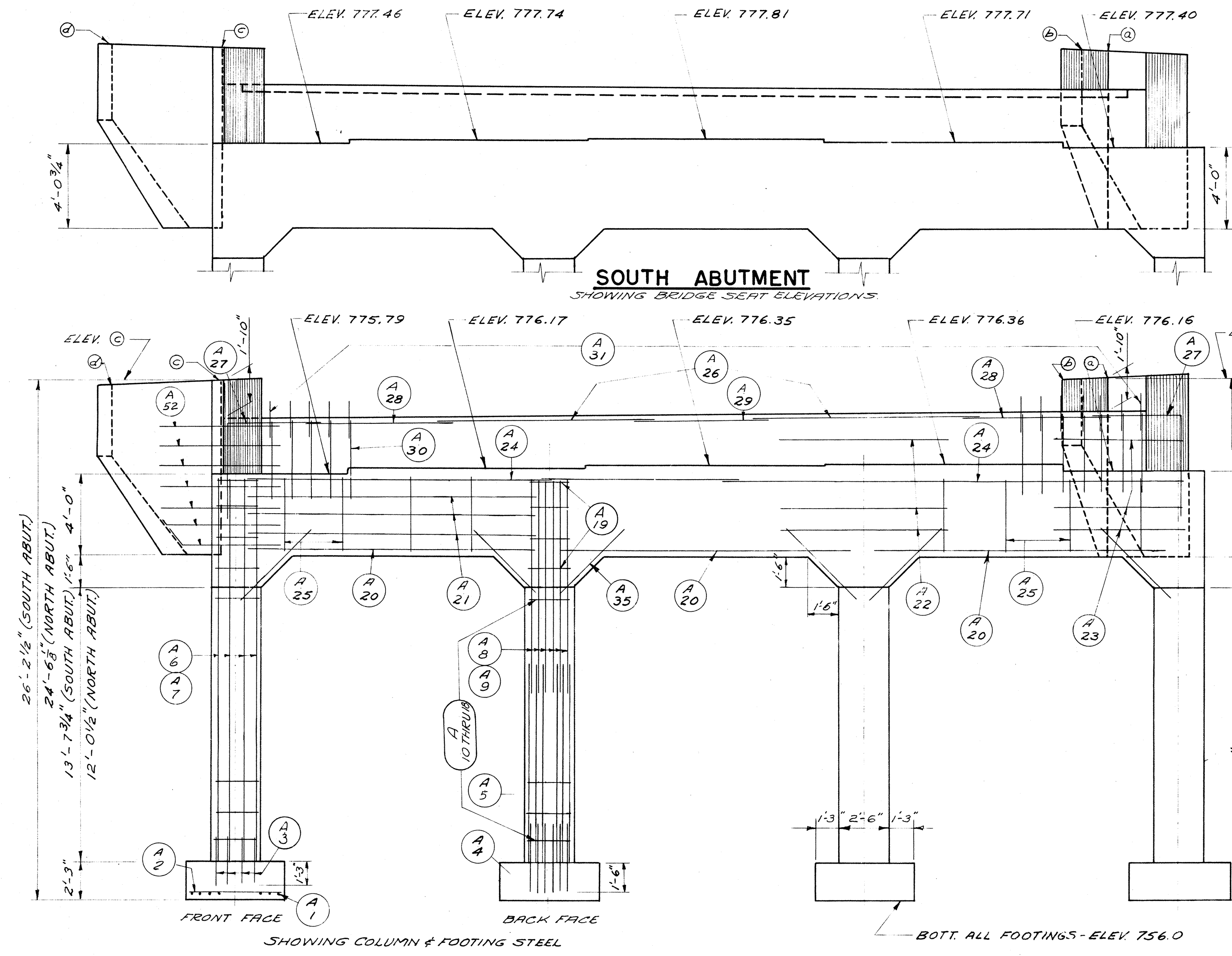


SECTION THRU COLUMN
PIER 2



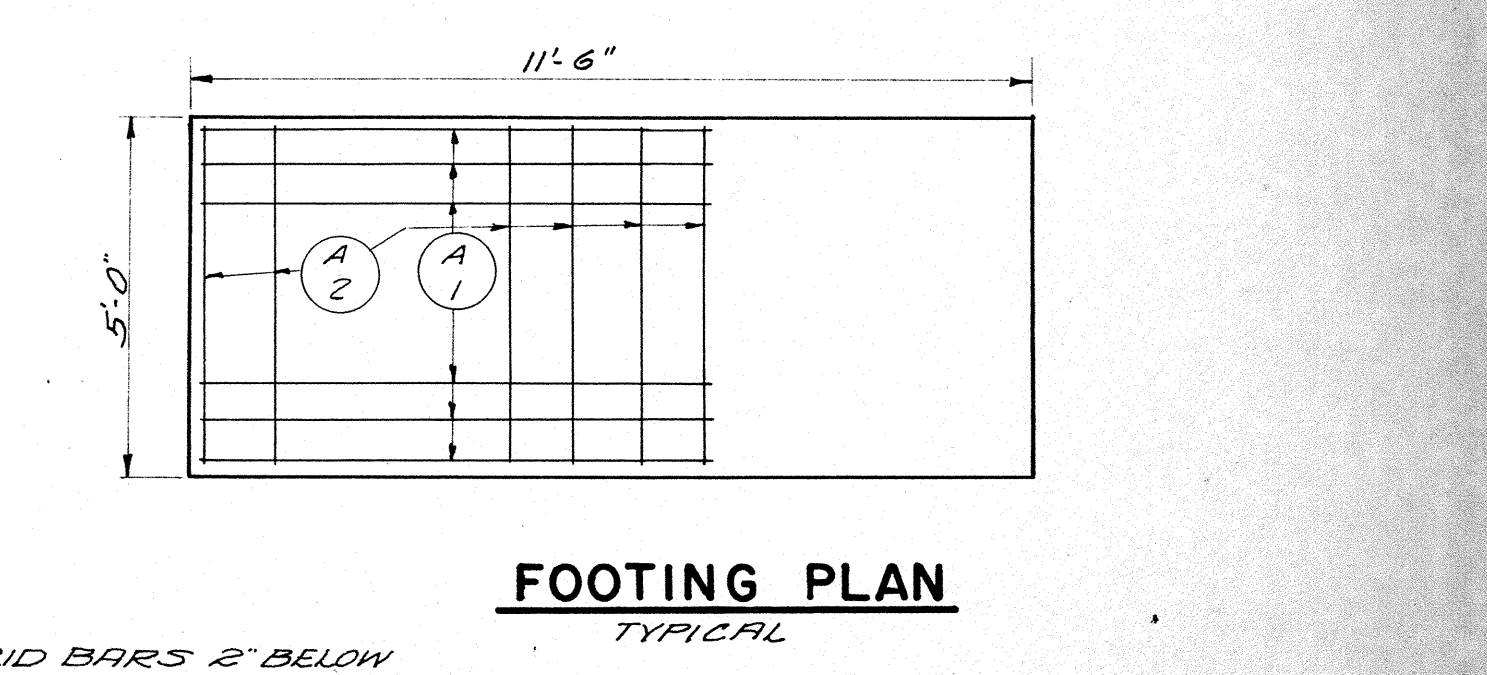
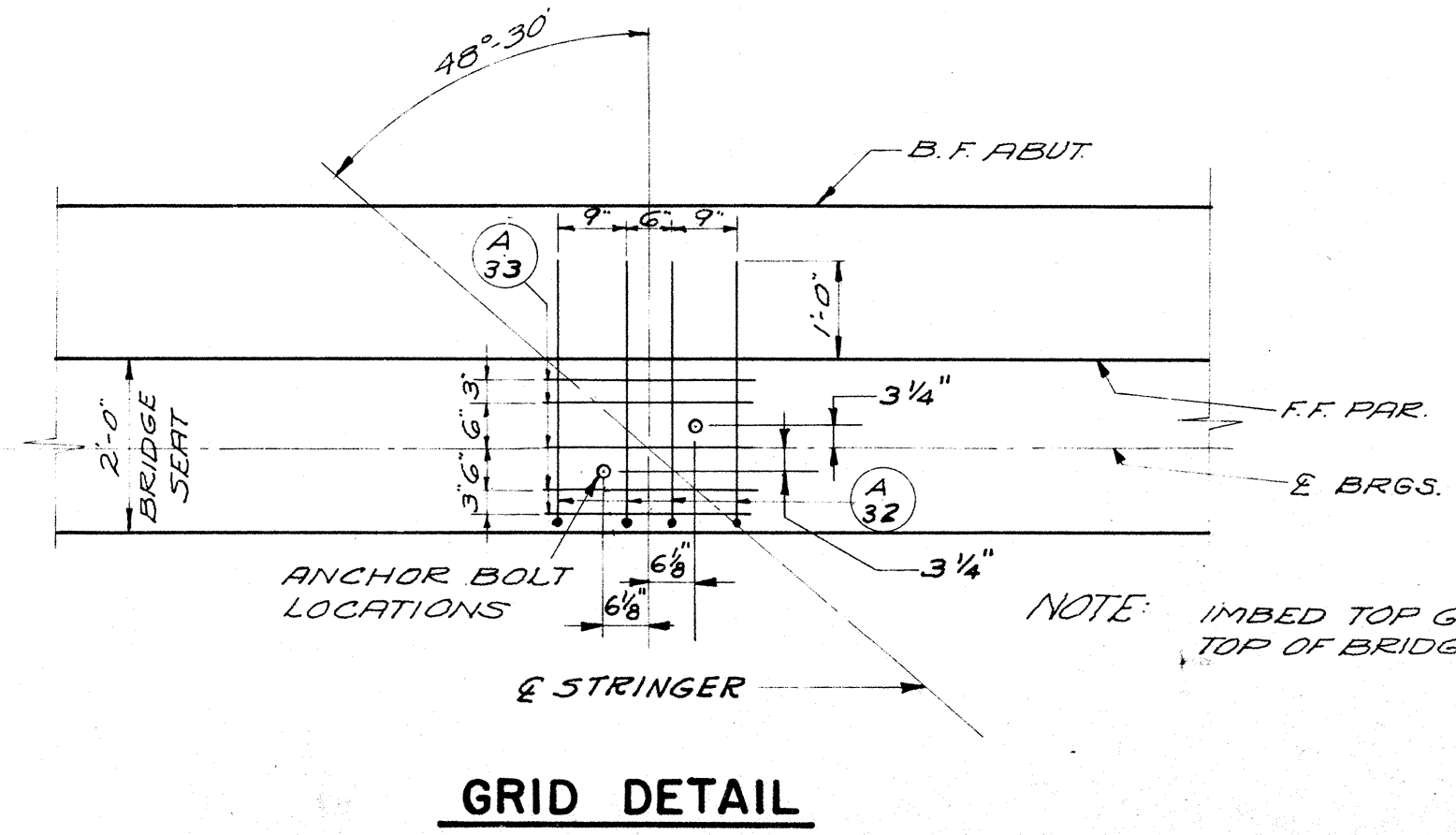
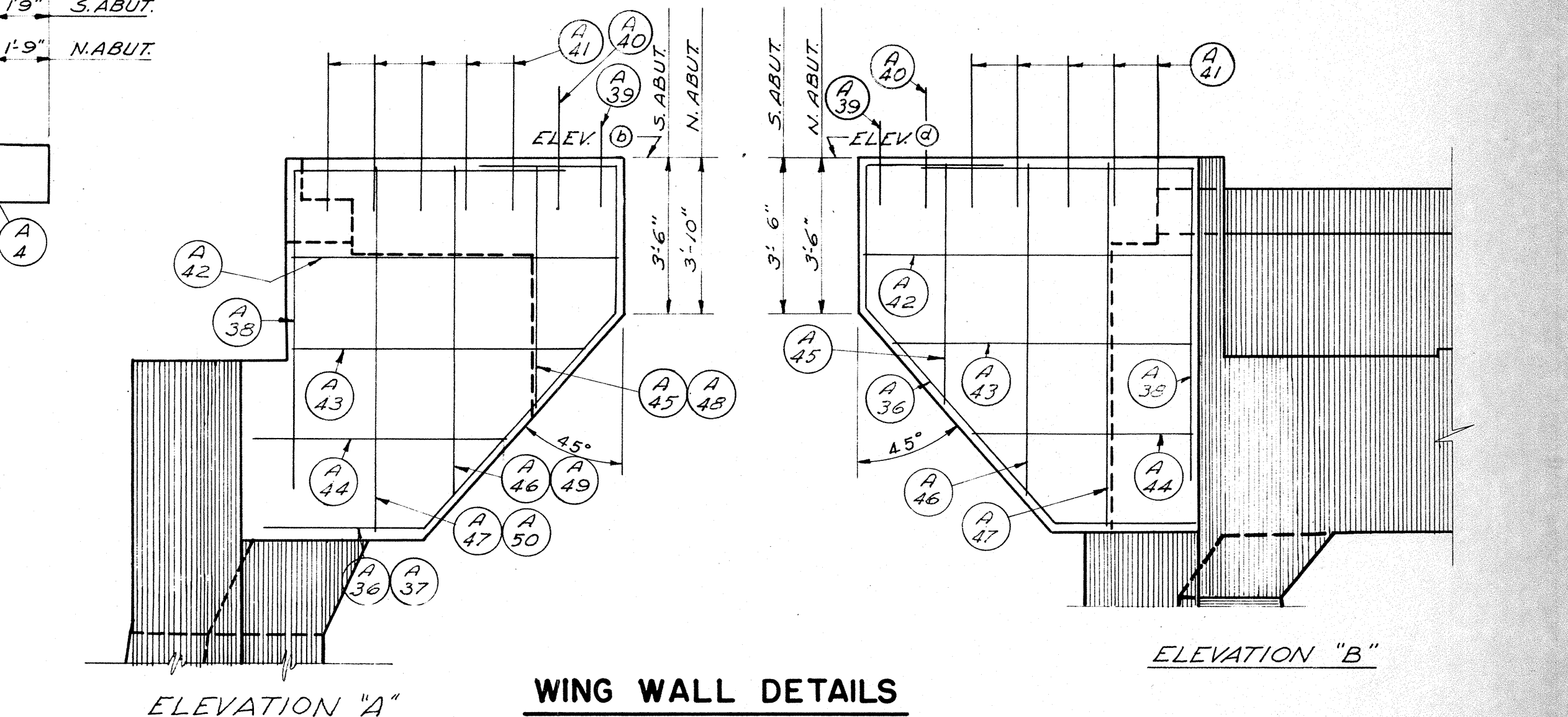
SECTION THRU GIRDER
PIER 2

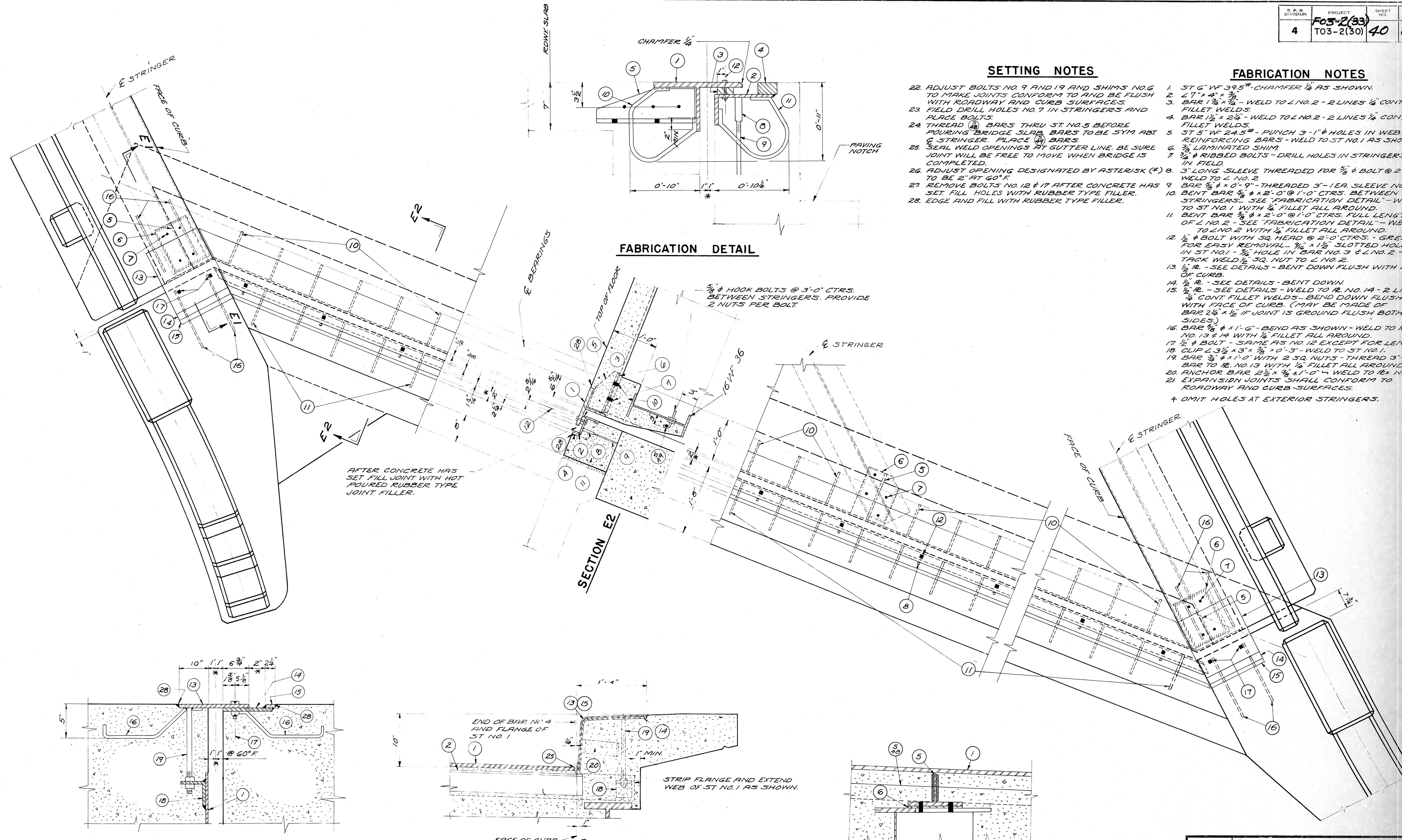
REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN		
	PIERS		
DATE: 9-23-57	DESIGN: JMT	CHKD: HB	SPEC: 1957 LOAD H20-516
STRUCTURE: B-70-26	SHEET 4 OF 8		



WING WALL ELEVATIONS

POINT	S. ABUT.	N. ABUT.
Ⓐ	782.17	780.86
Ⓑ	782.19	780.83
Ⓒ	782.21	780.51
Ⓓ	782.24	780.46





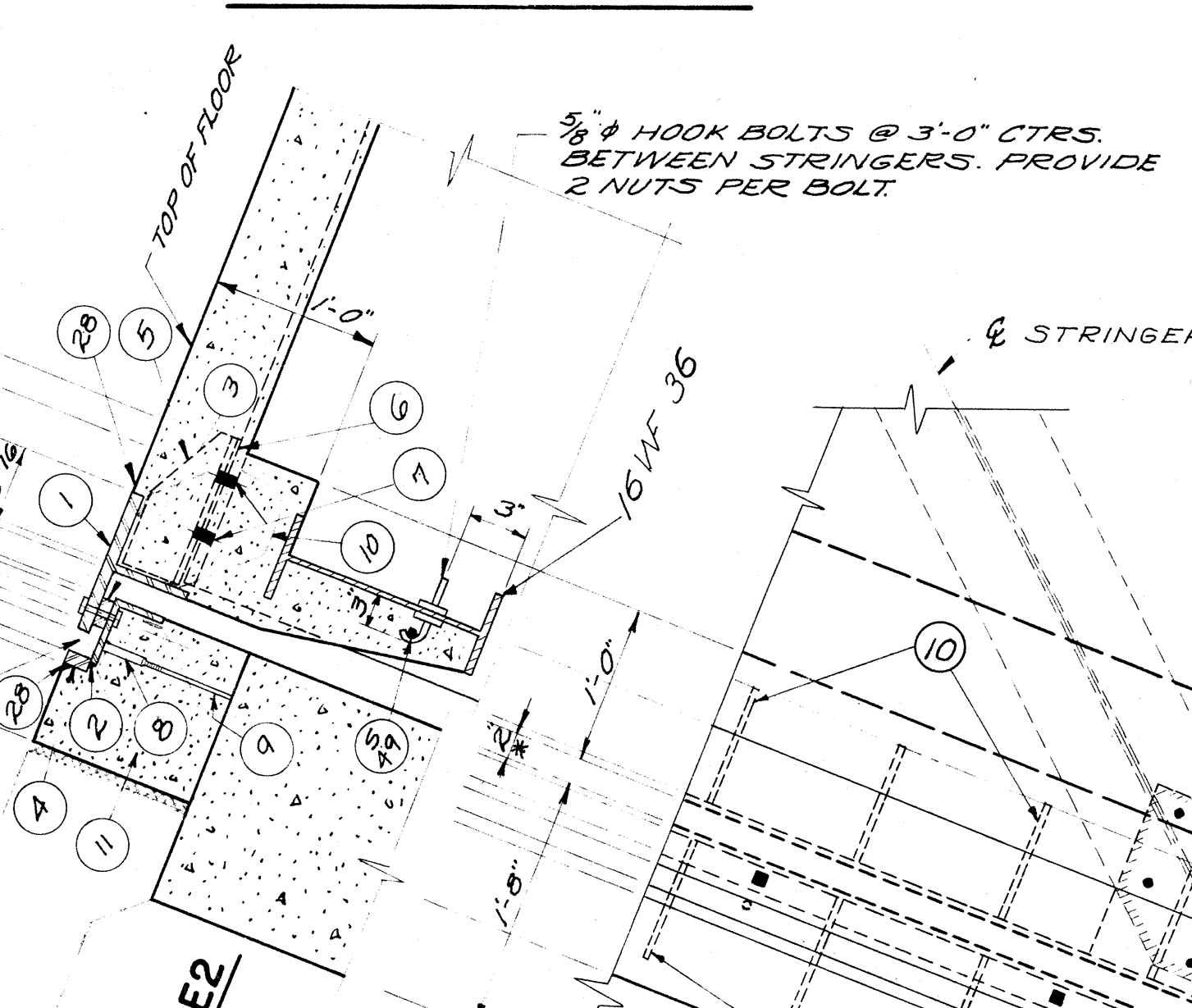
SETTING NOTES

22. ADJUST BOLTS NO. 9 AND 19 AND SHIMS NO. 6 TO MAKE JOINTS CONFORM TO AND BE FLUSH WITH ROADWAY AND CURB SURFACES.
23. FIELD DRILL HOLES NO. 7 IN STRINGERS AND PLACE BOLTS.
24. THREAD BARS THRU ST. NO. 5 BEFORE POURING BRIDGE SLAB. BARS TO BE SYM. ABT. C. STRINGER. PLACE BARS.
25. SEAL WELD OPENINGS AT GUTTER LINE. BE SURE JOINT WILL BE FREE TO MOVE WHEN BRIDGE IS COMPLETED.
26. ADJUST OPENING DESIGNATED BY ASTERISK (*) TO BE 2" AT 60°F.
27. REMOVE BOLTS NO. 12 & 17 AFTER CONCRETE HAS SET. FILL HOLES WITH RUBBER TYPE FILLER.
28. EDGE AND FILL WITH RUBBER TYPE FILLER.

FABRICATION NOTES

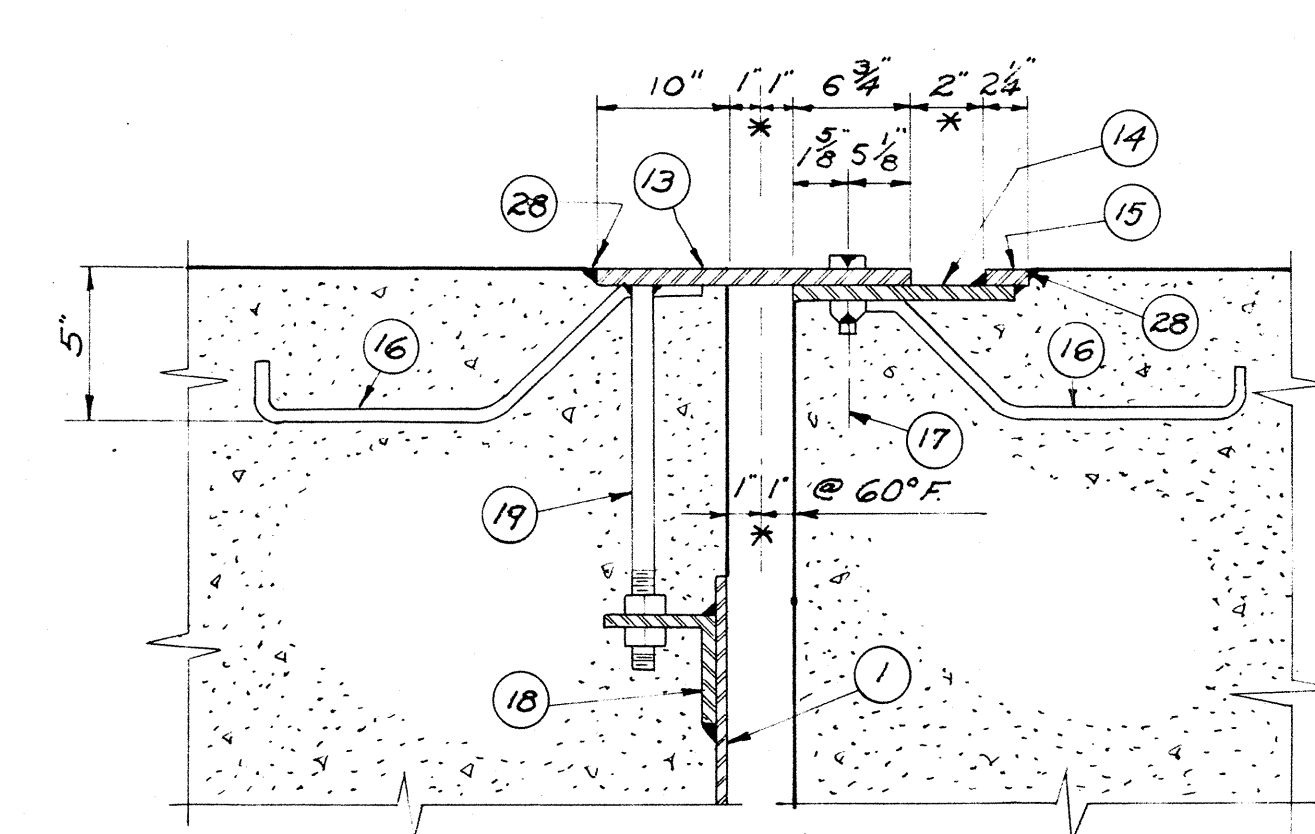
1. ST 6" WF 39.5# - CHAMFER 1/4" AS SHOWN.
2. 2" x 4" x 3/8"
3. BAR 1 1/2" x 1/2" - WELD TO L NO. 2 - 2 LINES 1/4" CONT. FILLET WELDS.
4. BAR 1 1/2" x 2 1/2" - WELD TO L NO. 2 - 2 LINES 1/4" CONT. FILLET WELDS.
5. ST 5" WF 24.5# - PUNCH 3"-1" HOLES IN WEB FOR REINFORCING BARS - WELD TO ST NO. 1 AS SHOWN. †
6. 3/8" LAMINATED SHIM.
7. 3/4" RIBBED BOLTS - DRILL HOLES IN STRINGERS IN FIELD.
8. 3" LONG SLEEVE THREADED FOR 3/8" BOLT @ 2'-0" CTRS. WELD TO L NO. 2.
9. BAR 3/8" x 0'-9" - THREADED 3"-1 EA. SLEEVE NO. 8 TO ST NO. 1 WITH 1/4" FILLET ALL AROUND.
10. BENT BAR 3/8" x 2'-0" @ 1'-0" CTRS. FULL LENGTH OF L NO. 2 - SEE "FABRICATION DETAIL" - WELD TO L NO. 2 WITH 1/4" FILLET ALL AROUND.
11. 1/2" BOLT WITH SQ. HEAD @ 2'-0" CTRS. - GREASE FOR EASY REMOVAL - 1/8" x 1 1/2" SLOTTED HOLE IN ST NO. 1 - 3/8" HOLE IN BAR NO. 3 & L NO. 2. TACK WELD 1/2" SQ. NUT TO L NO. 2.
12. 1/2" R. - SEE DETAILS - BENT DOWN FLUSH WITH FACE OF CURB.
13. 1/2" R. - SEE DETAILS - BENT DOWN FLUSH WITH FACE OF CURB.
14. 1/2" R. - SEE DETAILS - BENT DOWN FLUSH WITH FACE OF CURB. (MAY BE MADE OF BAR 2 1/4" x 1/2" IF JOINT IS GROUND FLUSH BOTH SIDES.)
15. NO. 13 & 14 WITH 1/4" FILLET ALL AROUND.
16. BAR 3/8" x 1'-6" - BEND AS SHOWN - WELD TO R. NO. 13 & 14 WITH 1/4" FILLET ALL AROUND.
17. 1/2" BOLT - SAME AS NO. 12 EXCEPT FOR LENGTH.
18. CLIP L 3 1/2" x 3" x 3/8" x 0'-3" - WELD TO ST NO. 1.
19. BAR 3/8" x 1'-0" WITH 2 SQ. NUTS - THREAD 3" - WELD BAR TO R. NO. 13 WITH 1/4" FILLET ALL AROUND.
20. ANCHOR BAR 2 1/2" x 3/8" x 1'-0" - WELD TO R. NO. 13 & 14.
21. EXPANSION JOINTS SHALL CONFORM TO ROADWAY AND CURB SURFACES.
- † OMIT HOLES AT EXTERIOR STRINGERS.

FABRICATION DETAIL

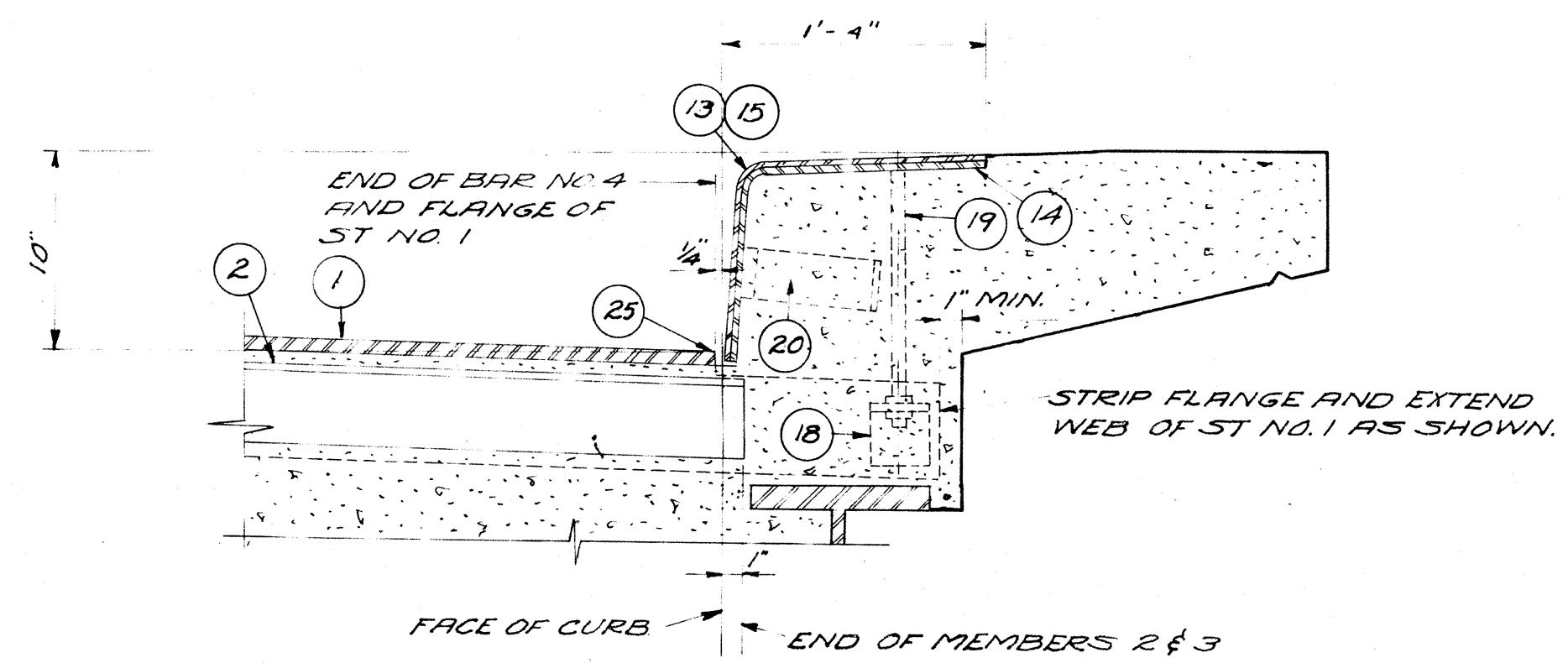


AFTER CONCRETE HAS SET FILL JOINT WITH HOT POURED RUBBER TYPE JOINT FILLER.

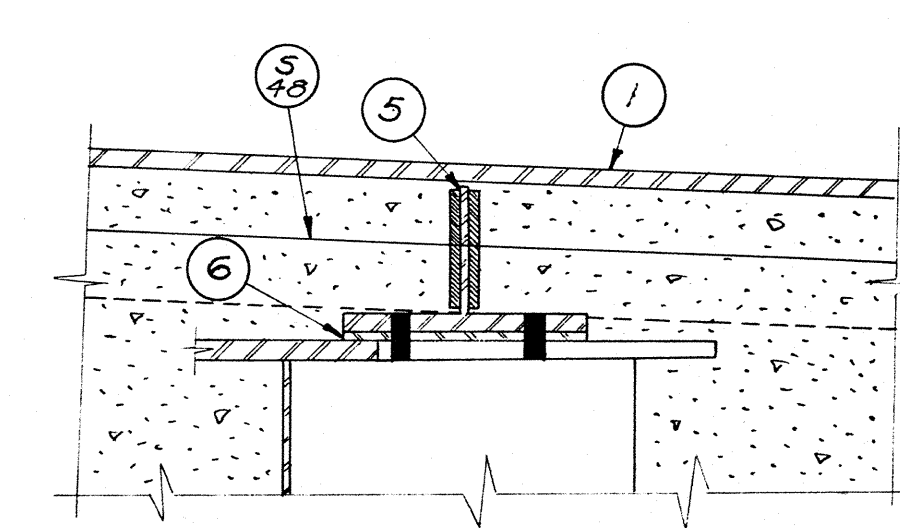
SECTION E1



DETAIL AT CURB



DETAIL AT STRINGERS



BILL OF BARS
DIMENSIONS IN BENDING ARE
OUT TO OUT

SUPERSTRUCTURE

(52,760#)

THREE PIERS

(8680#)

TWO ABUTMENTS

(11,780#)

POUR	MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
POUR 1 - SPAN 1	S 1	6	5	3-6	6 1/2	TRANSV. - TOP & BOT - STAGGERED	
	S 2	6	5	5-0	"	"	
	S 3	6	5	6-3	"	"	
	S 4	6	5	7-9	"	"	
	S 5	6	5	9-3	"	"	
	S 6	6	5	10-6	"	"	
	S 7	6	5	12-0	"	"	
	S 8	6	5	13-6	"	"	
	S 9	6	5	15-0	"	"	
	S 10	6	5	16-3	"	"	
	S 11	6	5	17-9	"	"	
	S 12	6	5	19-0	"	"	
	S 13	6	5	20-6	"	"	
	S 14	6	5	22-0	"	"	
	S 15	6	5	23-6	"	"	
	S 16	6	5	25-0	"	"	
	S 17	6	5	26-3	"	"	
	S 18	6	5	27-9	"	"	
	S 19	6	5	29-3	"	"	
	S 20	5/2	5	32-0	"	"	
	S 21	6	5	30-0	"	"	
	S 22	6	5	28-9	"	"	
	S 23	6	5	27-3	"	"	
	S 24	6	5	26-0	"	"	
	S 25	6	5	24-6	"	"	
	S 26	6	5	23-0	"	"	
	S 27	6	5	21-6	"	"	
	S 28	6	5	20-3	"	"	
	S 29	6	5	18-9	"	"	
	S 30	6	5	17-6	"	"	
	S 31	6	5	16-0	"	"	
	S 32	6	5	14-6	"	"	
	S 33	6	5	13-3	"	"	
	S 34	6	5	12-0	"	"	
	S 35	6	5	10-6	"	"	
	S 36	6	5	9-0	"	"	
	S 37	6	5	7-9	"	"	
	S 38	6	5	6-3	"	"	
	S 39	6	5	5-0	"	"	
	S 40	6	5	3-6	"	"	
	S 41	6	5	2-0	"	"	
	S 42	8/8	5	24-9	1-4	LONGIT. BETWEEN STRINGERS - UPPER BARS STAGGERED WITH LOWER BARS	
	S 43	9/8	4	4-0	1-0	TRANSV. - CURB & FLOOR	A
	S 44	12	5	26-0	SHOWN	LONGIT. - CURB	
	S 45	9/8	4	3-6	1-0	TRANSV. - CURB	B
	S 46	24	5	2-3	SHOWN	CURB & RAILPOST	
	S 47	24	5	3-0	"	CURB & RAILPOST	C
	S 48	9	6	6-0	"	EXP. JOINT - SYMM. ABT. STRINGERS	
	S 49	4	4	11-6	"	EXP. JOINT - BETWEEN STRINGERS	
POUR 2 - SPAN 2	S 20	4/26	5	32-0	6 1/2	TRANSV. - TOP & BOT - STAGGERED	
	S 21	12	5	30-0	"	"	
	S 22	12	5	28-9	"	"	
	S 23	12	5	27-3	"	"	
	S 24	12	5	26-0	"	"	
	S 25	12	5	24-6	"	"	
	S 26	12	5	23-0	"	"	
	S 27	12	5	21-6	"	"	
	S 28	12	5	20-3	"	"	
	S 29	12	5	18-9	"	"	
	S 30	12	5	17-6	"	"	
	S 31	12	5	16-0	"	"	
	S 32	12	5	14-6	"	"	
	S 33	12	5	13-3	"	"	
	S 34	12	5	12-0	"	"	
	S 35	12	5	10-6	"	"	
	S 36	12	5	9-0	"	"	
	S 37	12	5	7-9	"	"	
	S 38	12	5	6-3	"	"	
	S 39	12	5	5-0	"	"	
	S 40	12	5	3-6	"	"	
	S 41	12	5	2-0	"	"	
POUR 1 - SPAN 1	S 43	30/2	5	4-0	1-0	TRANSV. - CURB & FLOOR	A
	S 45	30/2	5	3-6	1-0	"	B
	S 46	7/2	5	2-3	SHOWN	CURB & RAILPOST	
	S 47	7/2	5	3-0	"	"	C
POUR 2 & 3	S 50	26/4	5	26-0	1-4	LONGIT. BETWEEN STRINGERS - UPPER BARS STAGGERED WITH LOWER BARS	
	S 51	10	5	30-0	SHOWN	LONGIT. OVER STRINGERS - CENTER ON PIER 1	
	S 52	36	5	27-0	"	LONGIT. - CURB	
	S 53	4	5	30-0	"	"	CENTER ON PIER 2
	S 54	10	7	11-3	"	STRUT AT PIERS 1 & 3 - BOTT.	
	S 55	16	4	11-3	"	"	STIRRUP SUP
	S 56	9/6	4	5-6	1-0	"	STIRRUPS
	S 57	18	7	6-0	SHOWN	"	STIRRUPS
	S 58	8/8	4	5-9	1-0	"	STIRRUPS

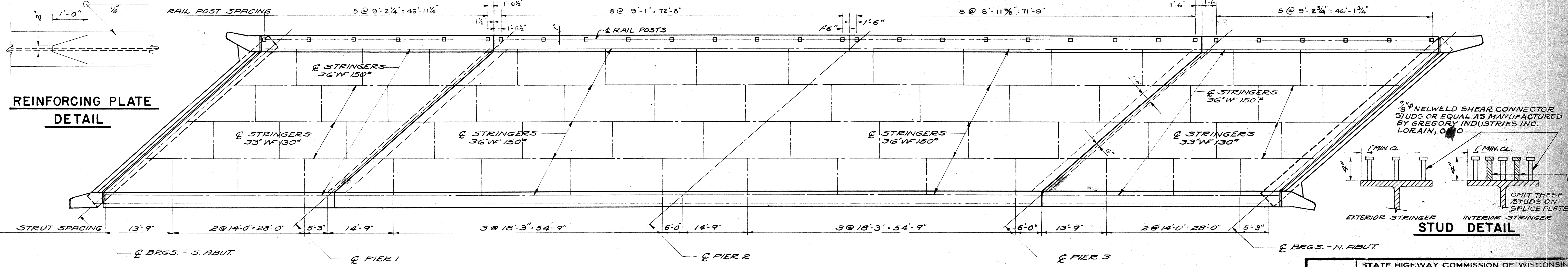
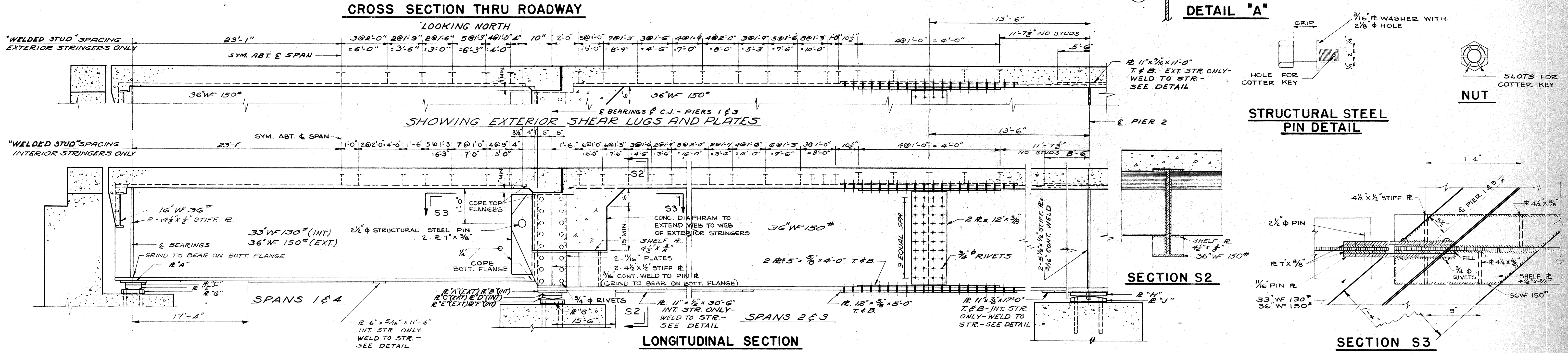
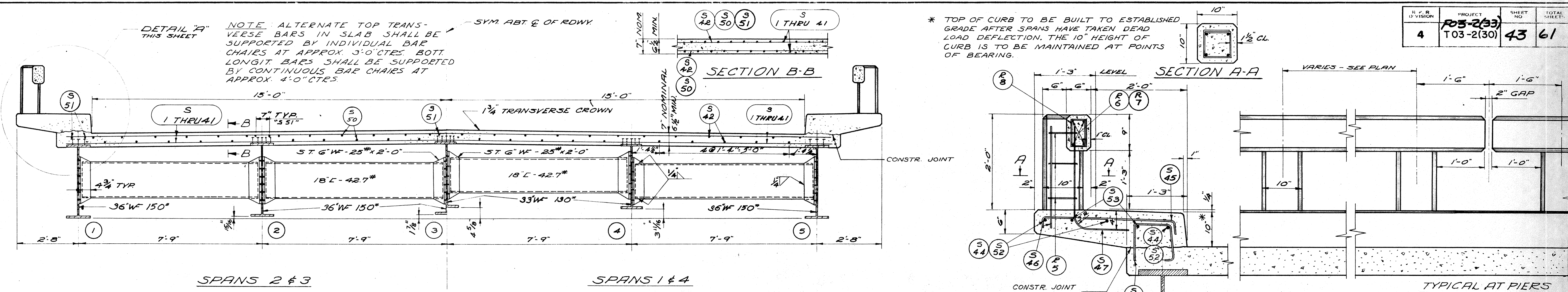
POUR	MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
POUR 3 - SPAN 3	S 1	6	5	3-6	6 1/2	TRANSV. - TOP & BOT - STAGGERED	
	S 2	6	5	5-0	"	"	
	S 3	6	5	6-3	"	"	
	S 4	6	5	7-9	"	"	
	S 5	6	5	9-3	"	"	
	S 6	6	5	10-6	"	"	
	S 7	6	5	12-0	"	"	
	S 8	6	5	13-6	"	"	
	S 9	6	5	15-0	"	"	
	S 10	6	5	16-3	"	"	
	S 11	6	5	17-9	"	"	
	S 12	6	5	19-0	"	"	
	S 13	6	5	20-6	"	"	
	S 14	6	5	22-0	"	"	
	S 15	6	5	23-6	"	"	
	S 16	6	5	25-0	"	"	
	S 17	6	5	26-3	"	"	
	S 18	6	5	27-9	"	"	
	S 19	6	5	29-3	"	"	
	S 20	5/2	5	32-0	"	"	
	S 21	6	5	30-0	"	"	
	S 22	6	5	28-9	"	"	
	S 23	6	5	27-3	"	"	
	S 24	6	5	26-0	"	"	
	S 25	6	5	24-6	"	"	
	S 26	6	5	23-0	"	"	
	S 27	6	5	21-6	"	"	
	S 28	6	5	20-3	"	"	
	S 29	6	5	18-9	"	"	
	S 30	6	5	17-6	"	"	
	S 31	6	5	16-0	"	"	
	S 32	6	5	14-6	"	"	
	S 33	6	5	13-3	"	"	
	S 34	6	5	12-0	"	"	
	S 35	6	5	10-6	"	"	
	S 36	6	5	9-0	"	"	
	S 37	6	5	7-9	"	"	
	S 38	6	5	6-3	"	"	
	S 39	6	5	5-0	"	"	
	S 40	6	5	3-6	"	"	
	S 41	6	5	2-0	"	"	
	S 42	8/8	5	24-9	1-4	LONGIT. BETWEEN STRINGERS - UPPER BARS STAGGERED WITH LOWER BARS	
	S 43	9/8	4	4-0	1-0	TRANSV. - CURB & FLOOR	A
	S 44	12	5	26-0	SHOWN	LONGIT. - CURB	
	S 45	9/8	4	3-6	1-0	TRANSV. - CURB	B
	S 46	24	5	2-3	SHOWN	CURB & RAILPOST	
	S 47	24	5	3-0	"	CURB & RAILPOST	C
	S 48	9	6	6-0	"	EXP. JOINT - SYMM. ABT. STRINGERS	
	S 49	4	4	11-6	"	EXP. JOINT - BETWEEN STRINGERS	
RAILING	R 1	16	4	5-6	SHOWN	END POSTS - HORIZONTAL	
	R 2	16	4	4-6	"	"	
	R 3	16	4	3-6	"	"	
	R 4	12	4	6-6	"	"	TIES
	R 5	180	3	2-9	"	RAIL POSTS - "	D
	R 6	32	5	25-6	"	RAILS - LONGIT. - SPANS 1 & 4	D
	R 7	48	5	26-6	"	"	2 & 3
	R 8	498	2	2-3	1-0	"	TIES

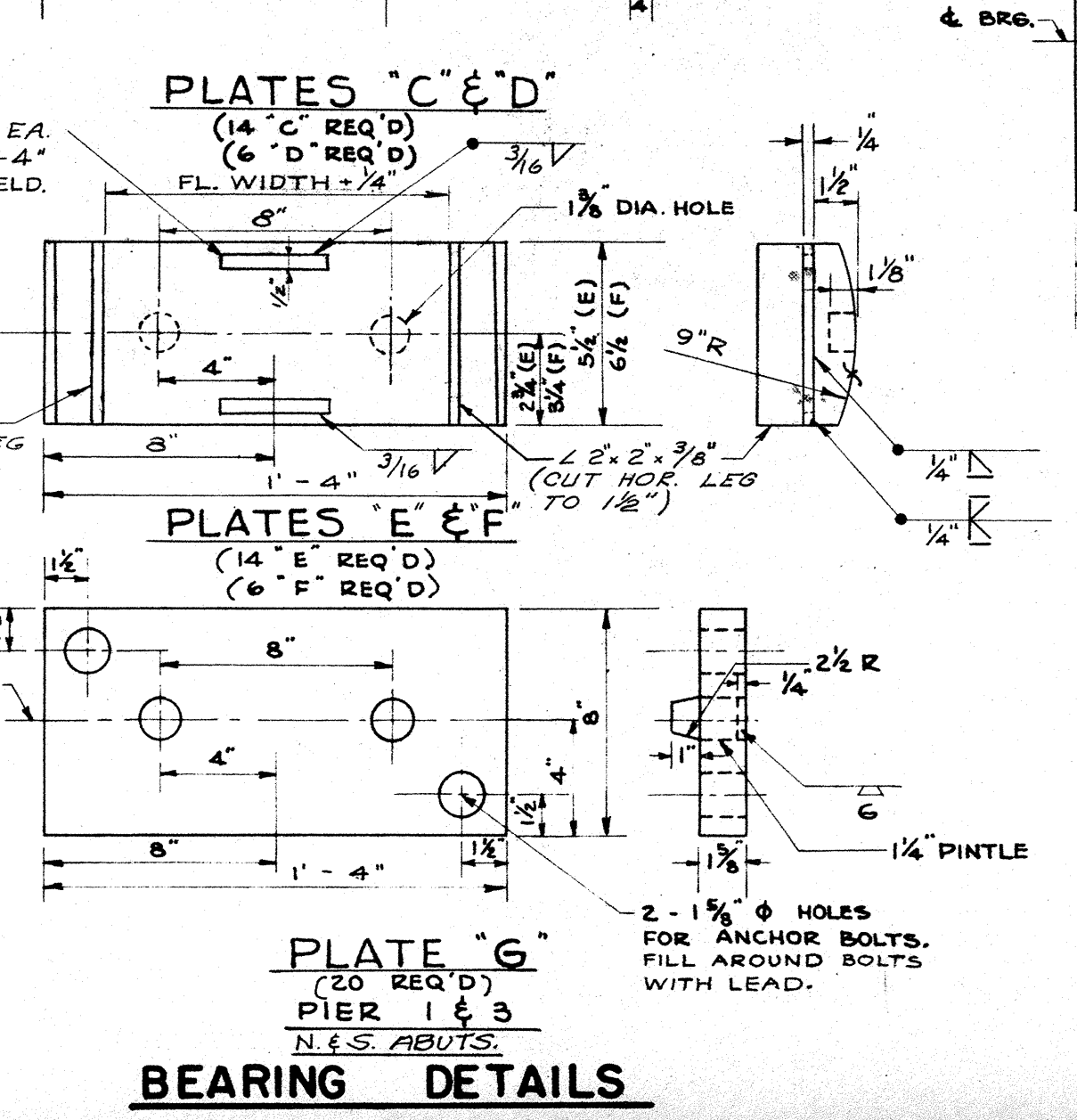
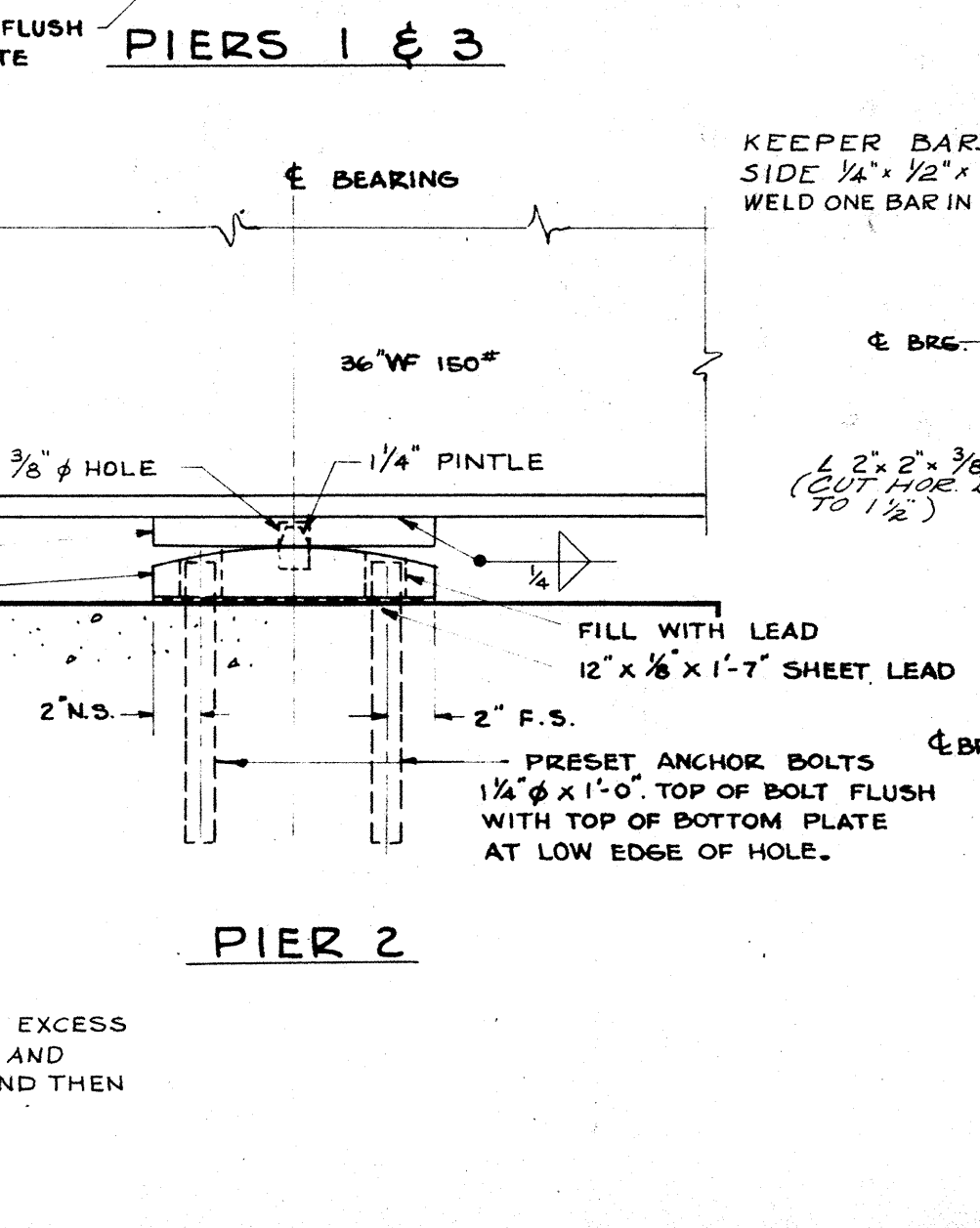
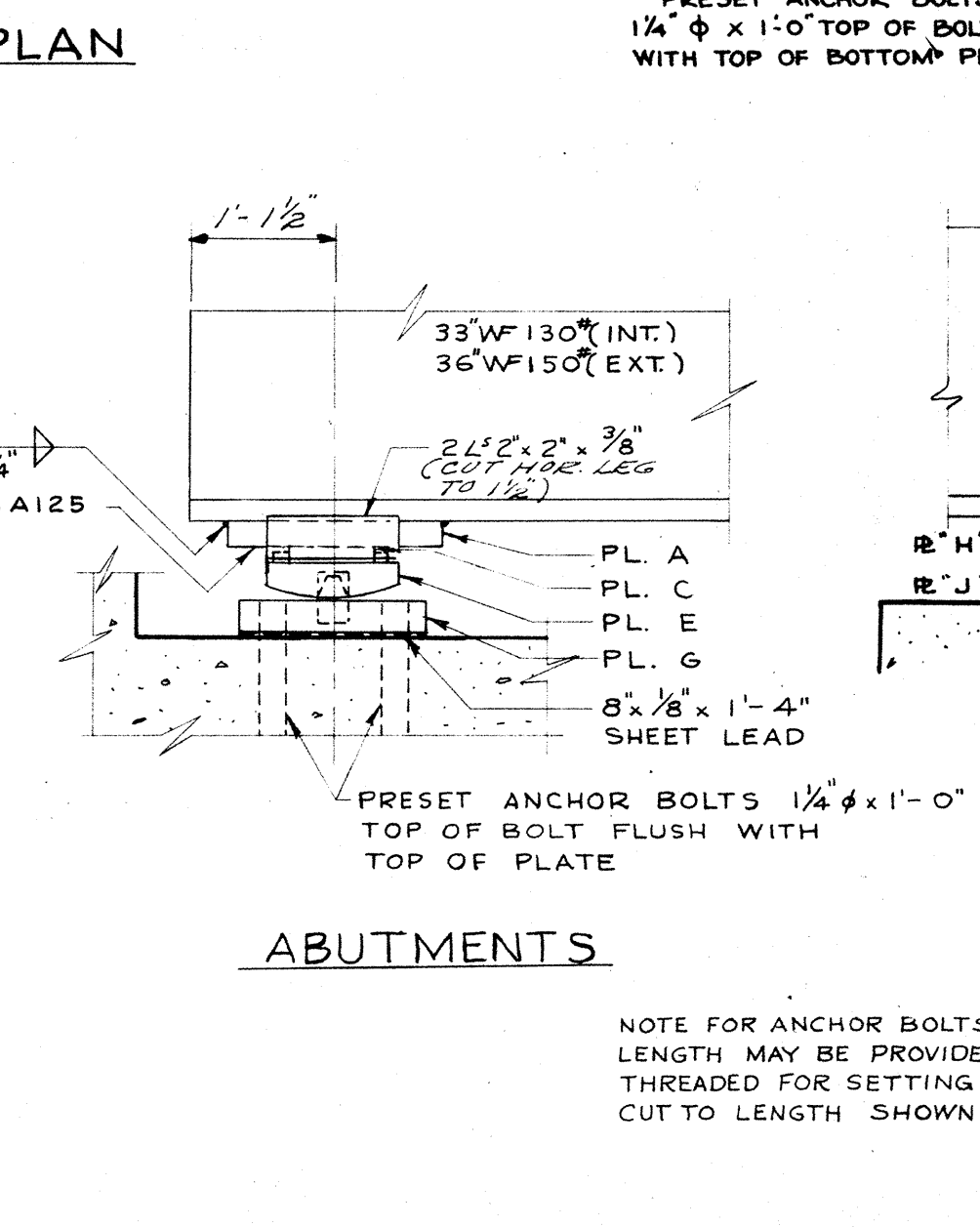
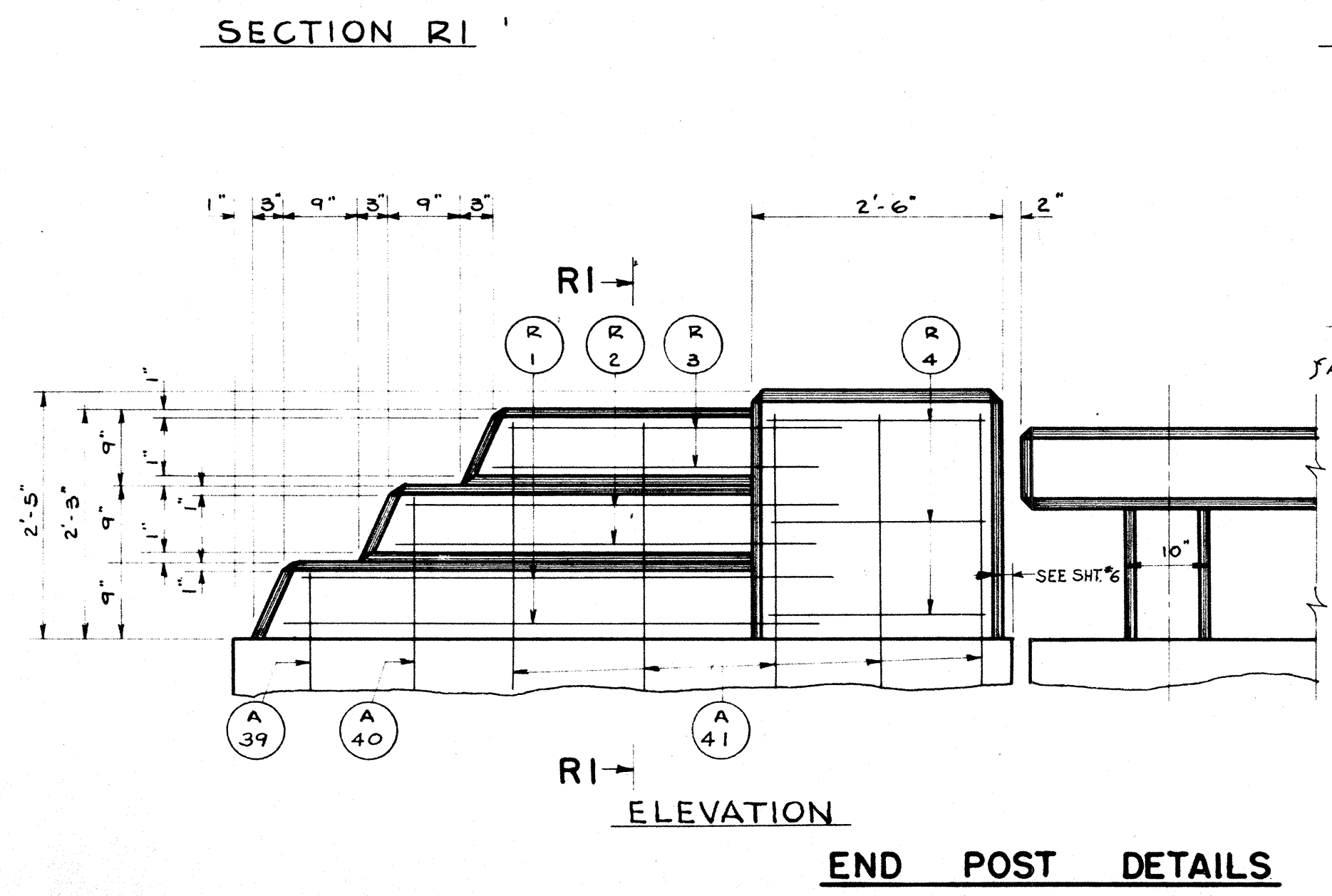
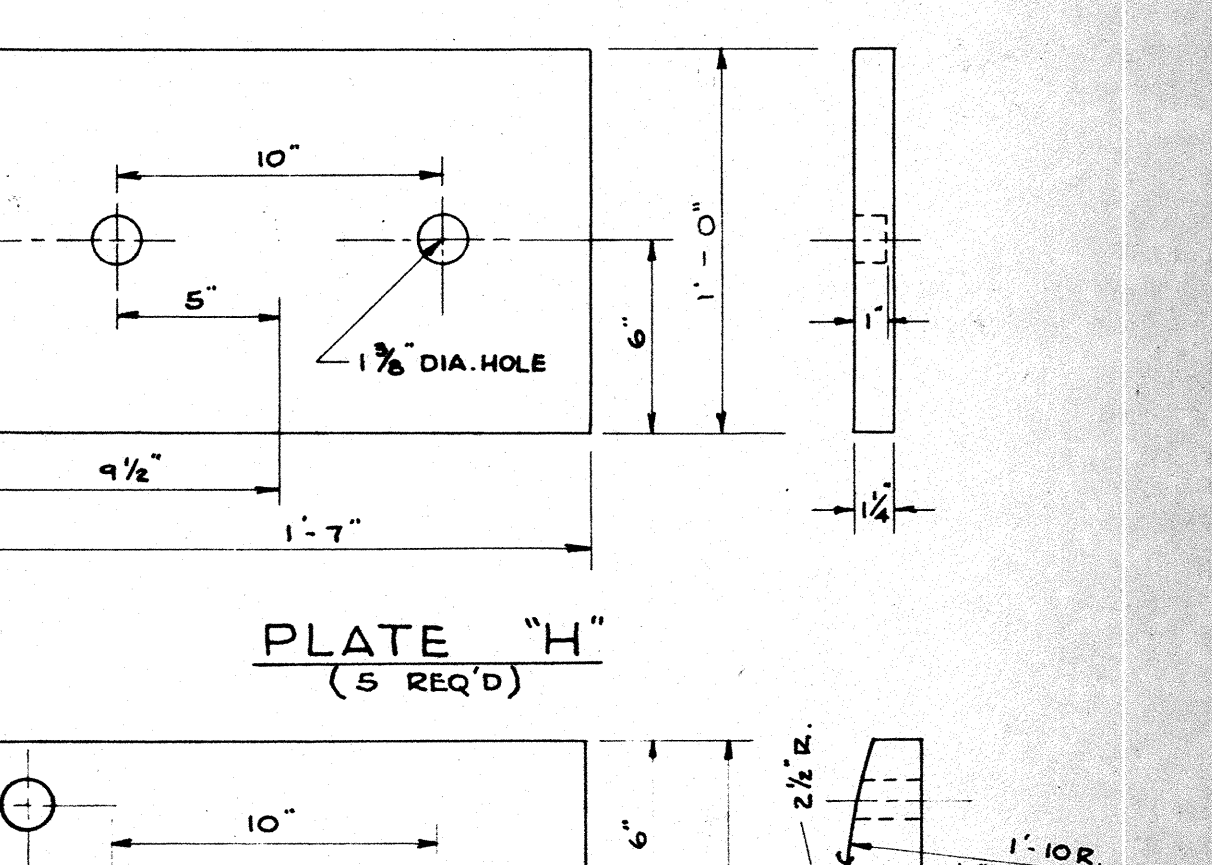
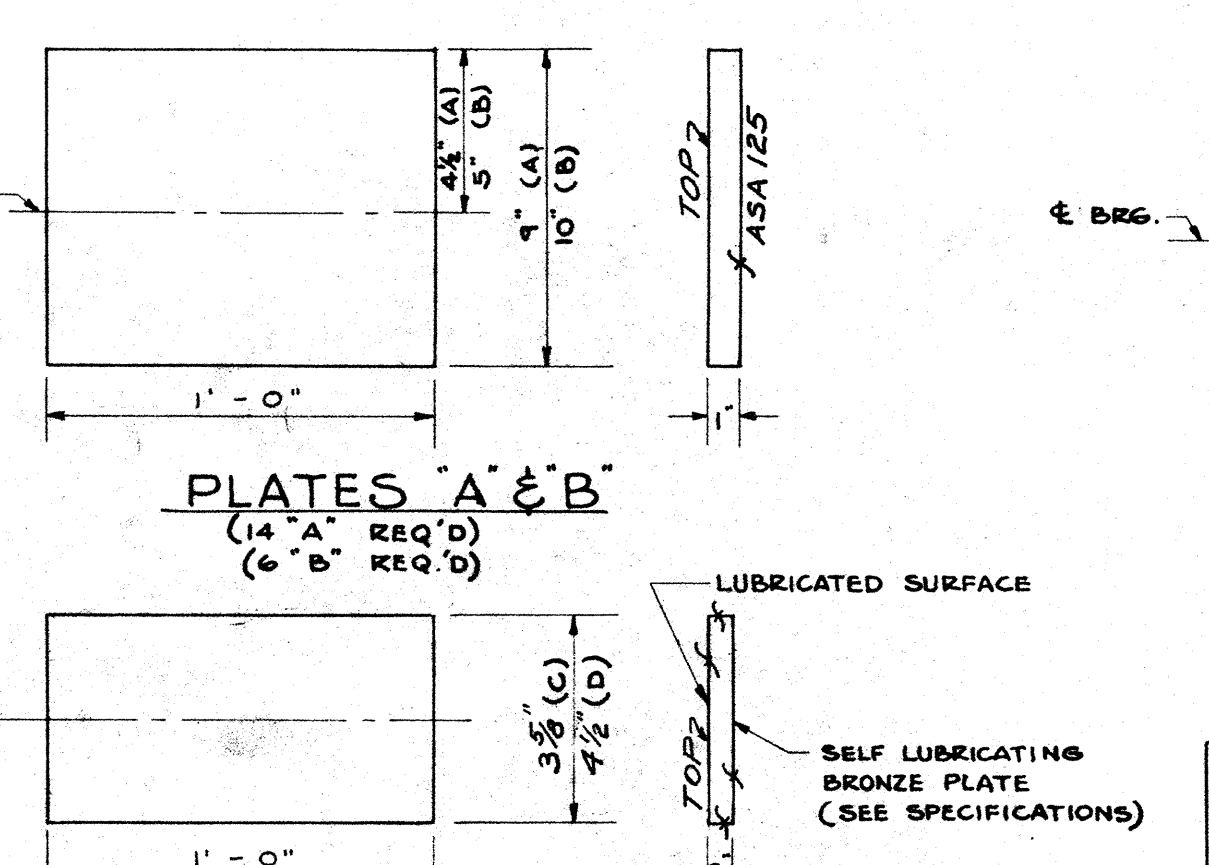
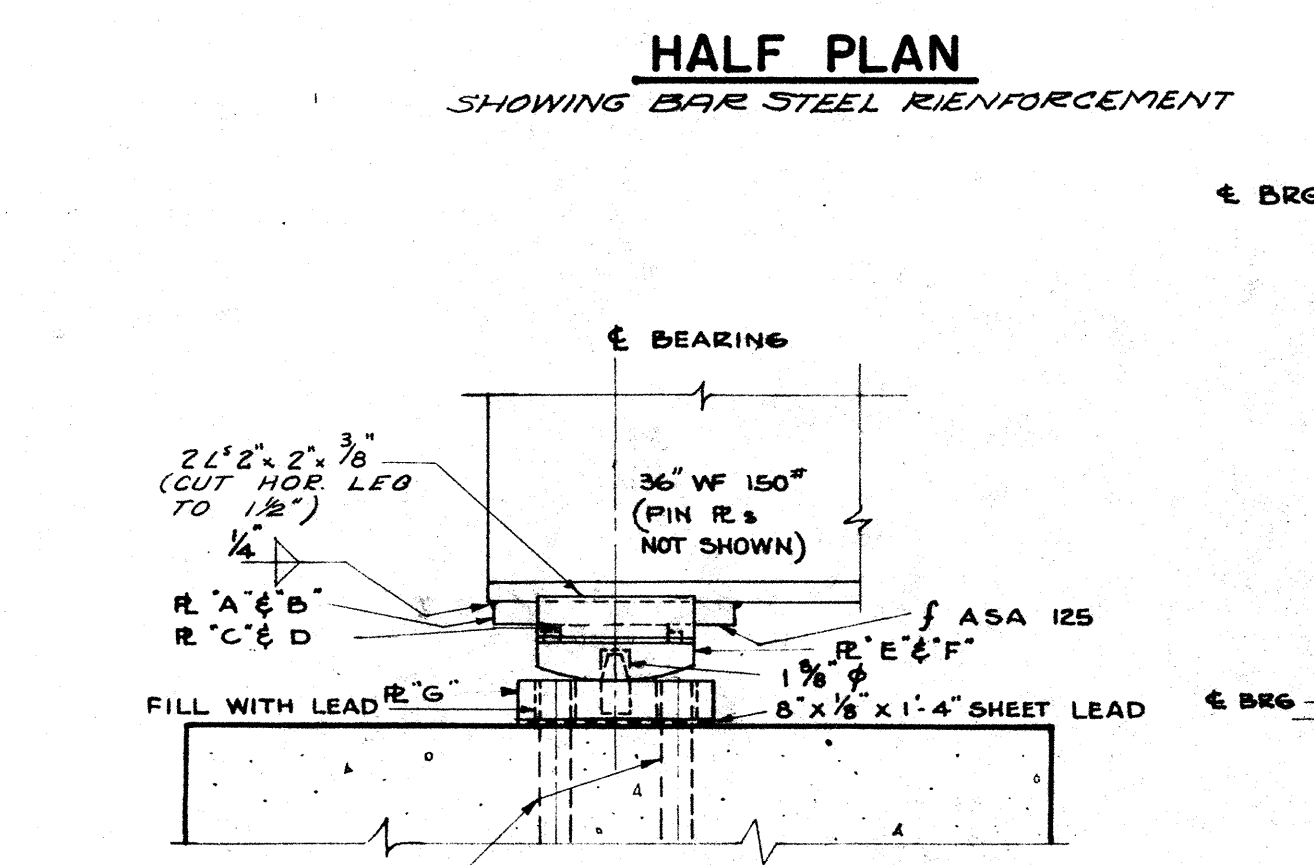
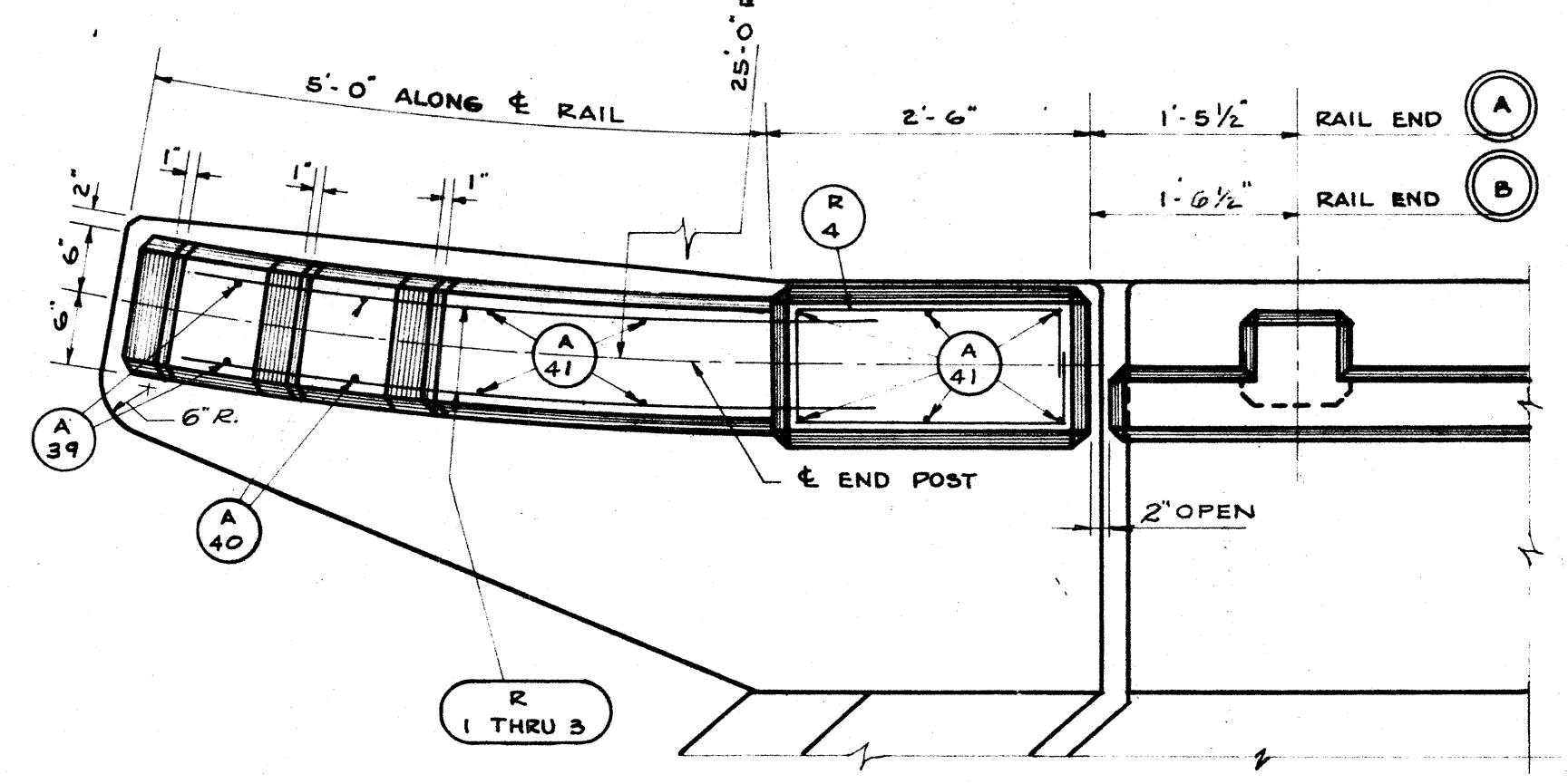
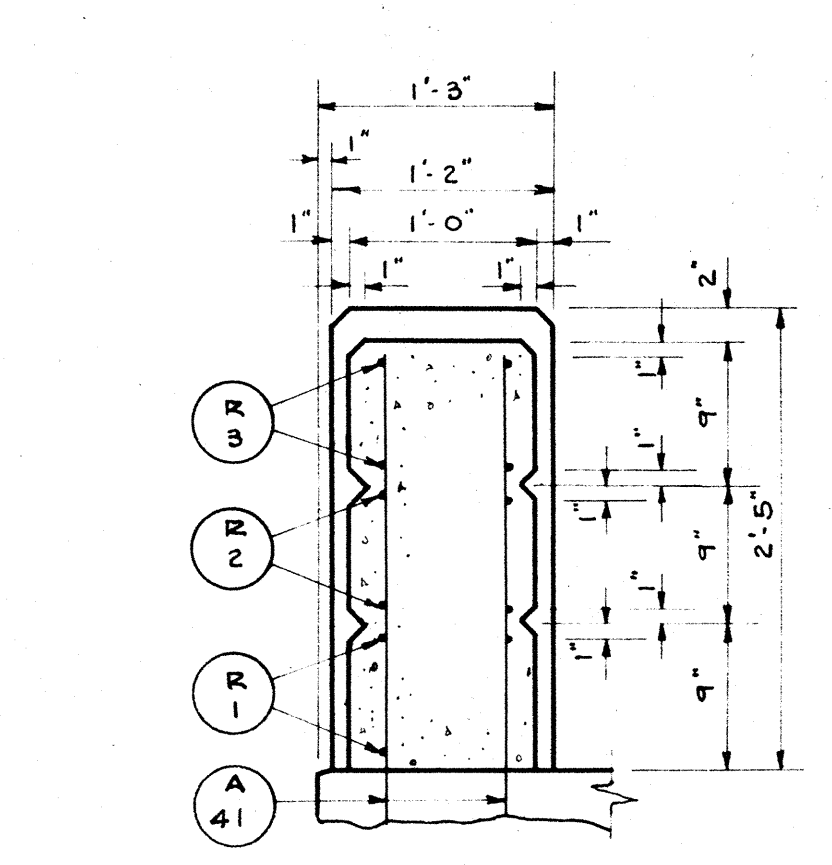
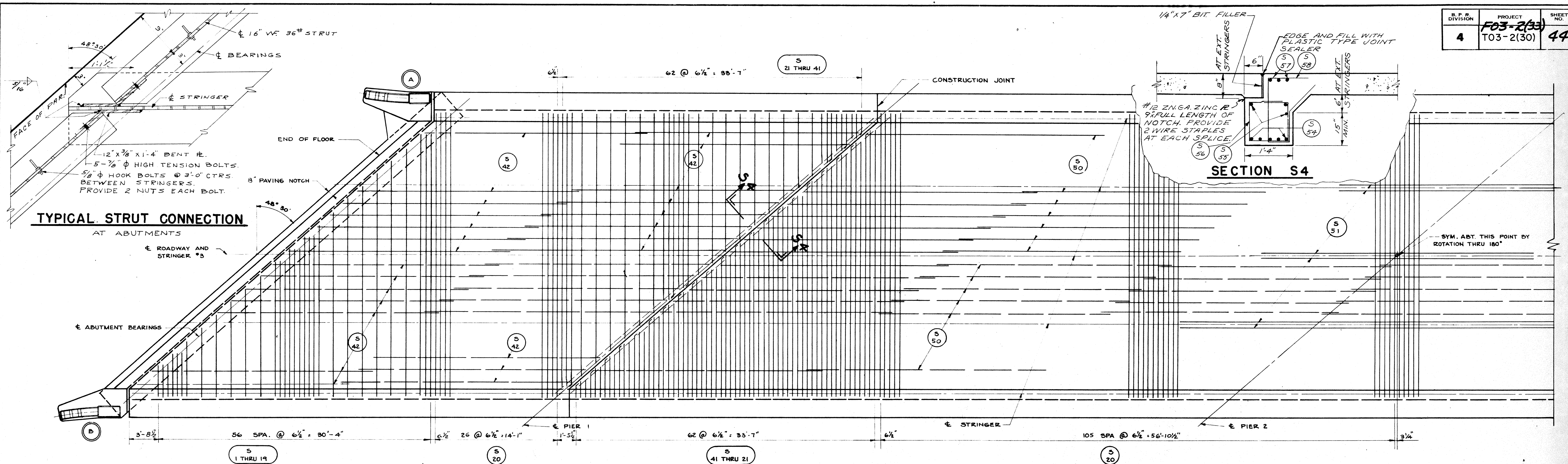
POUR	MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
FTGS.	P 1	80	4	5-6	6	FOOTINGS - LONGIT. - PIERS 1 & 3	
	P 2	56	5	4-6	10	"	TRANSV. - PIERS 1 & 3
	P 3	144	5	2-6	SHOWN	COLUMN DOWELS - ALL PIERS	
	P 4	72	5	6-0	9	FOOTINGS - LONGIT. & TRANSV. - PIER 2	
COLUMNS	P 5	8	5	16-0	SHOWN	COLUMNS - PIER 1	
	P 6	16	5	17-6	"	" & GIRDER - PIER 1	
	P 7	16	5	15-0	"	"	PIER 2 & 3
	P 8	32	5	16-9	"	" & GIRDER - PIER 2 & 3	
	P 9	196	4	9-6	1-0	COL. TIES (17 PIER 1, 16 PIERS 2 & 3 - EACH)	F
	P 22	72	5	5-0	SHOWN	COLUMNS - ALL PIERS	
GIRDERS	P 10	28	6	15-6	SHOWN	BOTTOM - END SPANS - PIERS 1 & 3	
	P 11	14	7	15-6	"	"	CENTER SPAN - PIERS 1 & 3
	P 12	28	7	15-6	"	"	END SPANS - PIER 2
	P 13	14	8	15-6	"	"	CENTER - PIER 2
	P 14	28	8	8-0	"	TOP - OVER INT. COLUMNS - PIERS 1 & 3	
	P 15	16	9	8-0	"	"	PIER 2
	P 16	12	4	11-6	"	"	STIRRUPS TIES - ALL PIERS
	P 17	6	4	9-9	"	"	"
	P 18	42	4	11-6	2-0	STIRRUPS - PIERS 1 & 3	D
	P 19	18	5	11-6	SHOWN	"	PIERS 2
	P 20	60	4	4-6	"	GRID	F
	P 21	60	4	2-3	"	"	"

POUR	MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
FTGS.	A 1	88	8	7-6	5	FOOTING - TOE - BOTT. - LONGIT.	
	A 2	64	5	4-6	1-0	"	TRANSV.
	A 3	96	5	2-6	SHOWN	" & COLUMN - FRONT SIDES	
	A 4	48	8	4-0	"	"	BACK FACE
COLUMNS, GIRDERS & WINGS	A 5	48	8	10-0	SHOWN	COLUMN - VERT. - BOTH ABUTS.	
	A 6	48	5	19-0	"	"	N. ABUTMENT
	A 7	48	5	20-6	"	"	S. ABUTMENT
	A 8	24	6	9-0	"	"	N. ABUTMENT
	A 9	24	6	10-6	"	"	S.
	A 10	4	4	14-3	1-6	"	TIES - "
	A 11	8	4	14-0	"	"	BOTH ABUTS.
	A 12	8	4	13-9	"	"	"
	A 13	8	4	13-6	"	"	"
	A 14	8	4	13-3	"	"	"
	A 15	8	4	13-0	"	"	"
	A 16	8	4	12-9	"	"	"
	A 17	8	4	12-6	"	"	"
	A 18	8	4	12-3	"	"	"
	A 19	32	4	12-0	"	"	"
	A 20	24	6	15-6	SHOWN	GIRDER - BOTTOM	
	A 21	18	4	15-6	"	"	FRONT FACE
	A 22	12	4	8-0	"	"	BACK FACE @ INT. COLUMN
	A 23	12	4	5-6	"	"	"
	A 24	8	4	25-0	"	"	"
	A 25	24	4	16-3	3-0	"	STIRRUPS
	A 26	16	7	9-0	SHOWN	PARAPET - TOP - INT. COLUMN	
	A 27	16	5	9-6	"	"	EXT.
	A 28	8	4	9-0	"	"	STIRRUP SUPPORT
	A 29	4	4	9-3	"	"	"
	A 30	88	4	12-6	1-0	"	STIRRUPS
	A 31	88	4	5-0	1-0	"	BLOCK STIRRUPS
	A 32	40	4	3-9	SHOWN	LONGIT. GRID BARS	G
	A 33	50	4	2-3	"	TRANSV. - "	
	A 34	16	4	11-0	"	"	EXP. JOINT SUPP. (NO. LAP REQ'D)
	A 35	48	5	5-0	1-1	"	HAUNCHES
	A 36	6	4	17-0	SHOWN	WING A - S. ABUT. - WING B - N. S. ABUT.	H
	A 37	2	4	17-3	"	WING A - NORTH ABUT.	H
	A 38	12	4	13-0	"	WING A & B - BOTH ABUTS.	G
	A 39	8</					

NOTE ALTERNATE TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROX. 3'-0" CTES. BOTT. LONGIT. BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROX. 4'-0" CTES.

* TOP OF CURB TO BE BUILT TO ESTABLISHED GRADE AFTER SPANS HAVE TAKEN DEAD LOAD DEFLECTION. THE 10" HEIGHT OF CURB IS TO BE MAINTAINED AT POINTS OF BEARING.

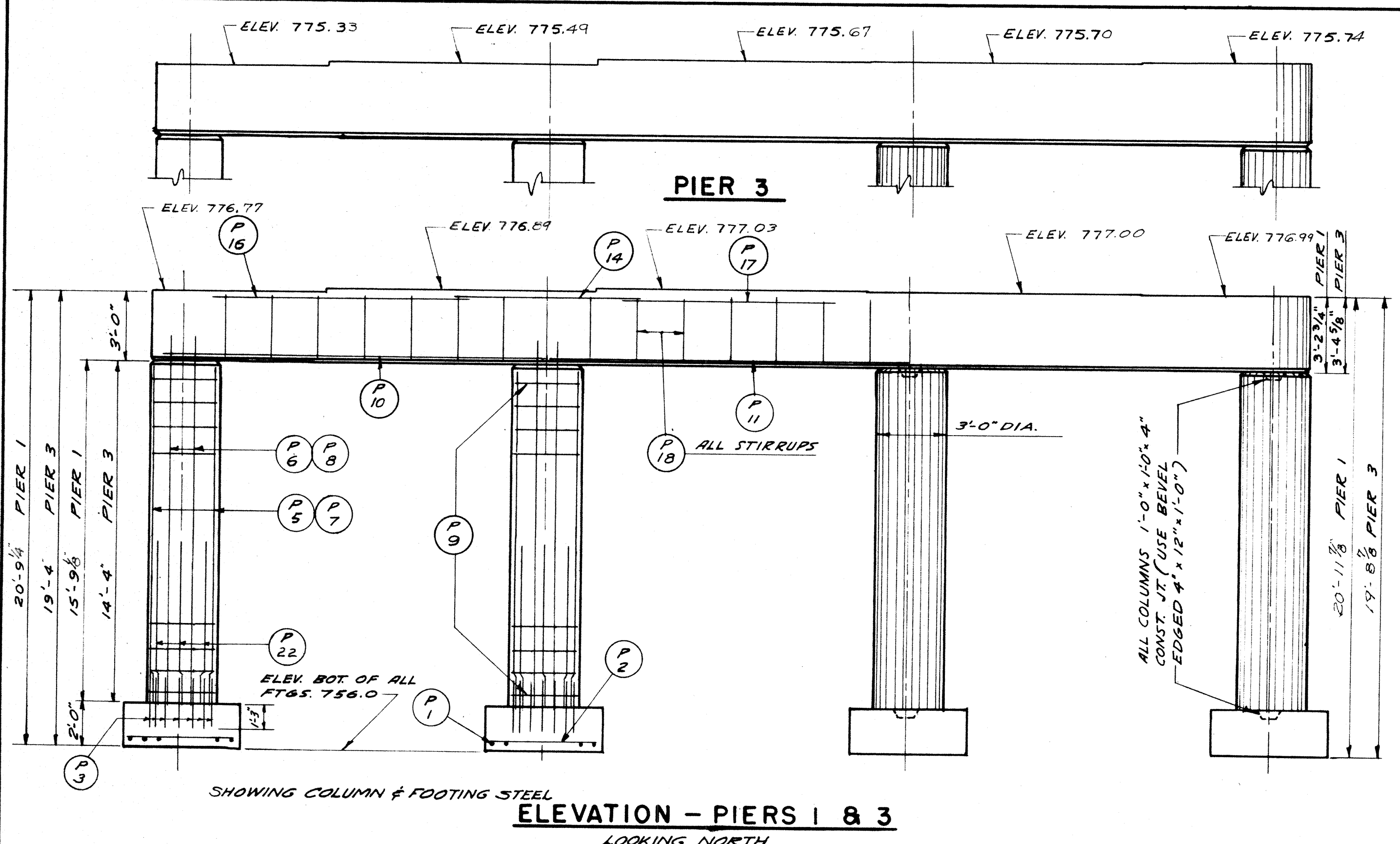




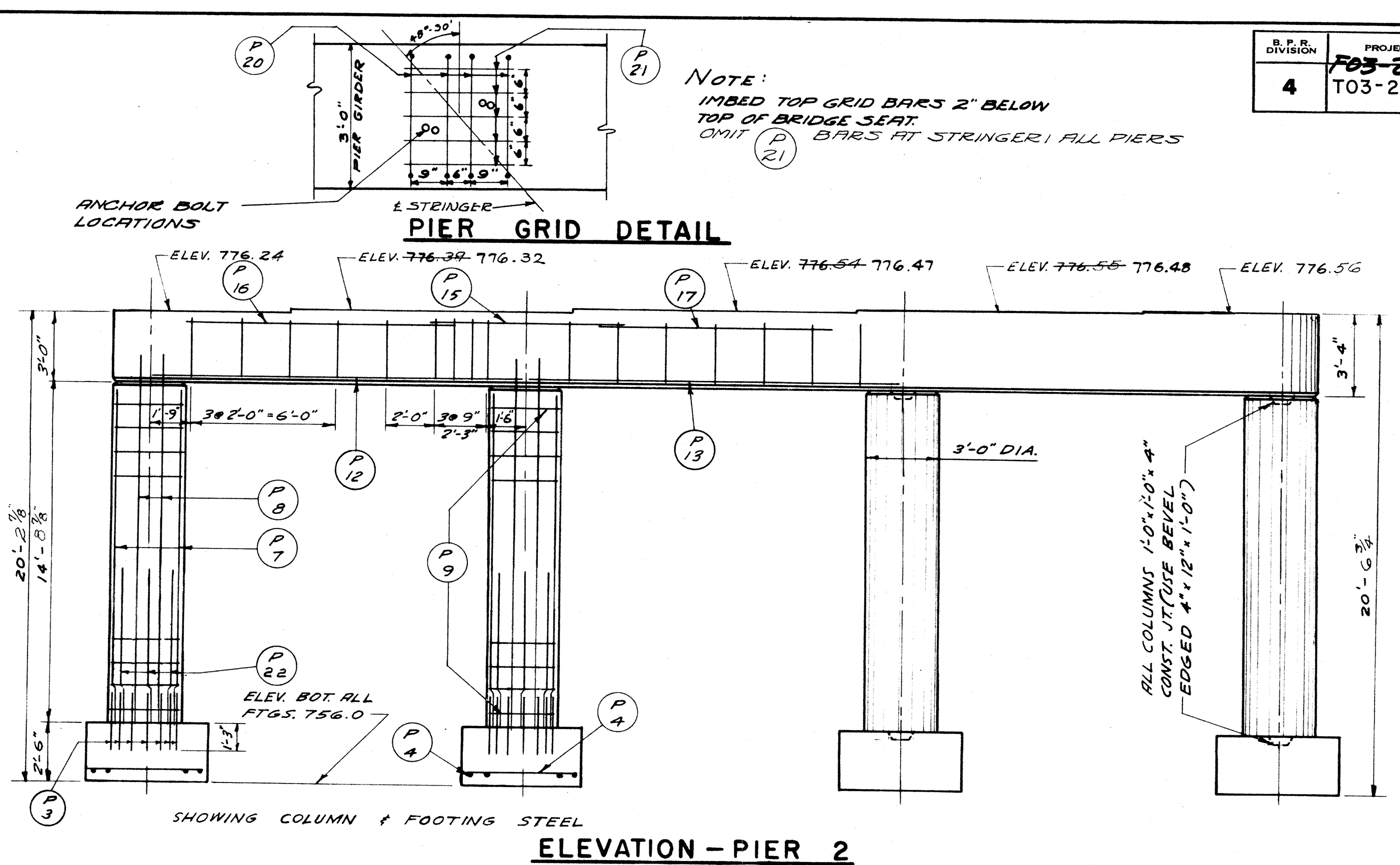
NOTE
ALL STRUCTURAL CARBON STEEL PLATES SHALL BE FLAT ROLLED PLATES, WITH ALL SURFACES SMOOTH AND FREE FROM WARP AND ALL EDGES SMOOTH, STRAIGHT AND VERTICAL. ALL CUTS SHALL BE MACHINE OR MACHINE FLAME CUT. SURFACES MARKED "C" SHALL BE MACHINE FINISHED.

REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN			
	SUPERSTRUCTURE			
	DATE 9/23/57	DRWN D.S.	CRD H.D.	SPEC 1957
	STRUCTURE B-70-27			SHEET 3 OF 8

B. P. R. DIVISION	PROJECT	SHEET NO.	TOTAL SHEETS
4	F03-2(33) T03-2(30)	45	61

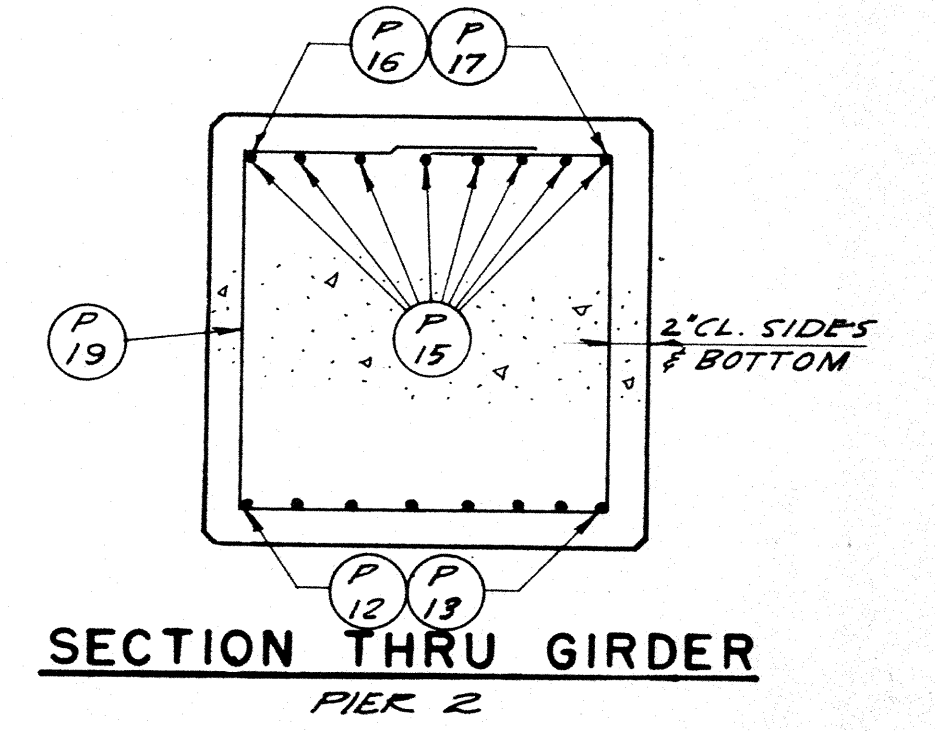
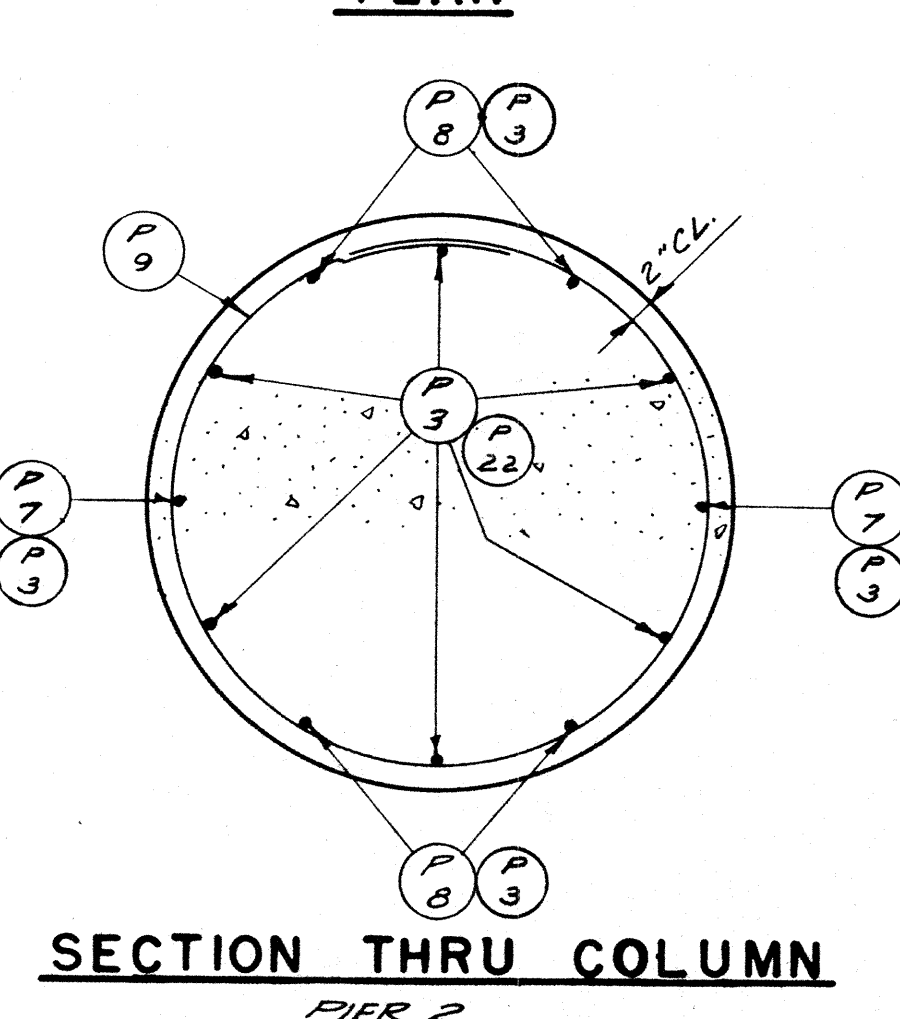
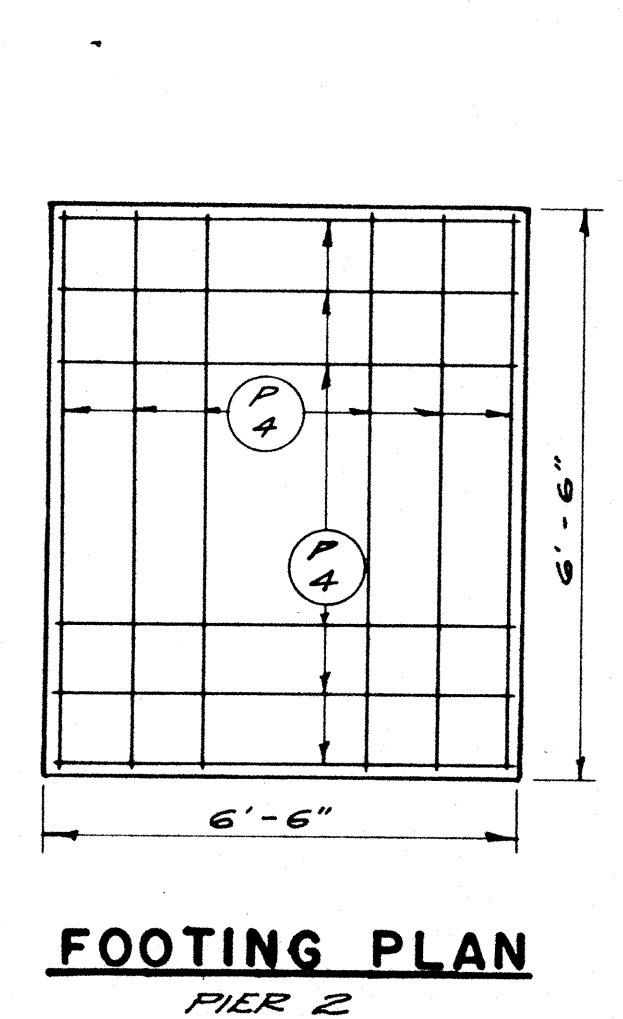
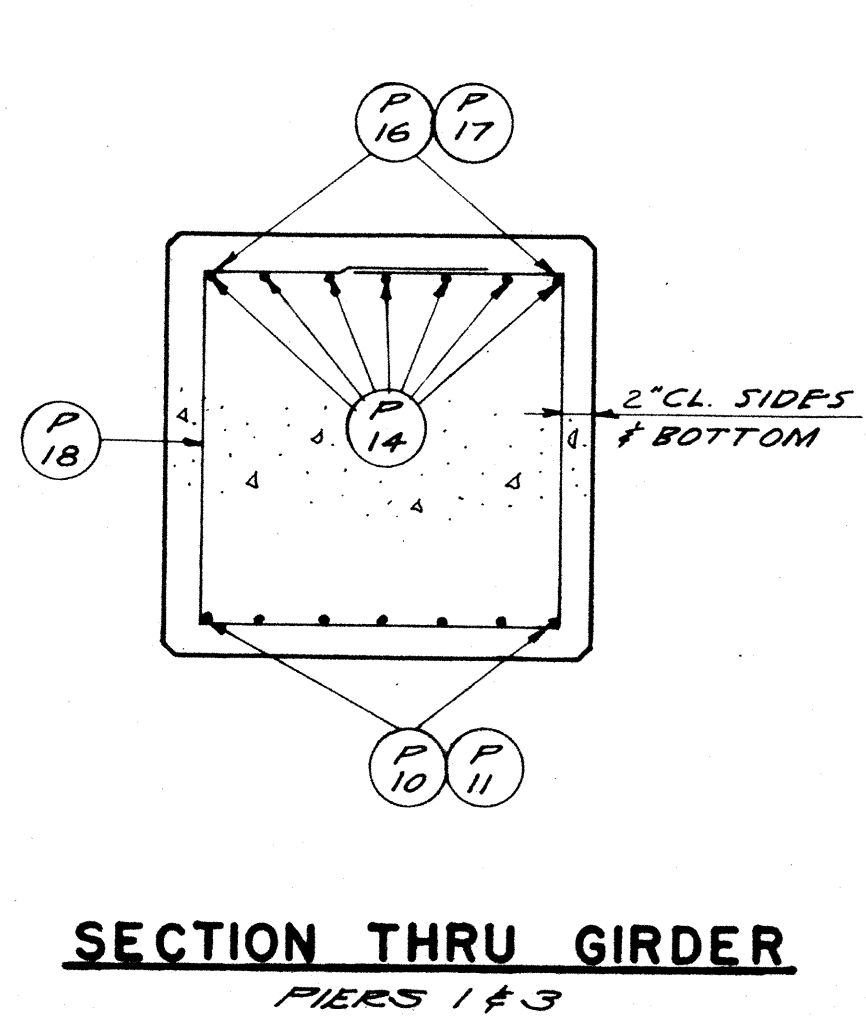
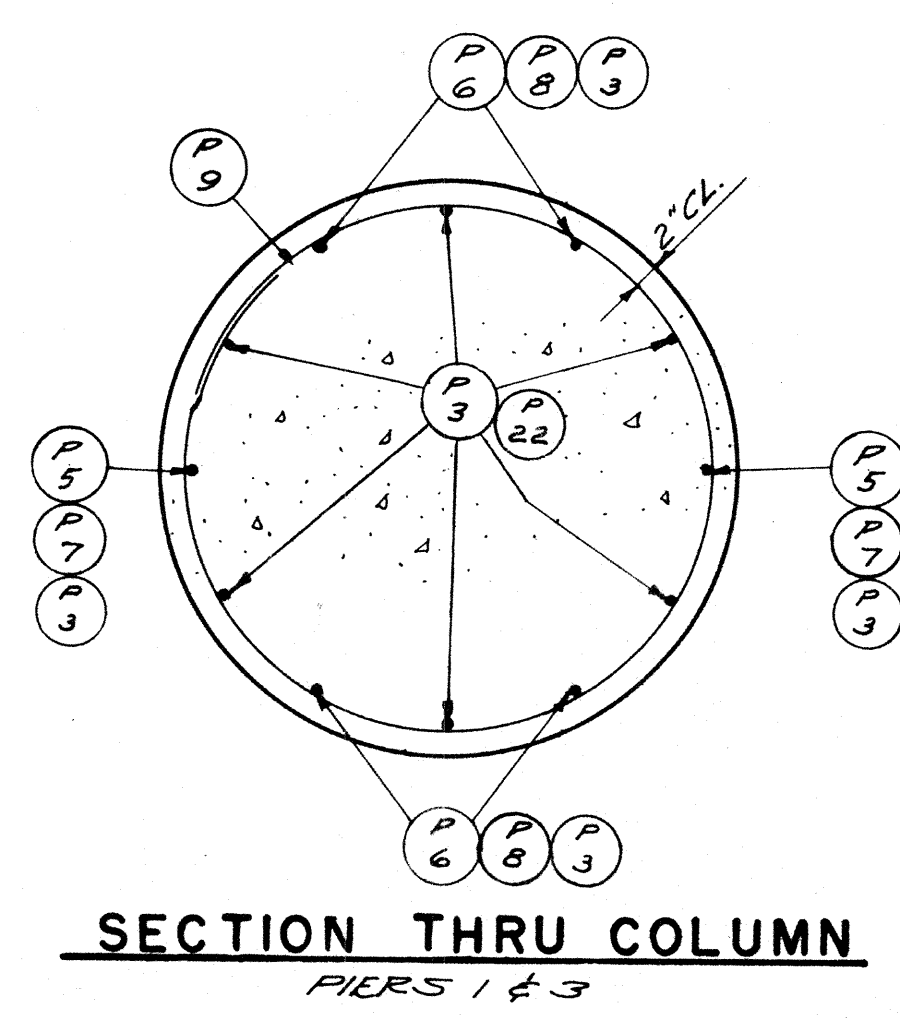
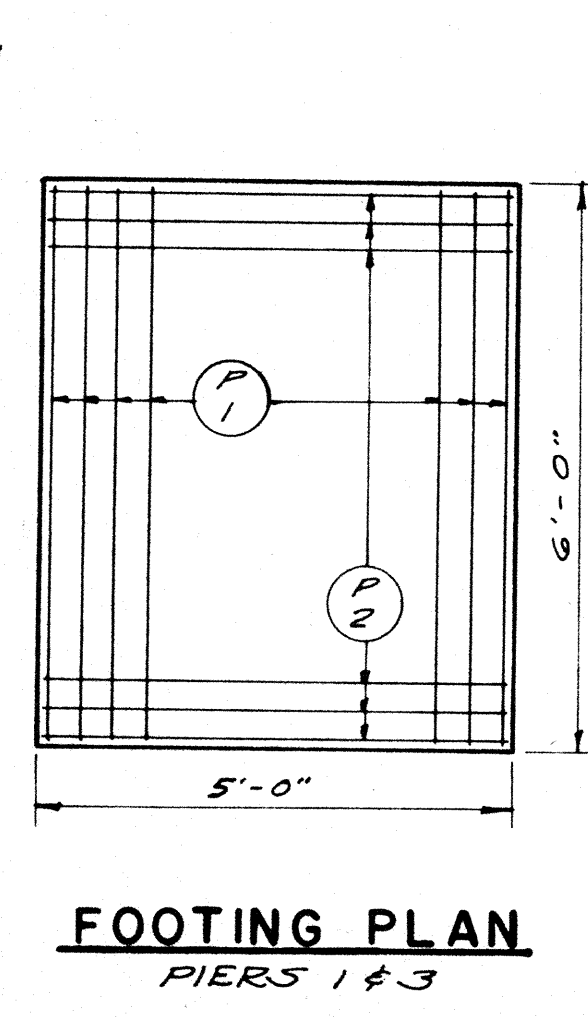
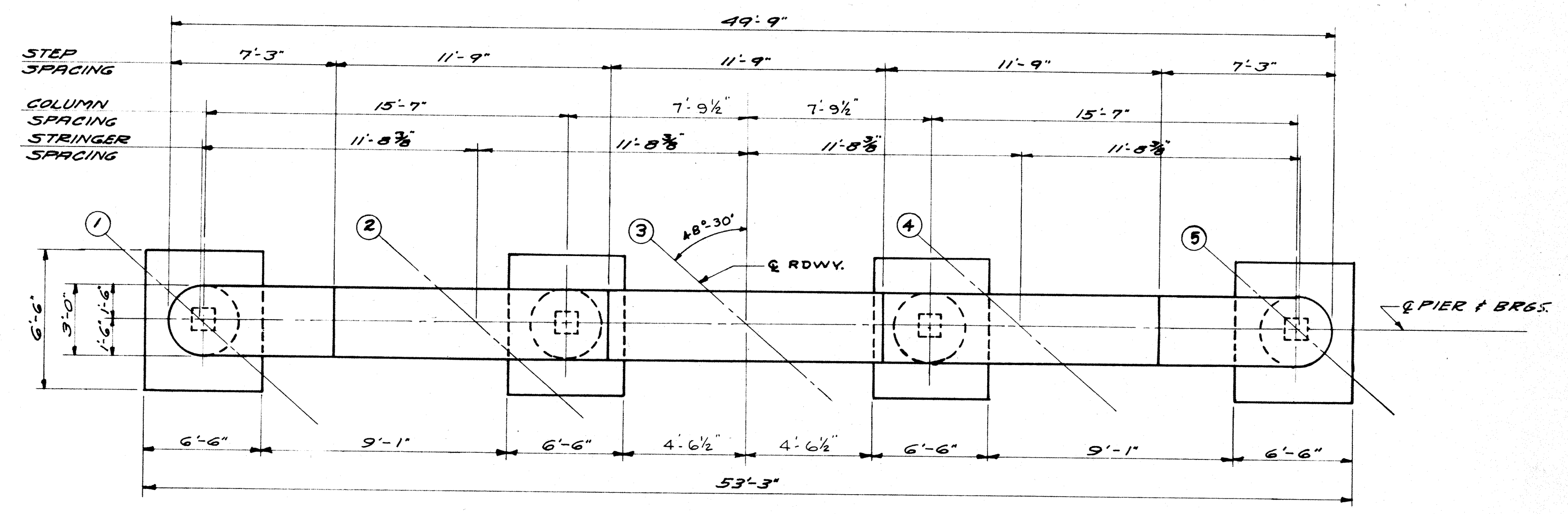
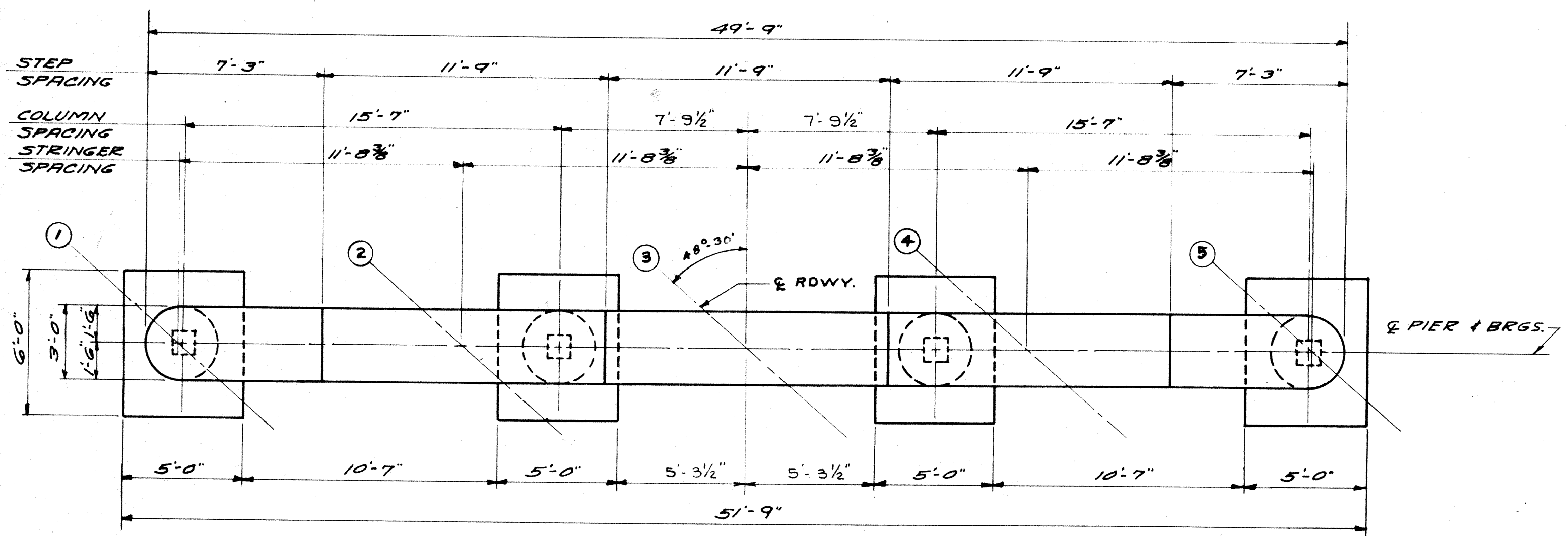


END VIEW

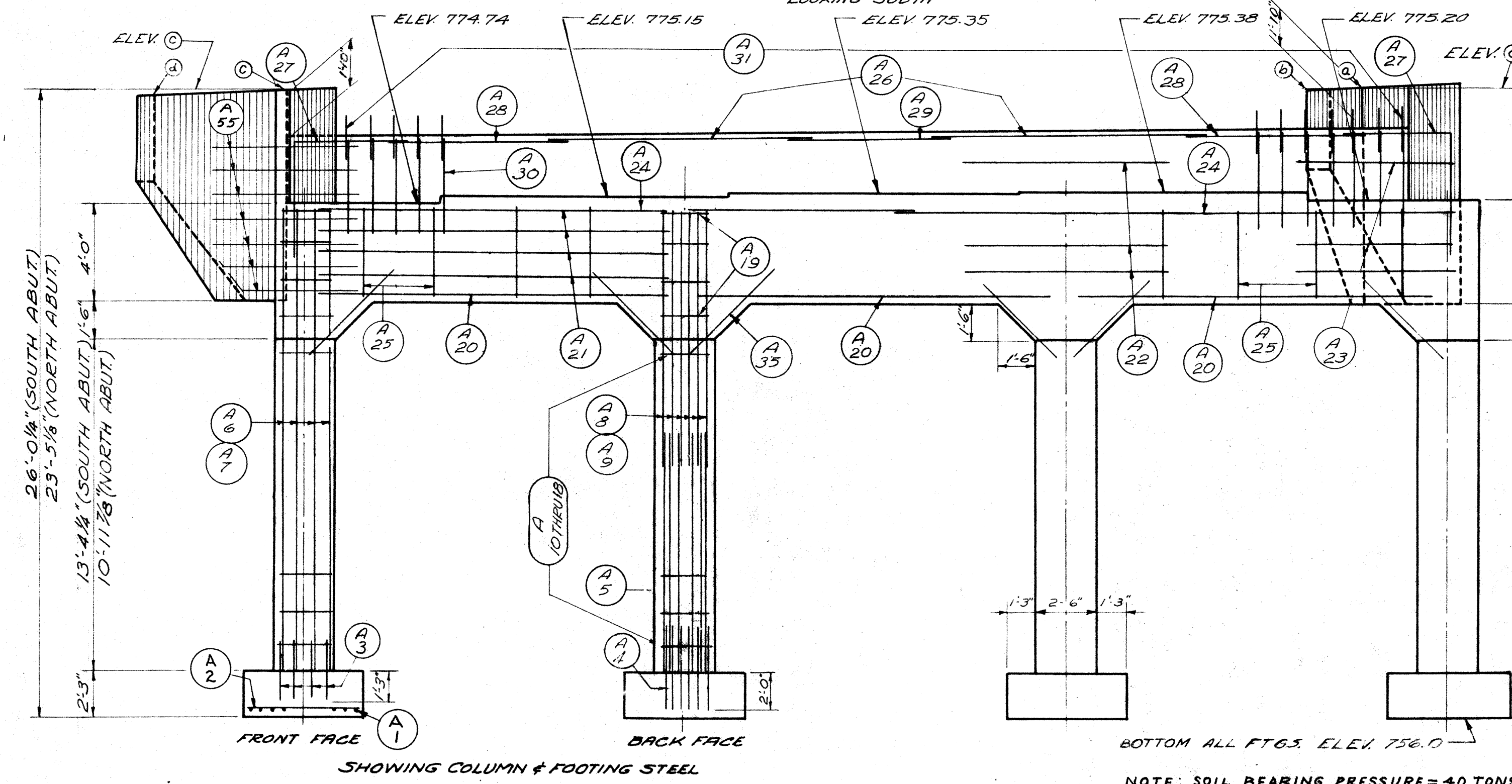
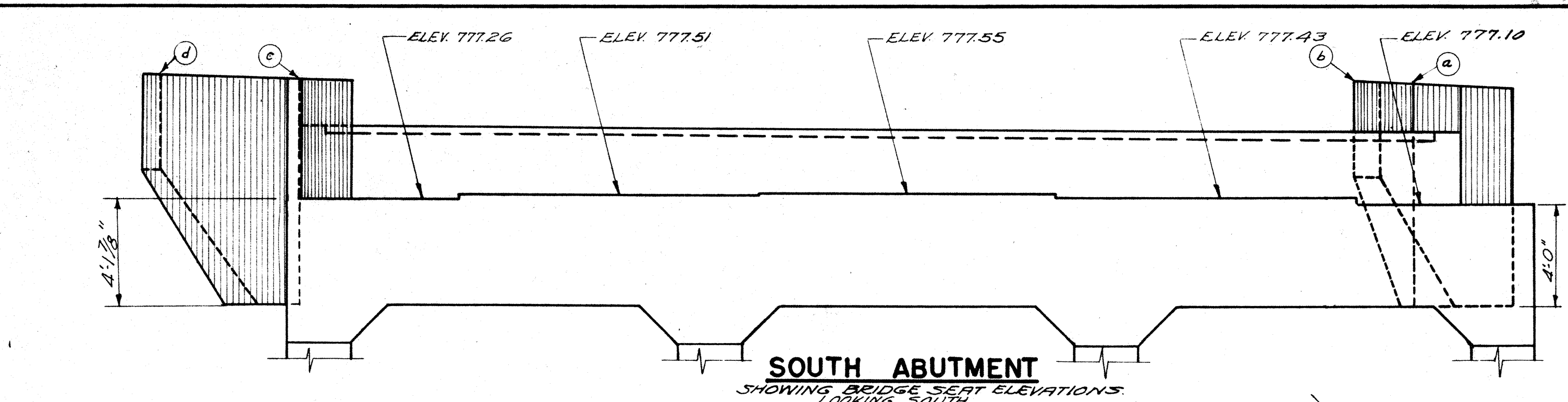


ELEVATION - PIER 2

NOTE: SOIL BEARING PRESSURE = 4.4 TONS PER SQ. FT.

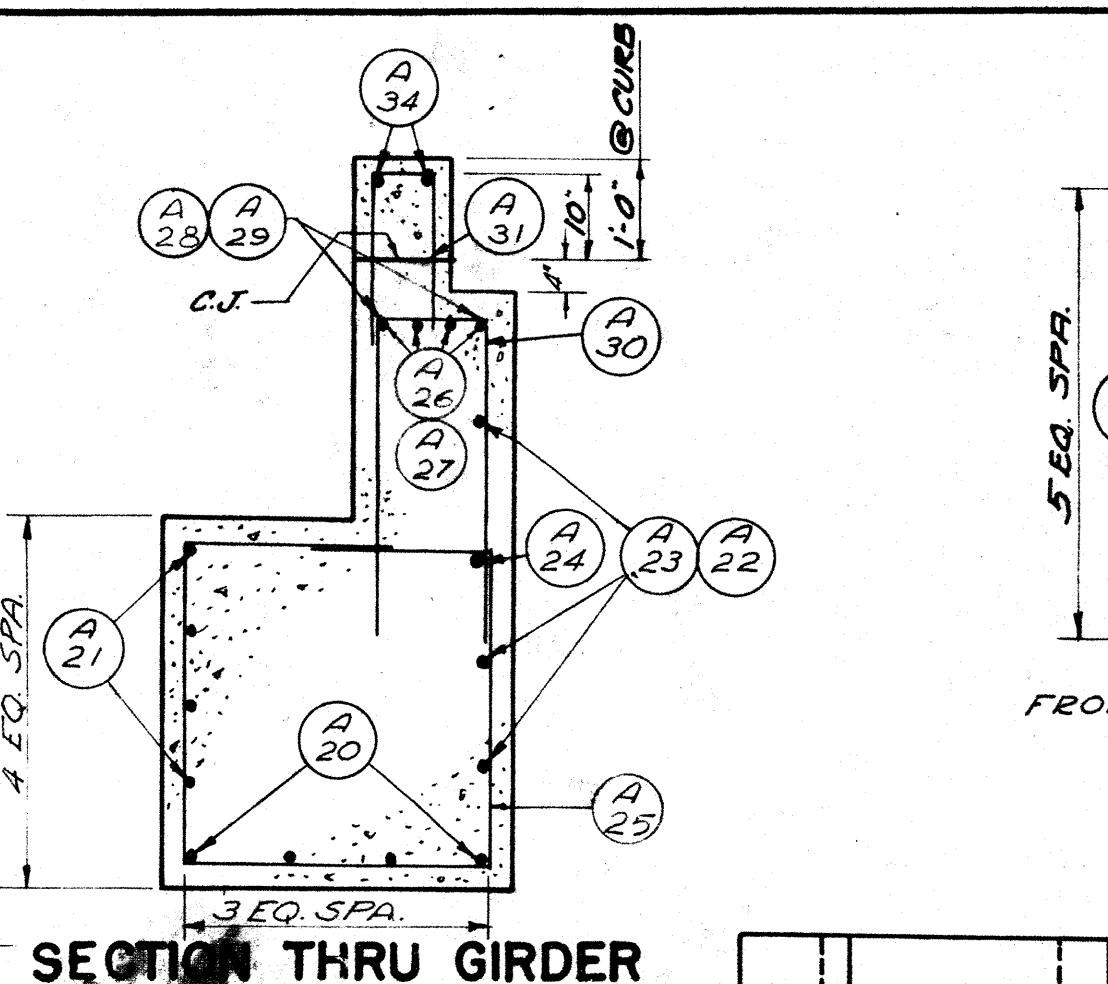


STATE HIGHWAY COMMISSION OF WISCONSIN					
PIERS					
DATE: 9-23-57	DRAWN: JMT	CHK: HB	SPEC: 1957	LOAD: H20-516	
STRUCTURE: B-70-27			SHEET: 4 OF 8		

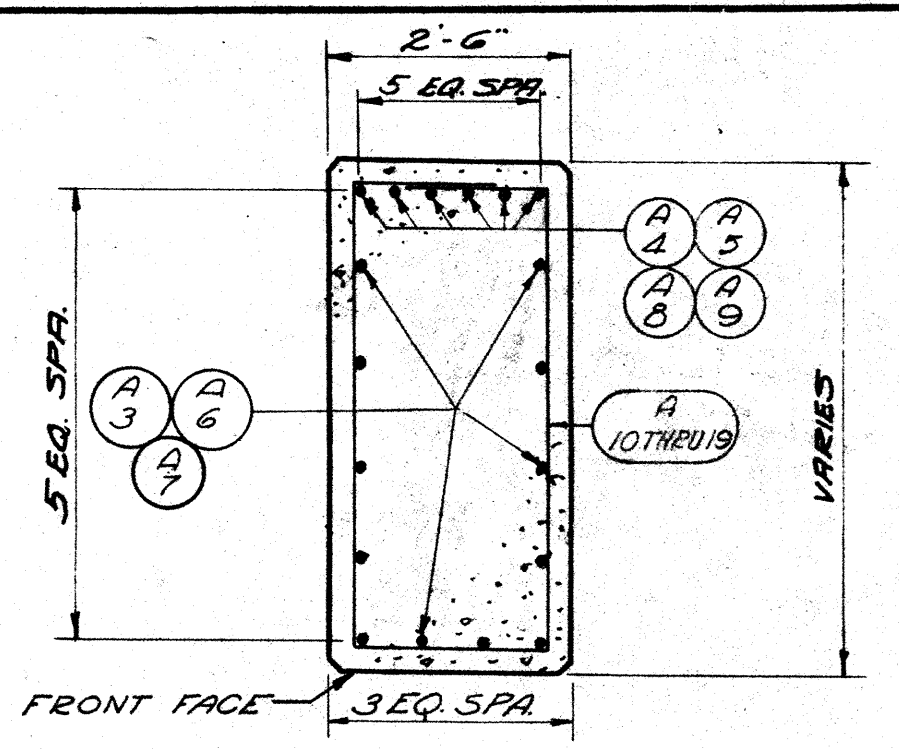


NORTH ABUTMENT ELEVATION
SHOWING STEEL FOR BOTH ABUTMENTS

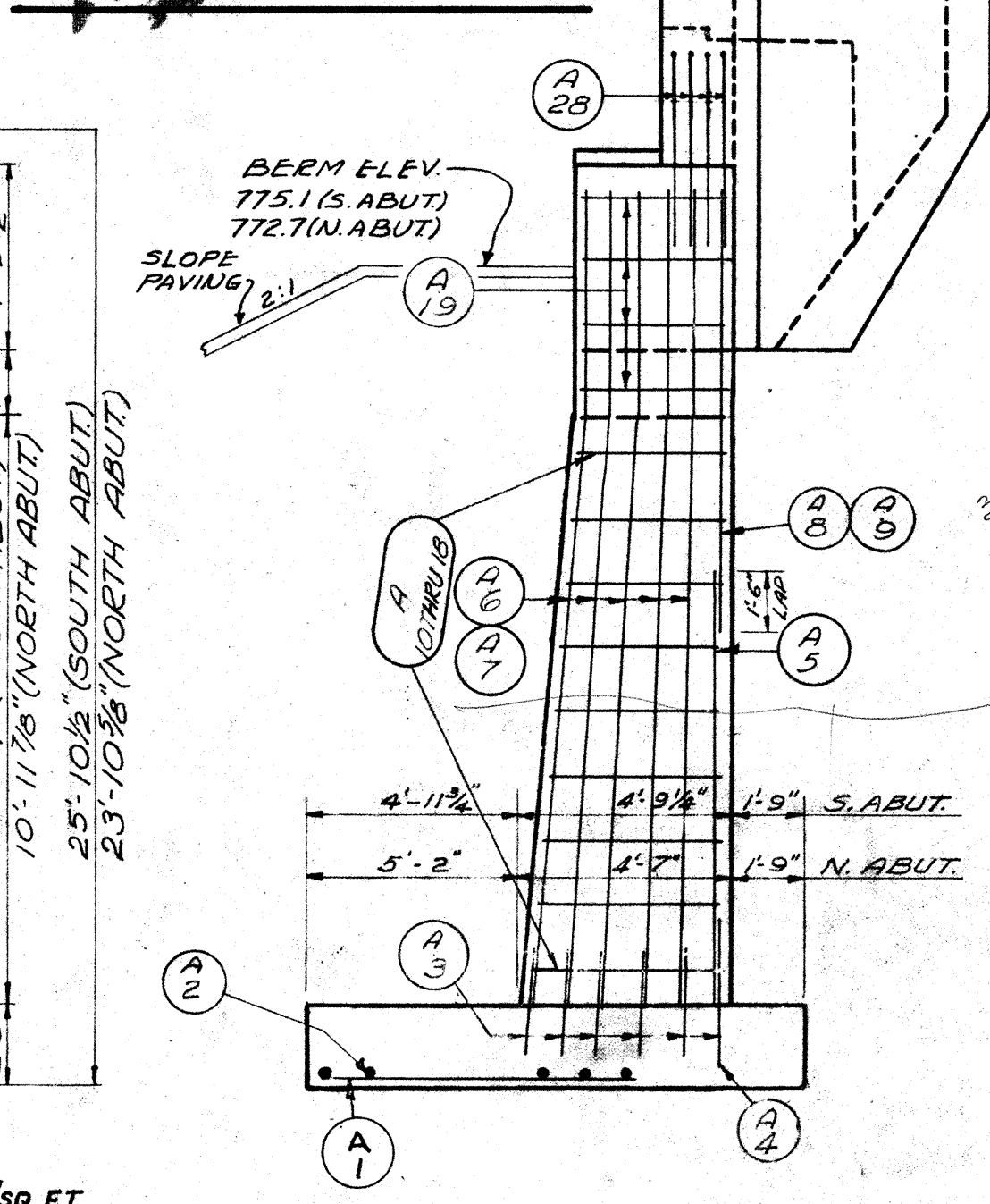
NOTE: SOIL BEARING PRESSURE = 4.0 TONS/SQ. FT.



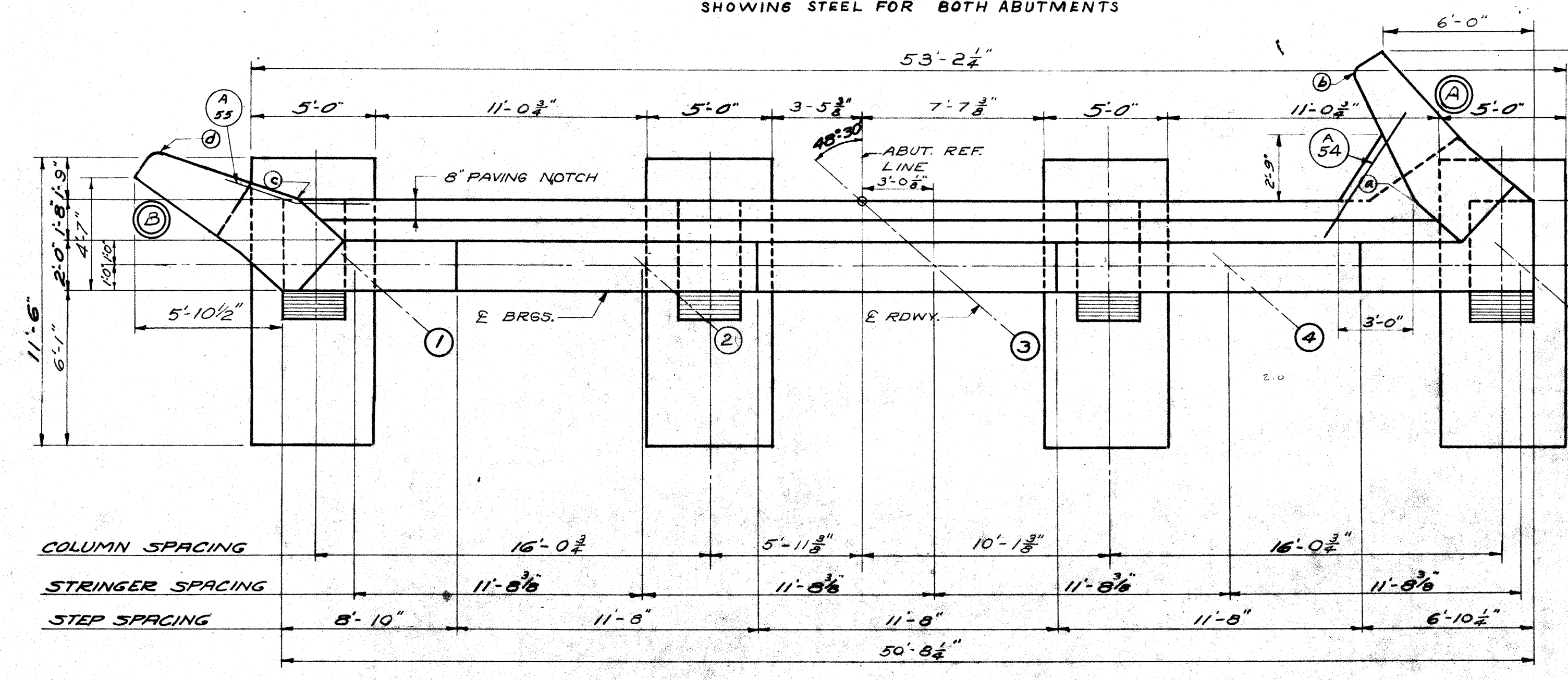
SECTION THRU GIRDER



SECTION THRU COLUMN



END VIEW

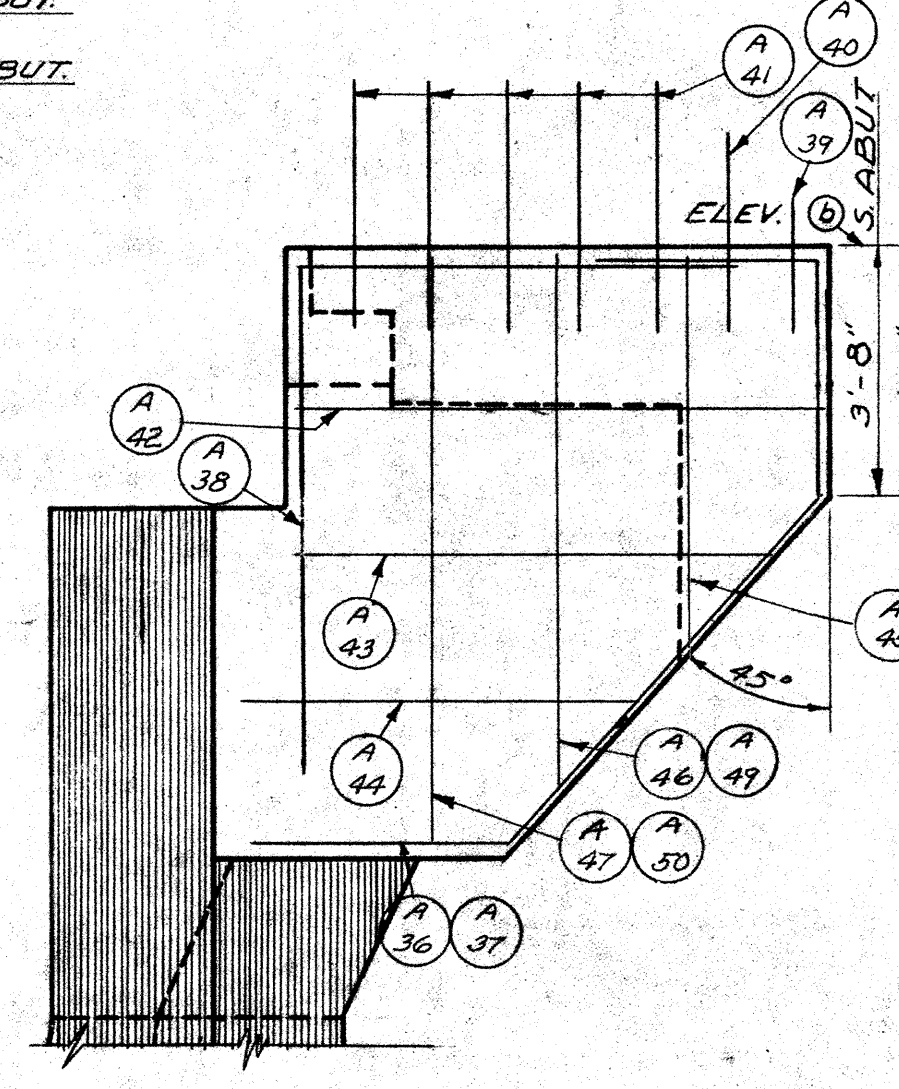


PLAN

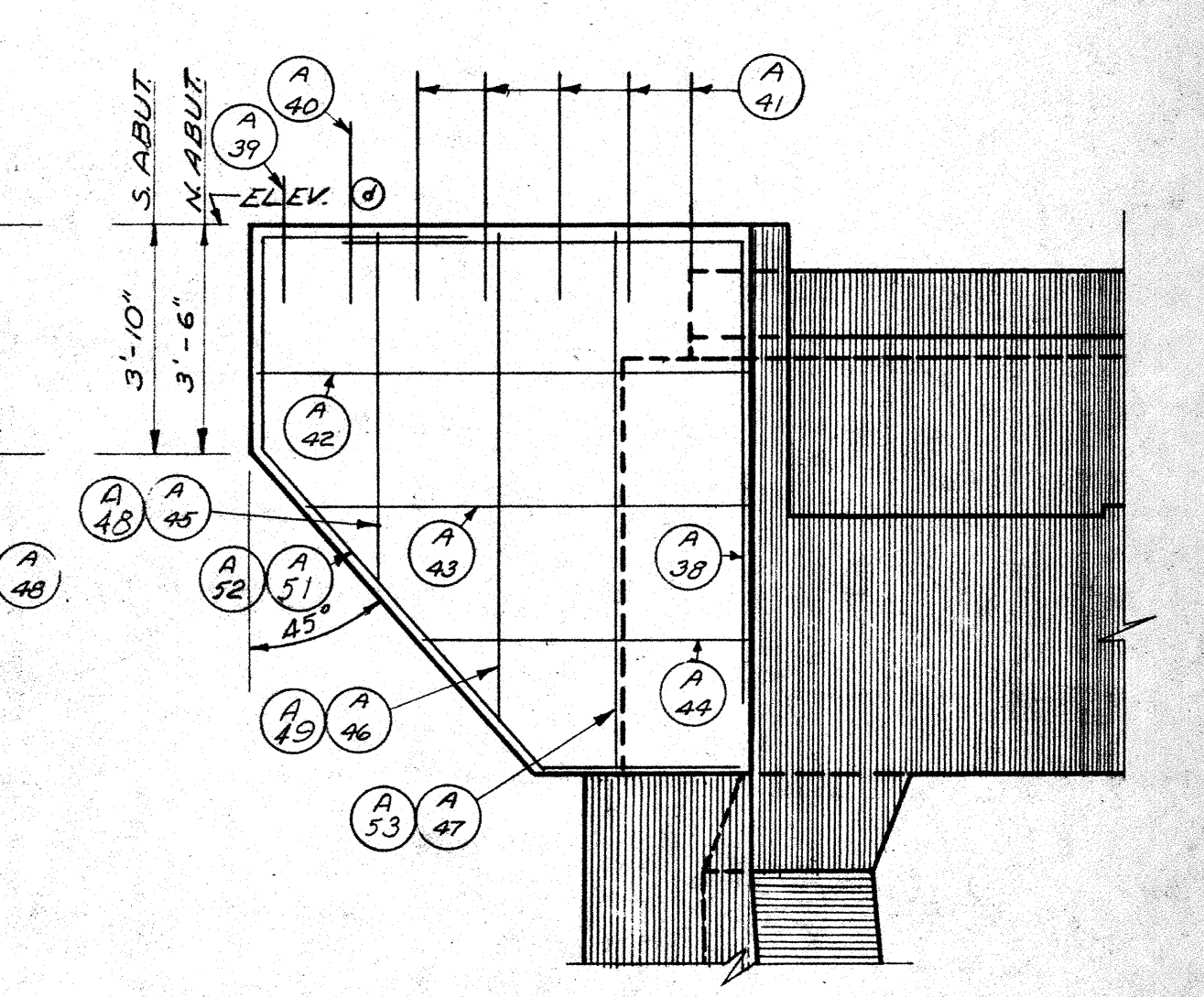
NORTH ABUTMENT SHOWN.
SOUTH ABUTMENT SIMILAR EXCEPT REVERSE STRINGER NOS.

POINT	S. ABUT.	N. ABUT.
A	781.87	779.89
B	781.91	779.85
C	782.02	779.43
D	782.06	779.38

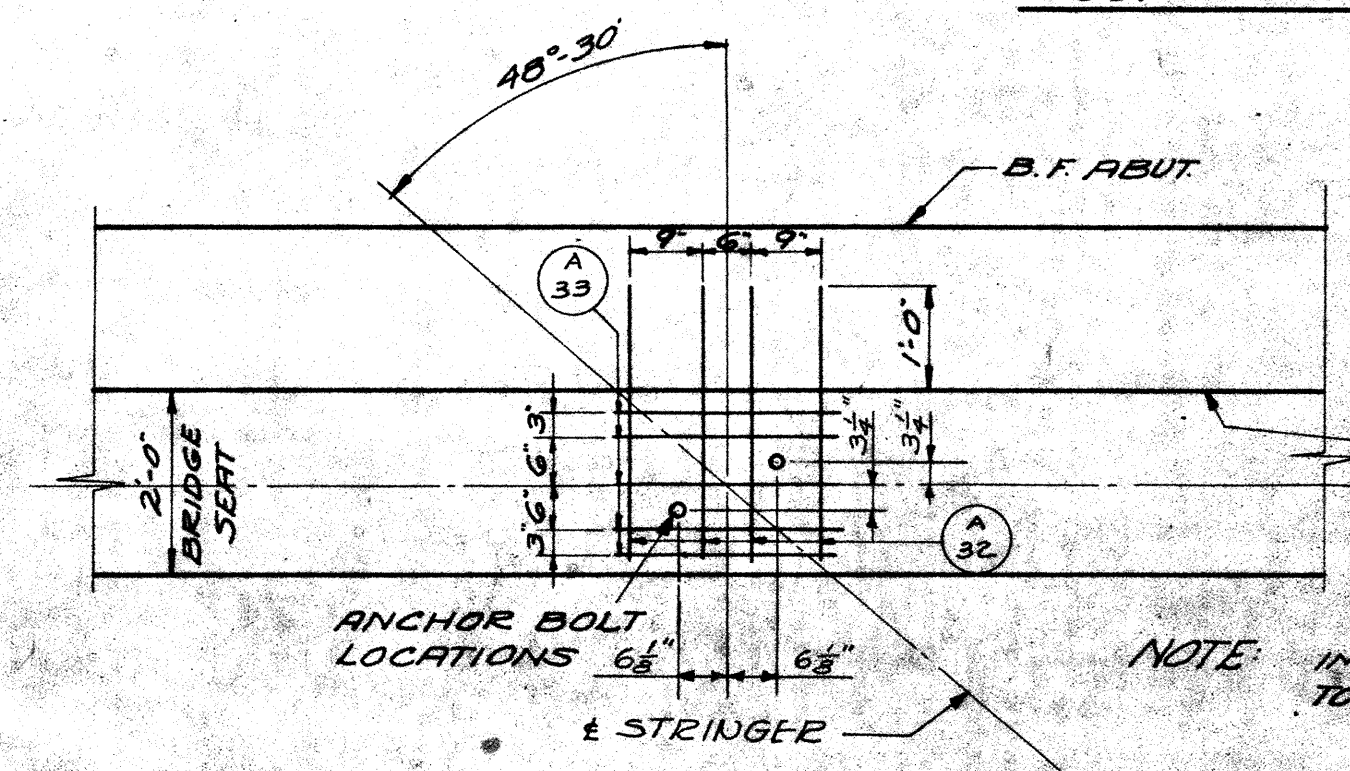
WING WALL ELEVATIONS



ELEVATION A

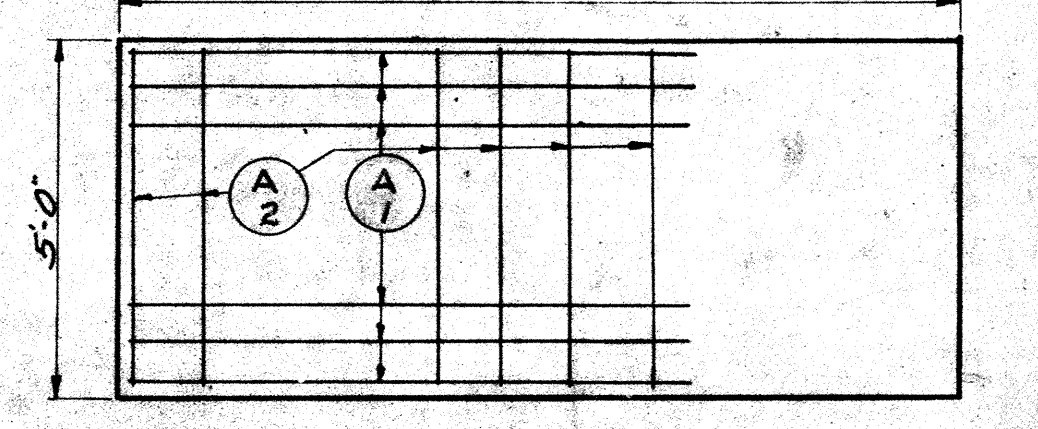


ELEVATION B



GRID DETAIL

WING WALL DETAILS



FOOTING PLAN
TYPICAL

STATE HIGHWAY COMMISSION OF WISCONSIN			
ABUTMENTS AND DETAILS			
DATE 7/23/57	DRAWN JWH	CHKD. HSB	SPEC. 1957
STRUCTURE B-70-27		SHEET 5 OF 8	

BILL OF BARS
DIMENSIONS IN BENDING ARE
OUT TO OUT

R. P. DIVISION	PROJECT	SHEET NO.	TOTAL SHEETS
4	F03-2(3) T03-2(30)	48	61

SUPERSTRUCTURE

(52760#)

THREE PIERS

(8530#)

TWO ABUTMENTS

(11,540#)

MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
S 1	6	5	3-6	6"	TRANSV. - TOP & BOT. - STAGGERED	
S 2	6	5	5-0	"	" " " " " " " "	
S 3	6	5	6-3	"	" " " " " " " "	
S 4	6	5	7-9	"	" " " " " " " "	
S 5	6	5	9-3	"	" " " " " " " "	
S 6	6	5	10-6	"	" " " " " " " "	
S 7	6	5	12-0	"	" " " " " " " "	
S 8	6	5	13-6	"	" " " " " " " "	
S 9	6	5	15-0	"	" " " " " " " "	
S 10	6	5	16-3	"	" " " " " " " "	
S 11	6	5	17-9	"	" " " " " " " "	
S 12	6	5	19-0	"	" " " " " " " "	
S 13	6	5	20-6	"	" " " " " " " "	
S 14	6	5	22-0	"	" " " " " " " "	
S 15	6	5	23-6	"	" " " " " " " "	
S 16	6	5	25-0	"	" " " " " " " "	
S 17	6	5	26-3	"	" " " " " " " "	
S 18	6	5	27-9	"	" " " " " " " "	
S 19	6	5	29-3	"	" " " " " " " "	
S 20	52	5	32-0	"	" " " " " " " "	
S 21	6	5	30-0	"	" " " " " " " "	
S 22	6	5	28-9	"	" " " " " " " "	
S 23	6	5	27-3	"	" " " " " " " "	
S 24	6	5	26-0	"	" " " " " " " "	
S 25	6	5	24-6	"	" " " " " " " "	
S 26	6	5	23-0	"	" " " " " " " "	
S 27	6	5	21-6	"	" " " " " " " "	
S 28	6	5	20-3	"	" " " " " " " "	
S 29	6	5	18-9	"	" " " " " " " "	
S 30	6	5	17-6	"	" " " " " " " "	
S 31	6	5	16-0	"	" " " " " " " "	
S 32	6	5	14-6	"	" " " " " " " "	
S 33	6	5	13-3	"	" " " " " " " "	
S 34	6	5	12-0	"	" " " " " " " "	
S 35	6	5	10-6	"	" " " " " " " "	
S 36	6	5	9-0	"	" " " " " " " "	
S 37	6	5	7-9	"	" " " " " " " "	
S 38	6	5	6-3	"	" " " " " " " "	
S 39	6	5	5-0	"	" " " " " " " "	
S 40	6	5	3-6	"	" " " " " " " "	
S 41	6	5	2-0	"	" " " " " " " "	
S 42	88	5	24-9	1-4	LONGIT. - BETWEEN STRINGERS - UPPER BARS STAGGERED WITH LOWER BARS	
S 43	98	4	4-0	1-0	TRANSV. - CURB & FLOOR	A
S 44	12	5	26-0	SHOWN	LONGIT. - CURB	
S 45	98	4	3-6	1-0	TRANSV. - CURB	B
S 46	24	5	2-3	SHOWN	CURB & RAILPOST	
S 47	24	5	3-0	"	CURB & RAILPOST	
S 48	9	6	6-0	"	EXP. JOINT - SYMM. ABT. STRINGERS	C
S 49	4	4	11-6	"	EXP. JOINT - BETWEEN STRINGERS	
S 20	426	5	32-0	6"	TRANSV. - TOP & BOT. - STAGGERED	
S 21	12	5	30-0	"	" " " " " " " "	
S 22	12	5	28-9	"	" " " " " " " "	
S 23	12	5	27-3	"	" " " " " " " "	
S 24	12	5	26-0	"	" " " " " " " "	
S 25	12	5	24-6	"	" " " " " " " "	
S 26	12	5	23-0	"	" " " " " " " "	
S 27	12	5	21-6	"	" " " " " " " "	
S 28	12	5	20-3	"	" " " " " " " "	
S 29	12	5	18-9	"	" " " " " " " "	
S 30	12	5	17-6	"	" " " " " " " "	
S 31	12	5	16-0	"	" " " " " " " "	
S 32	12	5	14-6	"	" " " " " " " "	
S 33	12	5	13-3	"	" " " " " " " "	
S 34	12	5	12-0	"	" " " " " " " "	
S 35	12	5	10-6	"	" " " " " " " "	
S 36	12	5	9-0	"	" " " " " " " "	
S 37	12	5	7-9	"	" " " " " " " "	
S 38	12	5	6-3	"	" " " " " " " "	
S 39	12	5	5-0	"	" " " " " " " "	
S 40	12	5	3-6	"	" " " " " " " "	
S 41	12	5	2-0	"	" " " " " " " "	
S 43	302	5	4-0	1-0	TRANSV. - CURB & FLOOR	A
S 45	302	5	3-6	1-0	" " " " " " " "	B
S 46	72	5	2-3	SHOWN	CURB & RAILPOST	
S 47	72	5	3-0	"	" " " " " " " "	
S 50	264	5	26-0	1-4	LONGIT. - BETWEEN STRINGERS - UPPER BARS STAGGERED WITH LOWER BARS	
S 51	10	5	30-0	SHOWN	LONGIT. - CENTER ON PIER 2	
S 52	36	5	27-0	"	" " " " " " " "	
S 53	4	5	30-0	"	" " " " " " " "	
S 54	40	7	11-3	"	STREUT AT PIERS 1 & 3 - BOT.	
S 55	16	4	11-3	"	" " " " " " " "	
S 56	96	4	5-6	1-0	" " " " " " " "	D
S 57	13	7	6-0	SHOWN	" " " " " " " "	
S 58	88	4	5-6	1-0	" " " " " " " "	J

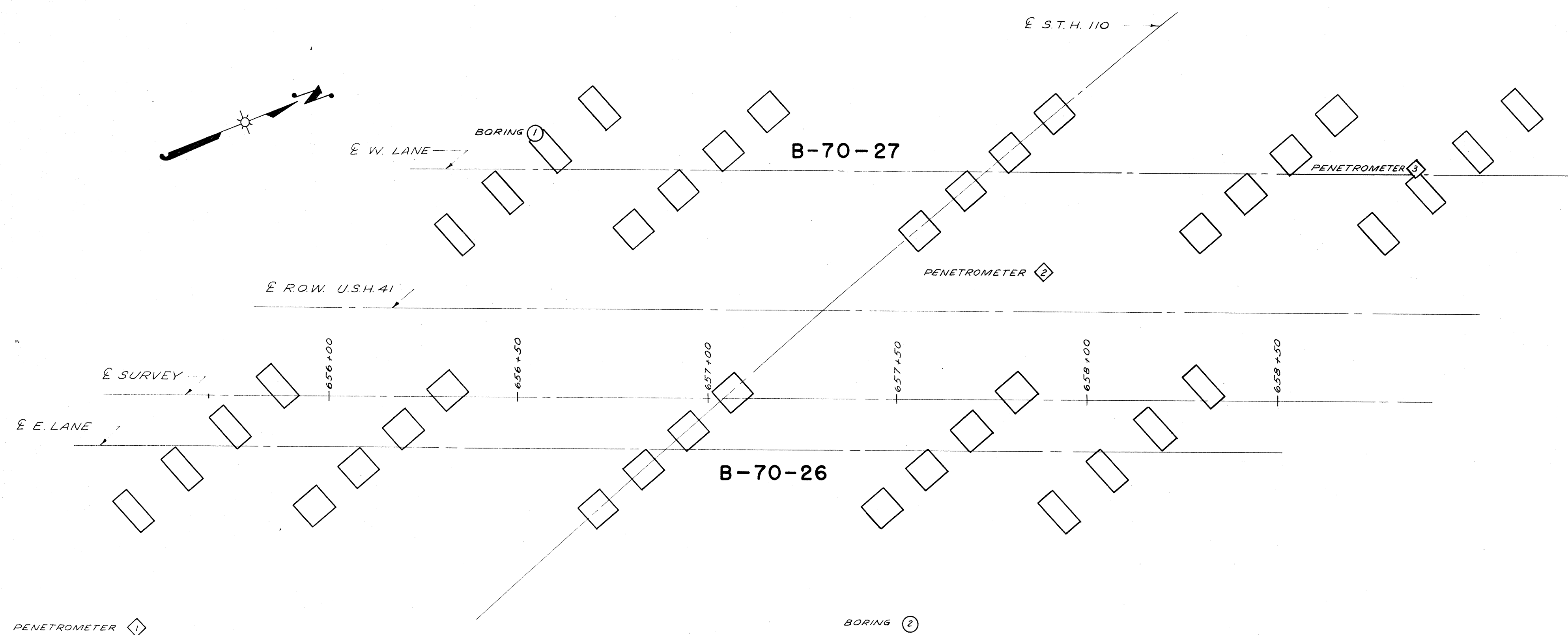
MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
S 1	6	5	3-6	6"	TRANSV. - TOP & BOT. - STAGGERED	
S 2	6	5	5-0	"	" " " " " " " "	
S 3	6	5	6-3	"	" " " " " " " "	
S 4	6	5	7-9	"	" " " " " " " "	
S 5	6	5	9-3	"	" " " " " " " "	
S 6	6	5	10-6	"	" " " " " " " "	
S 7	6	5	12-0	"	" " " " " " " "	
S 8	6	5	12-6	"	" " " " " " " "	
S 9	6	5	15-0	"	" " " " " " " "	
S 10	6	5	16-3	"	" " " " " " " "	
S 11	6	5	17-9	"	" " " " " " " "	
S 12	6	5	19-0	"	" " " " " " " "	
S 13	6	5	20-6	"	" " " " " " " "	
S 14	6	5	22-0	"	" " " " " " " "	
S 15	6	5	23-6	"	" " " " " " " "	
S 16	6	5	25-0	"	" " " " " " " "	
S 17	6	5	26-3	"	" " " " " " " "	
S 18	6	5	27-9	"	" " " " " " " "	
S 19	6	5	29-3	"	" " " " " " " "	
S 20	52	5	32-0	"	" " " " " " " "	
S 21	6	5	30-0	"	" " " " " " " "	
S 22	6	5	28-9	"	" " " " " " " "	
S 23	6	5	27-3	"	" " " " " " " "	
S 24	6	5	26-0	"	" " " " " " " "	
S 25	6	5	24-6	"	" " " " " " " "	
S 26	6	5	23-0	"	" " " " " " " "	
S 27	6	5	21-6	"	" " " " " " " "	
S 28	6	5	20-3	"	" " " " " " " "	
S 29	6	5	18-9	"	" " " " " " " "	
S 30	6	5	17-6	"	" " " " " " " "	
S 31	6	5	16-0	"	" " " " " " " "	
S 32	6	5	14-6	"	" " " " " " " "	
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S 34	6	5	12-0	"	" " " " " " " "	
S 35	6	5	10-6	"	" " " " " " " "	
S 36	6	5	9-0	"	" " " " " " " "	
S 37	6	5	7-9	"	" " " " " " " "	
S 38	6	5	6-3	"	" " " " " " " "	
S 39	6	5	5-0	"	" " " " " " " "	
S 40	6	5	3-6	"	" " " " " " " "	
S 41	6	5	2-0	"	" " " " " " " "	
S 42	88	5	24-9	1-4	LONGIT. - BETWEEN STRINGERS - UPPER BARS STAGGERED WITH LOWER BARS	
S 43	98	4	4-0	1-0	TRANSV. - CURB & FLOOR	A
S 44	12	5	26-0	SHOWN	LONGIT. - CURB	
S 45	98	4	3-6	1-0	TRANSV. - CURB	B
S 46	24	5	2-3	SHOWN	CURB & RAILPOST	
S 47	24	5	3-0	"	CURB & RAILPOST	
S 48	9	6	6-0	"	EXP. JOINT - SYMM. ABT. STRINGERS	C
S 49	4	4	11-6	"	EXP. JOINT - BETWEEN STRINGERS	
R 1	16	4	5-6	SHOWN	END POSTS - HORIZONTAL	
R 2	16	4	4-6	"	" " " " " " " "	
R 3	16	4	3-6	"	" " " " " " " "	
R 4	12	4	6-6	"	" " " " " " " "	D
R 5	130	3	2-9	"	RAIL POSTS - " " " "	D
R 6	32	5	25-6	"	RAILS - LONGIT. - SPANS 1 & 4	
R 7	48	5	28-6	"	" " " " " " " "	
R 8	198	2	2-3	1-0	" " " " " " " "	D

MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
P 1	80	4	5-6	6	FOOTINGS - LONGIT. - PIERS 1 & 3	
P 2	56	5	4-6	10	" " - TRANSV. - PIERS 1 & 3	
P 3	144	5	2-6	SHOWN	COLUMN DOWELS - ALL PIERS	
P 4	72	5	6-0	9	FOOTINGS - LONGIT. & TRANSV. - PIER 2	
P 5	8	5	15-6	SHOWN	COLUMNS - PIER 1	
P 6	16	5	17-0	"	" & GIRDER - PIER 1	
P 7	16	5	14-3	"	" - PIER 2 & 3	
P 8	32	5	16-0	"	" & GIRDER - PIER 2 & 3	
P 9	184	4	9-6	1-0	COL. TIPS (16 PIER 1, 15 PIERS 2 & 3 - EACH)	F
P 22	72	5	5-0	SHOWN	COLUMNS - ALL PIERS	
P 10	28	6	15-6	SHOWN	BOTTOM - END SPANS - PIERS 1 & 3	
P 11	14	7	15-6	"	" - CENTER SPAN - PIERS 1 & 3	
P 12	28	7	15-6	"	" - END SPANS - PIER 2	
P 13	14	8	15-6	"	" - CENTER - PIER 2	
P 14	28	8	8-0	"	TOP - OVER INT. COLUMNS - PIERS 1 & 3	
P 15	16	9	8-0	"	" " " " - PIER 2	
P 16	12	4	11-6	"	" - STIRRUPS TIPS - ALL PIERS	
P 17	6	4	9-9	"	" " " " " " " "	
P 18	42	4	11-6	2-0	STIRRUPS - PIERS 1 & 3	D
P 19	18	5	11-6	SHOWN	" - PIERS 2	D
P 20	60	4	4-6	"	GRID	F
P 21	60	4	2-3	"	"	

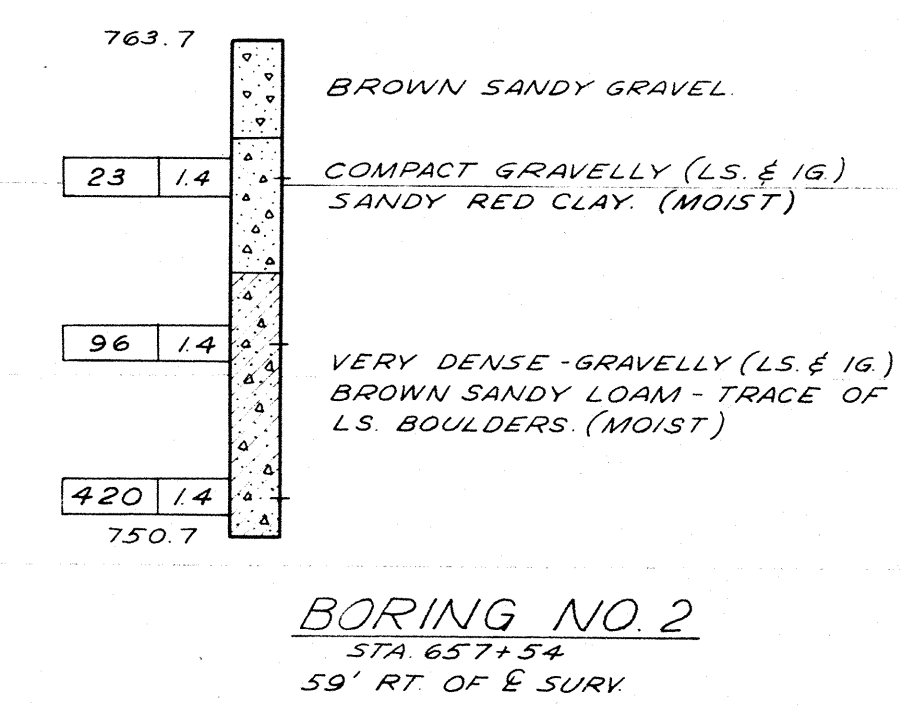
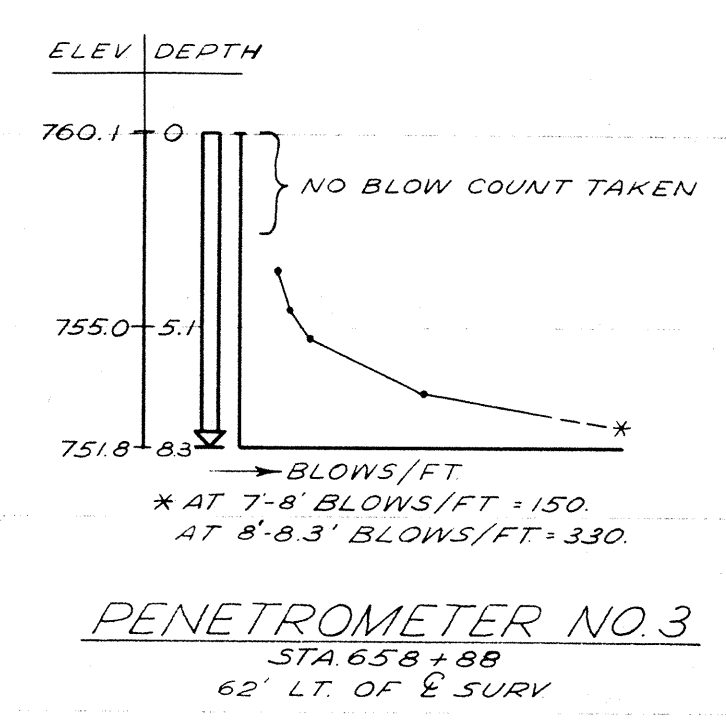
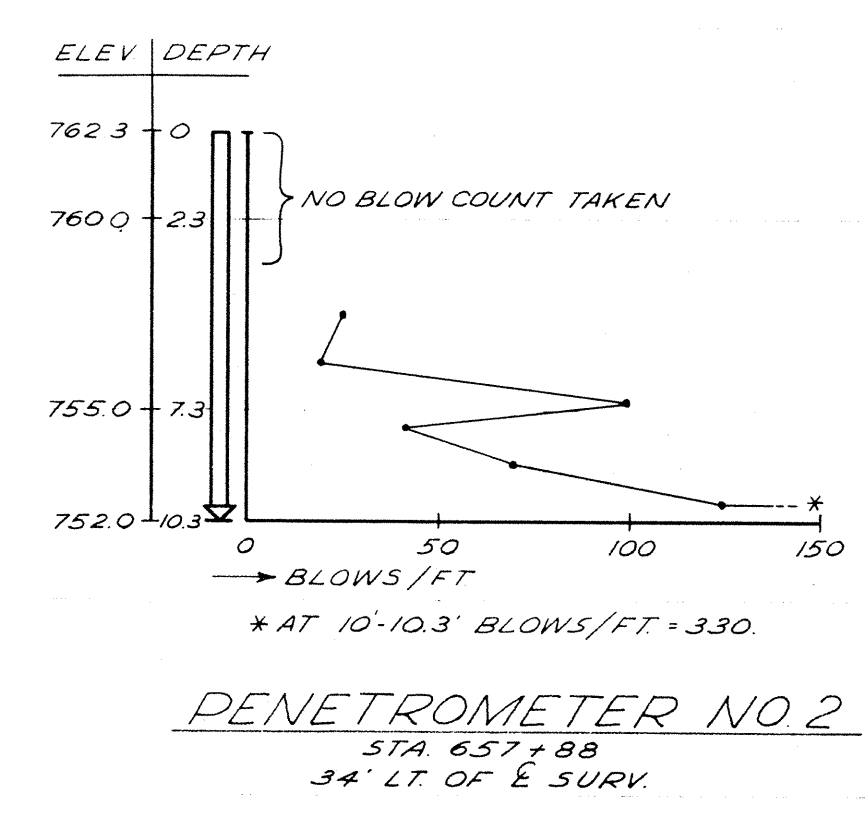
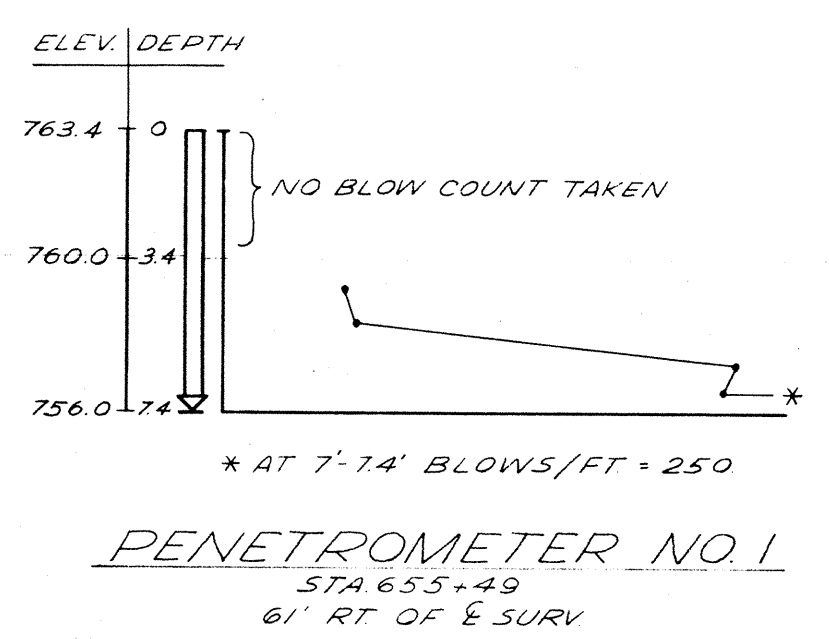
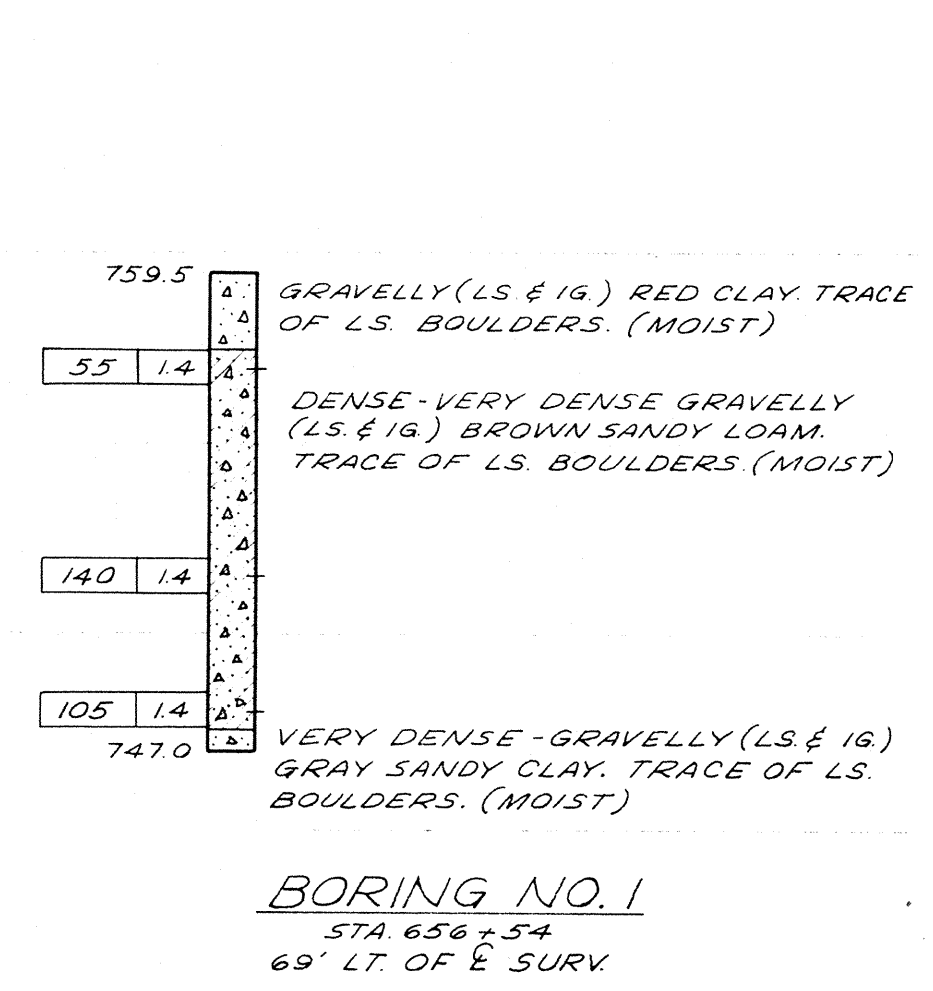
POUR	MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
A 1	88	8	7-6	5		FOOTING - TOE - BOT. - LONGIT.	
A 2	64	5	4-6	1-0	"	" " " " - TRANSV.	
A 3	96	5	2-6	SHOWN	"	" & COLUMN - FRONT & SIDES	
A 4	48	8	4-0	"	"	" " " " - BACK FACE	
A 5	48	8	10-0	SHOWN		COLUMN - VERT. - BOTH ABUTS.	
A 6	48	5	16-3	"	"	" " " " - N. ABUTMENT	*
A 7	48	5	18-6	"	"	" " " " - S. ABUTMENT	*
A 8	24	6	7-9	"	"	" " " " - N. ABUTMENT	
A 9	24	6	10-0	"	"	" " " " - S. " "	D
A 10	4	4	14-3	1-6	"	" - TIES - " " "	D
A 11	8	4	14-0	"	"	" " " " - BOTH ABUTS.	D
A 12	8	4	13-9	"	"	" " " " " " " "	D
A 13	8	4	13-6	"	"	" " " " " " " "	D
A 14	8	4	13-3	"	"	" " " " " " " "	D
A 15	8	4	13-0	"	"	" " " " " " " "	D
A 16	8	4	12-9	"	"	" " " " " " " "	D
A 17	8	4	12-6	"	"	" " " " " " " "	D
A 18	8	4	12-3	"	"	" " " " " " " "	D
A 19	32	4	12-0	"	"	" " " " " " " "	D
A 20	24	6	15-6	SHOWN		GIRDER - BOTTOM	
A 21	18	4	15-6	"	"	" - FRONT FACE	
A 22	12	4	8-6	"	"	" - BACK FACE @ INT. COLUMN	
A 23	12	4	5-6	"	"	" " " " " EXT. " "	
A 24	8	4	25-0	"	"	" " " " " CONT. " "	
A 25	24	4	16-3	3-0	"	" - STIRRUPS	D
A 26	16	7	9-0	SHOWN		PARAPET - TOP - INT. COLUMN	
A 27	16	5	9-6	"	"	" - EXT. " " "	G
A 28	8	4	9-0	"	"	" - STIRRUP SUPPORT	
A 29	4	4	9-3	"	"	" " " " " " " "	
A 30	88	4	12-6	1-0	"	" - STIRRUPS	F
A 31	88	4	5-0	1-0	"	" - BLOCK STIRRUPS	F
A 32	40	4	3-9	SHOWN		LONGIT. GRID BARS	G
A 33	50	4	2-3	"	"	TRANSV. " " "	
A 34	16	4	11-0	"	"	EXP. JOINT SUPP. (NO LAP REQ.)	
A 35	48	5	5-0	1-1	"	BRANCHES	
A 36	2	4	17-0	SHOWN		WING A'S ABUT	H
A 37	2	4	17-6	"	"	WING A-N. ABUT	H
A 38	12	4	13-0	"	"	WING A & B - BOTH ABUTS.	G
A 39	8	5					

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN

FOR THE DESIGN OF THE STRUCTURE FOUNDATION, TO OBTAIN RELATIVE DATA CONCERNING THE CHARACTER OF MATERIAL IN AND UPON WHICH THE FOUNDATION MIGHT BE BUILT, BORINGS AND/OR SOUNDINGS WERE MADE AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING WITH THE LOG OF SUCH EXPLORATION DATA AS INTERPRETED FOR SUCH DESIGN PURPOSE AS SHOWN. THE EXPLORATIONS WERE MADE BY ORDINARY AND CONVENTIONAL METHODS AND CARE DEEMED ADEQUATE FOR SUCH PURPOSE. HOWEVER, SINCE IT IS A MATTER OF COMMON KNOWLEDGE THAT THE EXACT CHARACTER OF ANY MATERIAL AND ITS REACTION IS DIFFICULT TO DETERMINE FROM SUCH SUBSURFACE EXPLORATION AND THAT THE KIND AND CHARACTER OF MATERIAL AT THE SITE WHERE THE FOUNDATIONS ARE BUILT MAY VARY SUBSTANTIALLY FROM THAT INDICATED BY THE LOG, THEY ARE MADE AVAILABLE TO THE BIDDERS SIMPLY FOR WHAT THEY ARE WORTH, WITHOUT ANY WARRANTY, EXPRESS OR IMPLIED, THAT THE MATERIAL TO BE ENCOUNTERED IN BUILDING THE FOUNDATION WILL CONFORM THEREIN. IF THE LOG IS USED BY THE CONTRACTOR IN MAKING HIS BID, IT IS HEREBY EXPRESSLY STIPULATED THAT THE COMMISSION ACCEPTS NO RESPONSIBILITY FOR SAID USE. UNLESS OTHERWISE SPECIFIED, THE BLOWS PER FOOT AT THE LOCATIONS INDICATED ARE BASED ON DRIVING A 2" OD x 14" ID SPLIT SPOON SAMPLER WITH A 140 LB. HAMMER HAVING A FREE FALL OF 30". THE BLOW COUNT IS TAKEN IN UNDISTURBED SOIL IMMEDIATELY BELOW A CASED OR OPEN HOLE ELIMINATING SIDE FRICTION ON THE DRIVE PIPE.

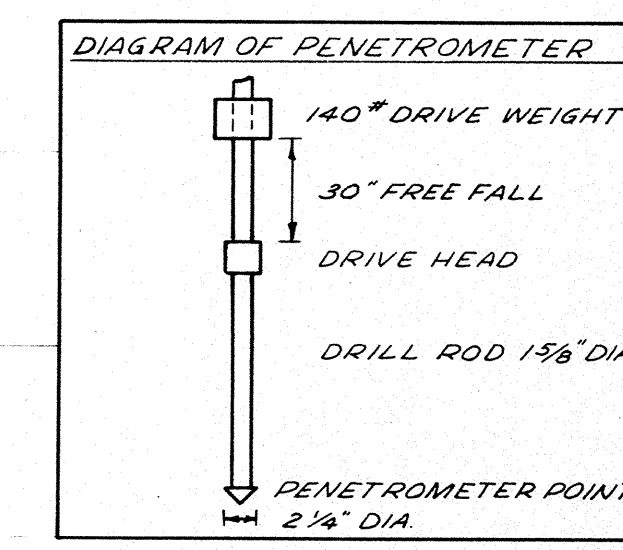


765
760
755
750
745
740



LOG OF BORING OPERATIONS

GROUND ELEV.	DESCRIPTION OF MATERIAL	SIZE OF SAMPLER	LOCATION OF BLOW COUNT
763.7	BROWN SANDY GRAVEL	(N) 7	14
760.1	COMPACT GRAVELLY (LS & IG) SANDY RED CLAY. (MOIST)	(N) 7	14
750.7	VERY DENSE- GRAVELLY (LS & IG) BROWN SANDY LOAM - TRACE OF LS. BOULDERS. (MOIST)	(N) 7	14



STATE HIGHWAY COMMISSION OF WISCONSIN

LOG OF SOUNDINGS

DATE 9-23-57 DRAWN C.V.L. CKD JDH SPEC 1957 LOAD H20-516

STRUCTURE **B-70-26/27** SHEET **8** OF **8**

COUNTY & HIGHWAY	ROUTE & SECTION	CLASS & AGREEMENT	FEDERAL	B. P. R. DIVISION	PROJECT	SHEET NO.	TOTAL SHEETS
70.1	3.2	STATE	11.33	4	705-273	50	61

BILL OF BARS

DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT

POUR	MARK	NO.	SIZE	LENGTH	SPACING	LOCATION	DET.
180	C1	5	5-0	6 1/2		FLOOR + PAVEMENT	
20	C2	4	24-9	1-0		"	
98	C5	4	4-9	1-0		SIDES	
12	C6	4	25-9	1-6		"	
2	C8	4	5-3	1-0		APRON	
2	C9	4	6-6	1-0		"	
2	C10	4	7-6	1-0		"	
4	C11	4	12-0	1-0		"	*
1	C12	4	8-9	1-0		"	*
1	C13	4	9-0	1-0		"	*
4	C14	4	5-0	1-0		WING 2+1 VERT	*
2	C15	4	5-6	1-0		" 2	*
2	C16	4	6-6	1-0		" 2	*
1	C17	4	4-0	1-6		" 2 HOR.	*
2	C18	4	7-0	1-6		" 2	*
2	C19	4	8-0	SHOWN		" 2 TOP	*
2	C20	4	9-0	"		" 1	*
2	C21	4	6-0	1-0		" 1 VERT.	*
2	C22	4	6-9	1-0		" 1	*
1	C23	4	4-6	1-6		" 1 HOR.	*
2	C24	4	8-9	1-6		" 1	*
4	C25	4	6-9	SHOWN		APRON	*
5	C27	4	8-0	1-0		CUTOFF	*
173	C1	5	5-0	6 1/2		FLOOR + PAVEMENT	
10	C3	4	25-3	1-0		FLOOR	
10	C4	4	22-3	1-0		PAVEMENT	
88	C5	4	4-9	1-0		SIDES	
12	C7	4	22-3	1-6		"	
4	C28	4	5-0	SHOWN		HEADER WALL	
18	C26	5	4-0	1-0		DOWELS	

* SEE "BAR DETAIL"

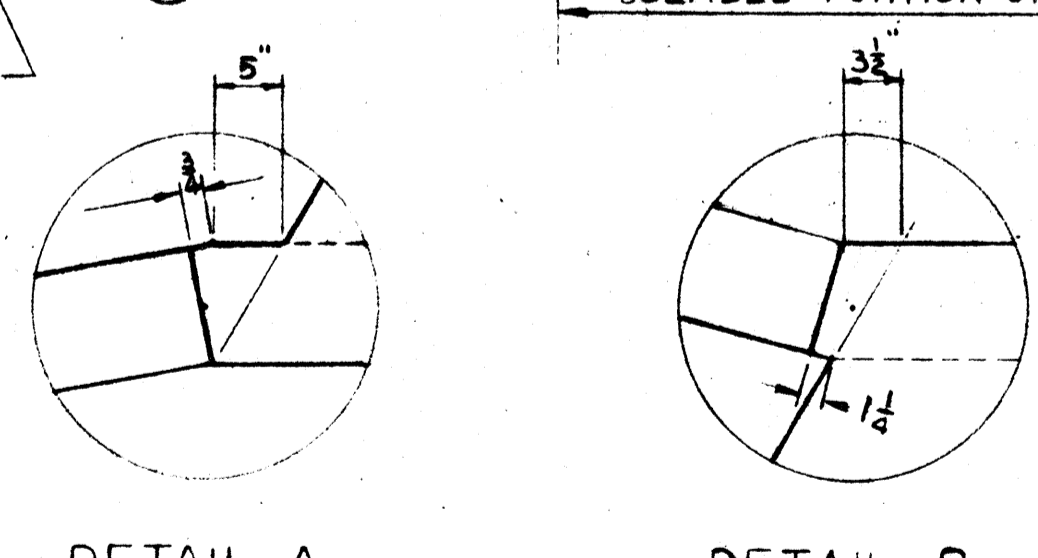
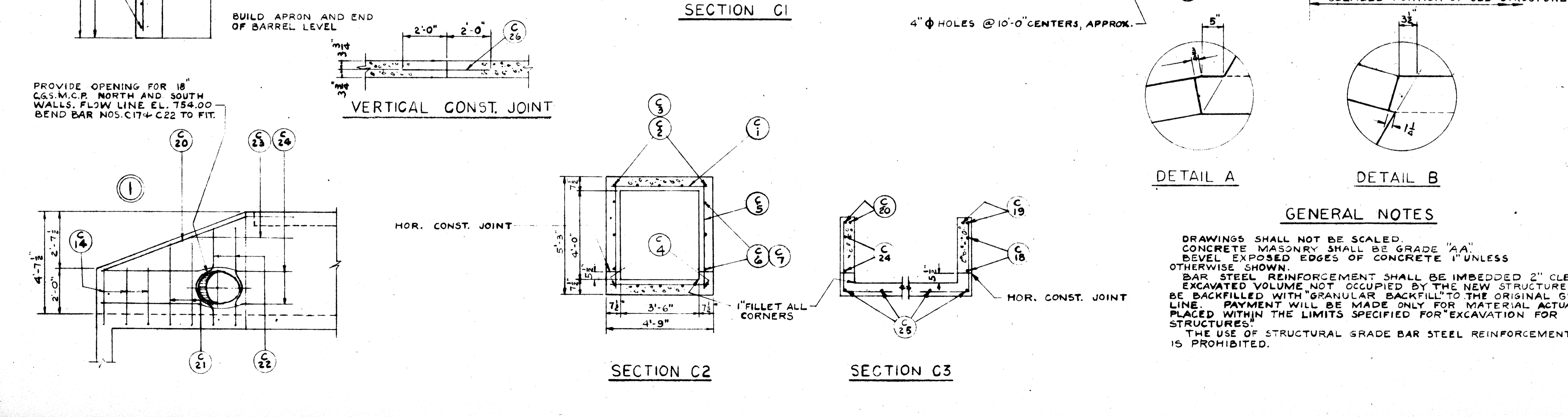
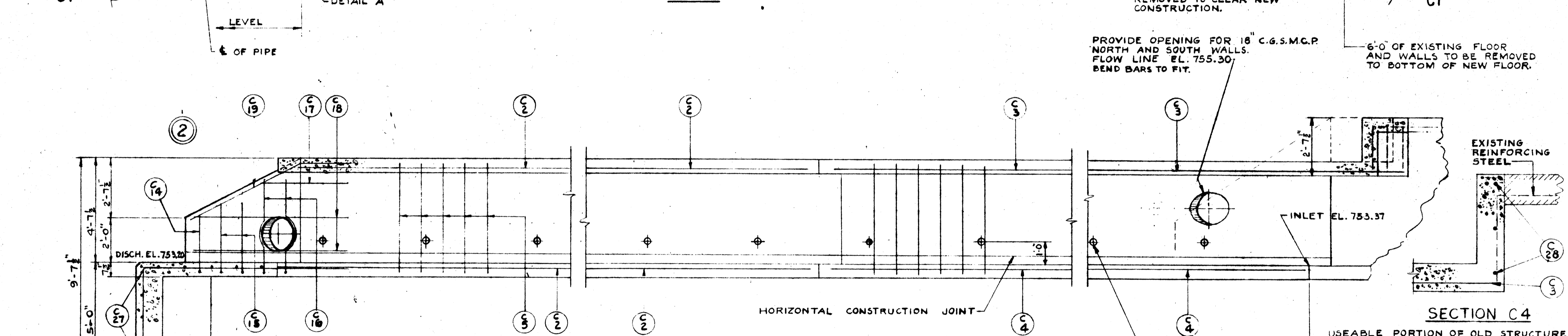
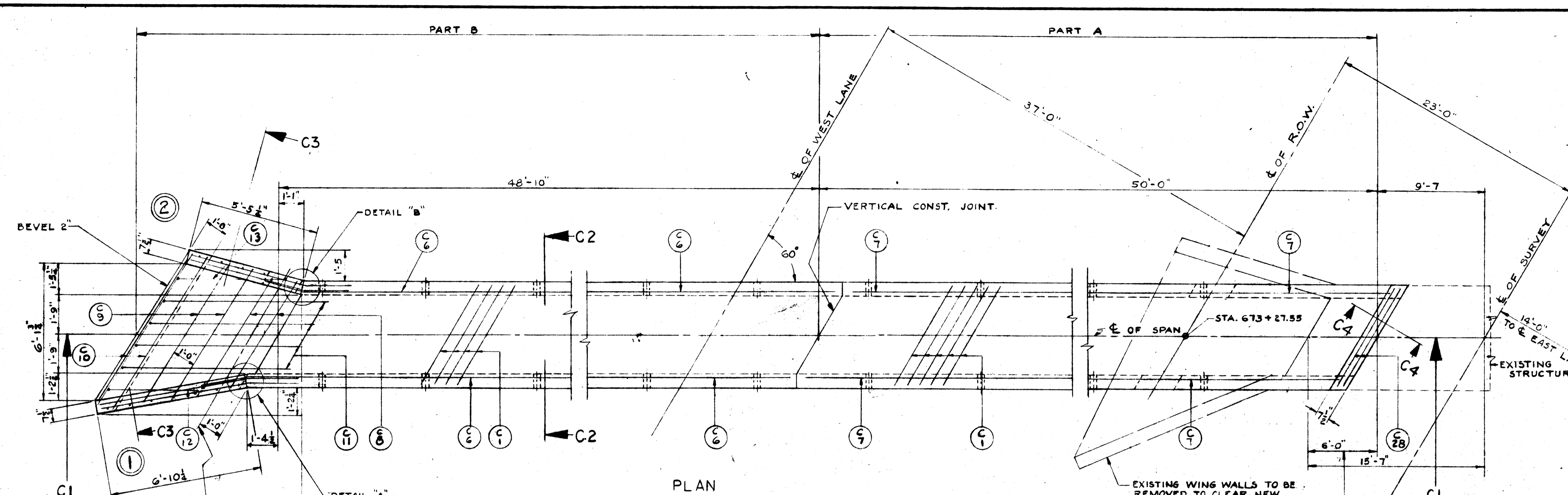
BAR DETAIL

BAR	A	B
C11	4-6	7-6
C12	4-6	4-3
C13	4-6	4-6
C14	2-6	2-6
C15	3-0	2-6
C16	4-0	2-6
C21	3-6	2-6
C22	4-3	2-6

TOTAL ESTIMATED QUANTITIES

BID ITEMS	QUANTITY
EXCAVATION FOR STRUCTURES	40 C.Y.
GRANULAR BACKFILL	20 C.Y.
CONCRETE MASONRY	41.8 C.Y.
BAR STEEL REINFORCEMENT	3760*

REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN		
	DETAILS		
	CO-WINNEBAGO	OSHKOSH	STA. 673+27.55
	SECTION 3	TOWN 18 N	RANGE 16 E
	DESIGN SPEC. 1953 AASHO	LOADING H20S16	CONSTR. SPEC. 1957
	DATE 7-24-57	DESIGN	DRAWN HVVZ
	SUBMITTED	W. B. Schult	
	APPROVED	E. B. Ruettim	
	STRUCTURE C-70-16		
	SHEET 1 OF		



GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.
 CONCRETE MASONRY SHALL BE GRADE "AA".
 LEVEL EXPOSED EDGES OF CONCRETE 1" UNLESS OTHERWISE SHOWN.
 BAR STEEL REINFORCEMENT SHALL BE IMBEDDED 2" CLEAR.
 EXCAVATED VOLUME NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH "GRANULAR BACKFILL" TO THE ORIGINAL GROUND LINE. PAYMENT WILL BE MADE ONLY FOR MATERIAL ACTUALLY PLACED WITHIN THE LIMITS SPECIFIED FOR "EXCAVATION FOR STRUCTURES."
 THE USE OF STRUCTURAL GRADE BAR STEEL REINFORCEMENT IS PROHIBITED.

ORIGINAL STRUCTURE DESIGNED FOR 1-0 FILL.

B.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	F 03-2 (33)	52	61

FINAL SURVEY SURVEYED: _____ DATE: _____
 NOTE BOOK: _____ TEMPLATE: _____
 AREAS CHECKED: _____
 NO. _____

ORIGINAL SURVEY SURVEYED: _____ DATE: _____
 NOTE BOOK: _____ TEMPLATE: _____
 AREAS CHECKED: _____
 NO. 10752

CONSTR. G



STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
649			
	357		0
650			
	208		0
651			
	70		56
652			
	424		56
653			
	680		0
654			
	346		7
655			
	269		7
656			
	196		0
+50			
	204		0
657			

SHEET TOTAL 2,754 126

B.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	F03-2 (33)	51	61

FINAL SURVEY SURVEYED BY DATE
 NOTE BOOK TEMPLATE NO. 10752 AREAS CHECKED

ORIGINAL SURVEY SURVEYED BY DATE
 NOTE BOOK TEMPLATE NO. 10752 AREAS CHECKED

CONSTR. C

649
755

648
755

647
755

646
755

645
750

644
750

643
755

642
755

641
755

SCALE 1" = 5'

STATION	DISTANCE	YARDAGE	
		EXCAVATION	
		UNCL.	FILL
641			
642	0		0
643	0		237
644	2		498
645	6		456
646	19		287
647	48		107
648	204		15
649	380		0
SHEET TOTAL		659	1,600

B.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	F 03-2 (33)	54	61

FINAL SURVEY DRAWING
 DATE: _____
 BY: _____
 NO. 10752

ORIGINAL SURVEY DRAWING
 DATE: 6-56
 BY: R.L.J.
 NO. 10752

CONSTR. C

673
755

672
755

671
755

670
755

669
755

668
755

667
755

666
755

665
755

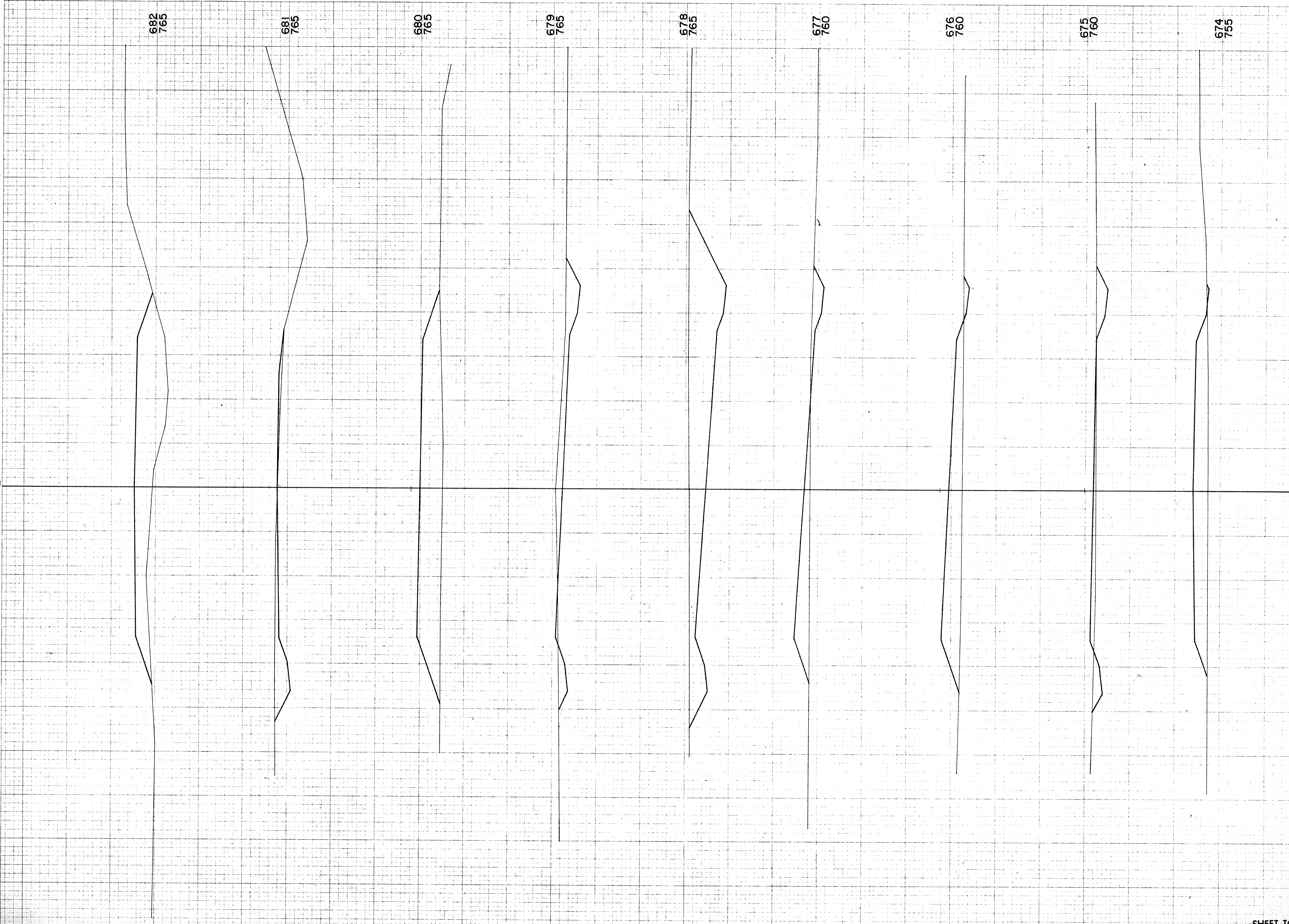
STATION	DISTANCE	YARDAGE		
		EXCAVATION		FILL
		UNCL.		
664				
	28			228
665	61			137
666	91			19
667	57			69
668	120			59
669	126			48
670	69			65
671	61			119
672	19			254
673				
SHEET TOTAL		632		998

B.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	F03-2(33)	55	61

FINAL SURVEY
 DATE: _____
 BY: _____
 NOTE BOOK/TABLET NO. _____
 DATE CHECKED _____

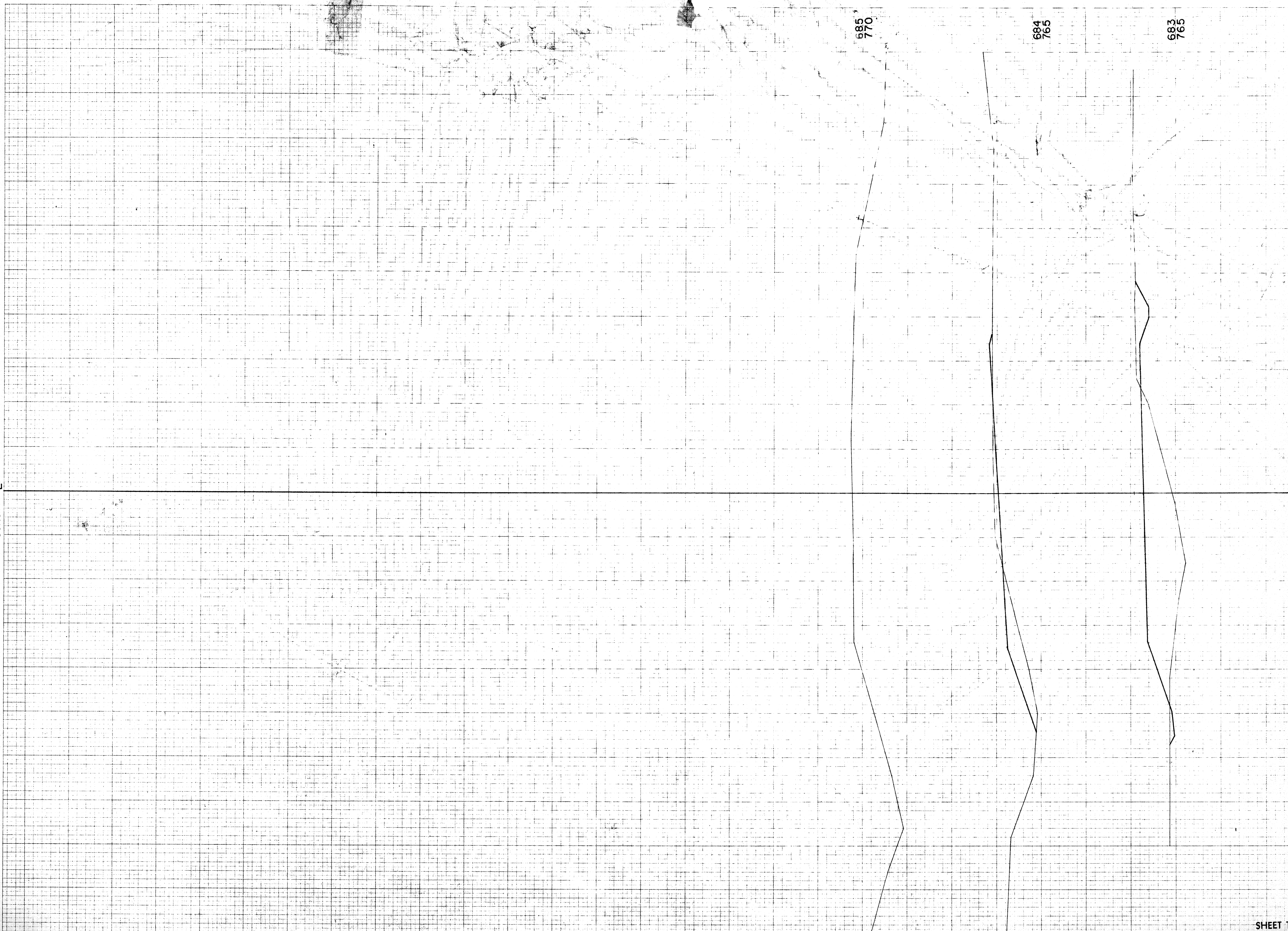
ORIGINAL SURVEY
 DATE: 6-56
 BY: R.L.J.
 DATE: 5-57
 BY: F.J.B.
 DATE: 1-58
 BY: T.S.G.
 DATE: 9-57

CONSTR. C



STATION	DISTANCE	YARDAGE		
		EXCAVATION		FILL
		UNCL.		
673				
674	0			261
675	17			132
676	20			156
677	19			191
678	230			57
679	274			2
680	59			191
681	33			198
682	33			185
SHEET TOTAL		685		1,373

B.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	F03-2 (33)	56	61



STATION	DISTANCE	YARDAGE		
		EXCAVATION		FILL
		UNCL.		
682				
683	19			350
684	33			213
685	15			39
SHEET TOTAL		67		602

FINAL SURVEY
 SURVEYED BY: _____
 PLOTTED BY: _____
 DATE: _____
 NOTE BOOK TEMPLATE NO. _____
 AREAS CHECKED: _____

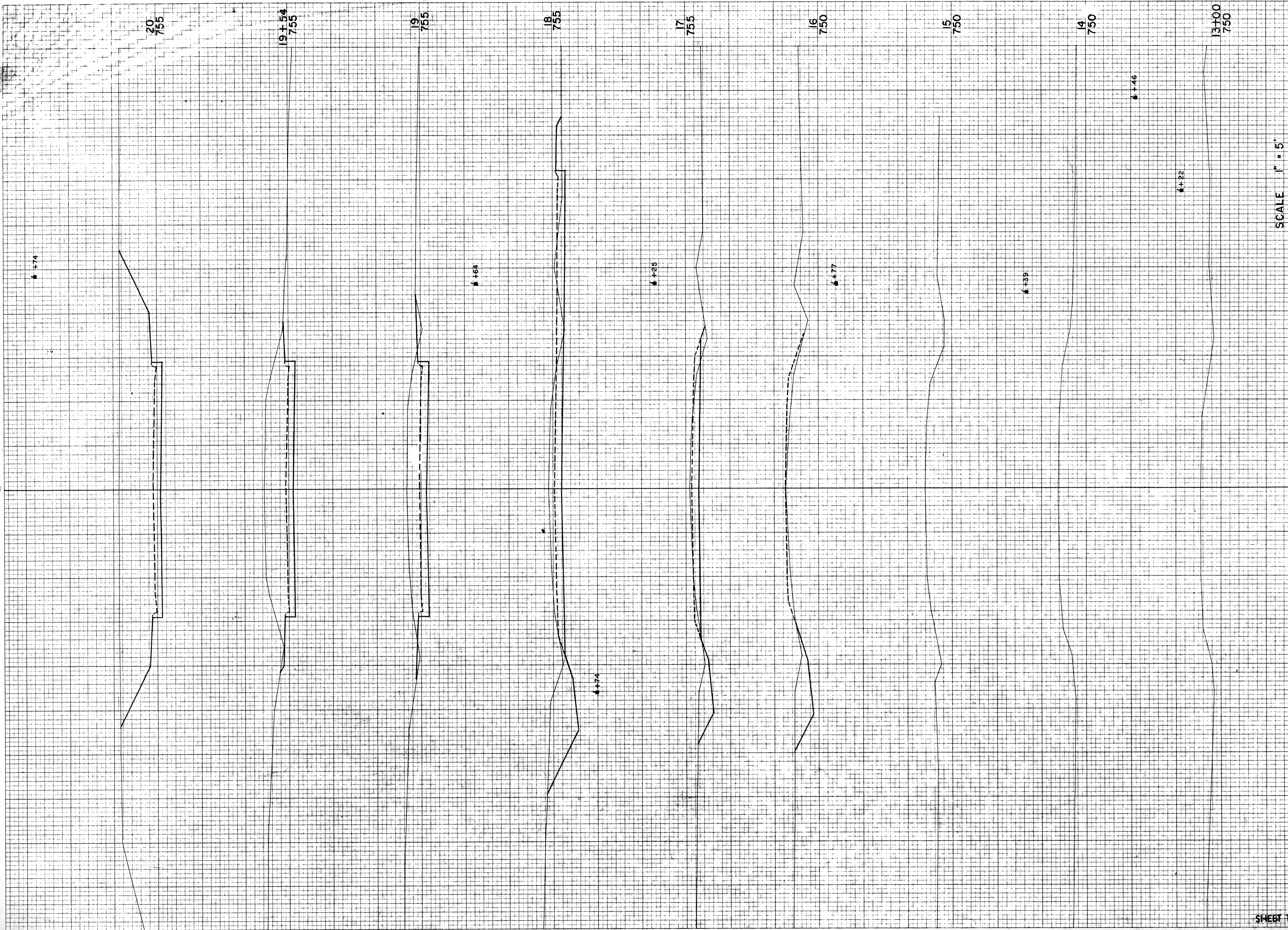
ORIGINAL SURVEY
 SURVEYED BY: _____
 PLOTTED BY: _____
 DATE: _____
 NOTE BOOK TEMPLATE NO. 10782
 AREAS CHECKED: _____

CONSTR. C.

B.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	F 03-2(33)	57	61

FINAL SURVEY SURVEYED BY DATE
 NO. 10753 SURVEYED BY DATE
 NOTE BOOK NO. 10753 PLOTTED BY DATE
 AREAS CHECKED AREAS CHECKED

ORIGINAL SURVEY SURVEYED BY DATE
 NO. 10753 SURVEYED BY DATE
 NOTE BOOK NO. 10753 PLOTTED BY DATE
 AREAS CHECKED AREAS CHECKED



SCALE 1" = 5'

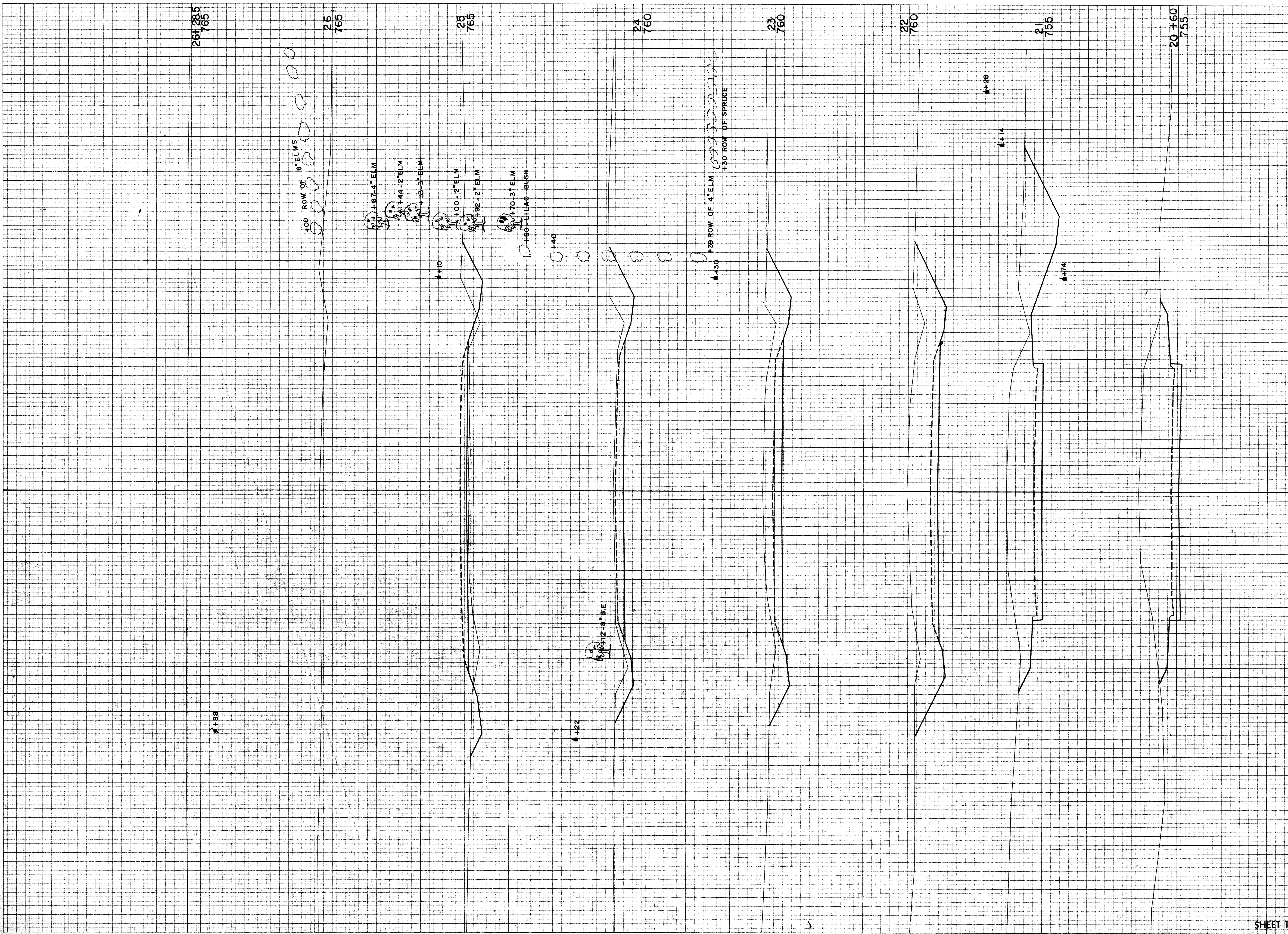
STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
16	48		4
17	150		4
18	219		6
19	159		4
+54	256		0
20			

SHEET TOTAL 832 18

FINAL SURVEY
 DATE: _____
 BY: _____
 REVIEWED: _____
 PLOTTED: _____
 NOTE BOOK NO. _____
 AREAS CHECKED: _____

ORIGINAL SURVEY
 DATE: _____
 BY: _____
 REVIEWED: _____
 PLOTTED: _____
 NOTE BOOK NO. 10759
 AREAS CHECKED: _____

B.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	F03-2 (33)	58	61



STATION	DISTANCE	YARDAGE		
		UNCL.	EXCAVATION	FILL
20		385		0
+60		236		0
21		606		0
22		454		0
23		278		0
24		143		7
25		37		7
26				
SHEET TOTAL		2139		14

SUNSET LANE TOTAL 2971 32

SURV.
℄

TEMP ROAD STH. 110

WIS. 4	F 03-2 (33)	59	61
B.P.R. DISTRICT OFFICE		SHEET NUMBER	TOTAL SHEETS

FINAL SURVEY
NOTE BOOK NO. _____

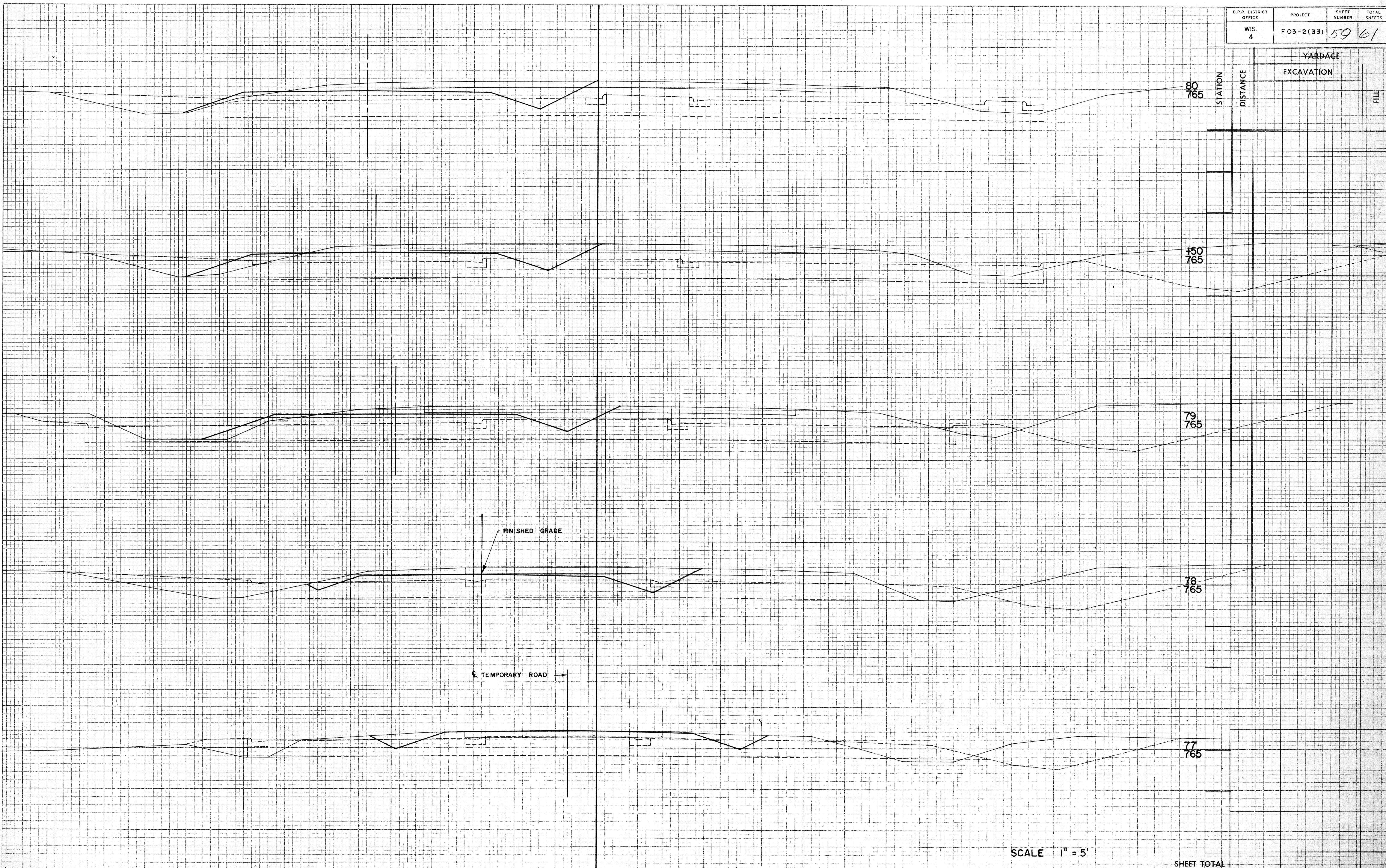
SURVEYED, PLOTTED, TEMPLATE, AREAS CHECKED

BY _____ DATE _____

ORIGINAL SURVEY
NOTE BOOK NO. 10752

SURVEYED, PLOTTED, TEMPLATE, AREAS CHECKED

BY R. J. R. DATE 6-56



STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL

SCALE 1" = 5'

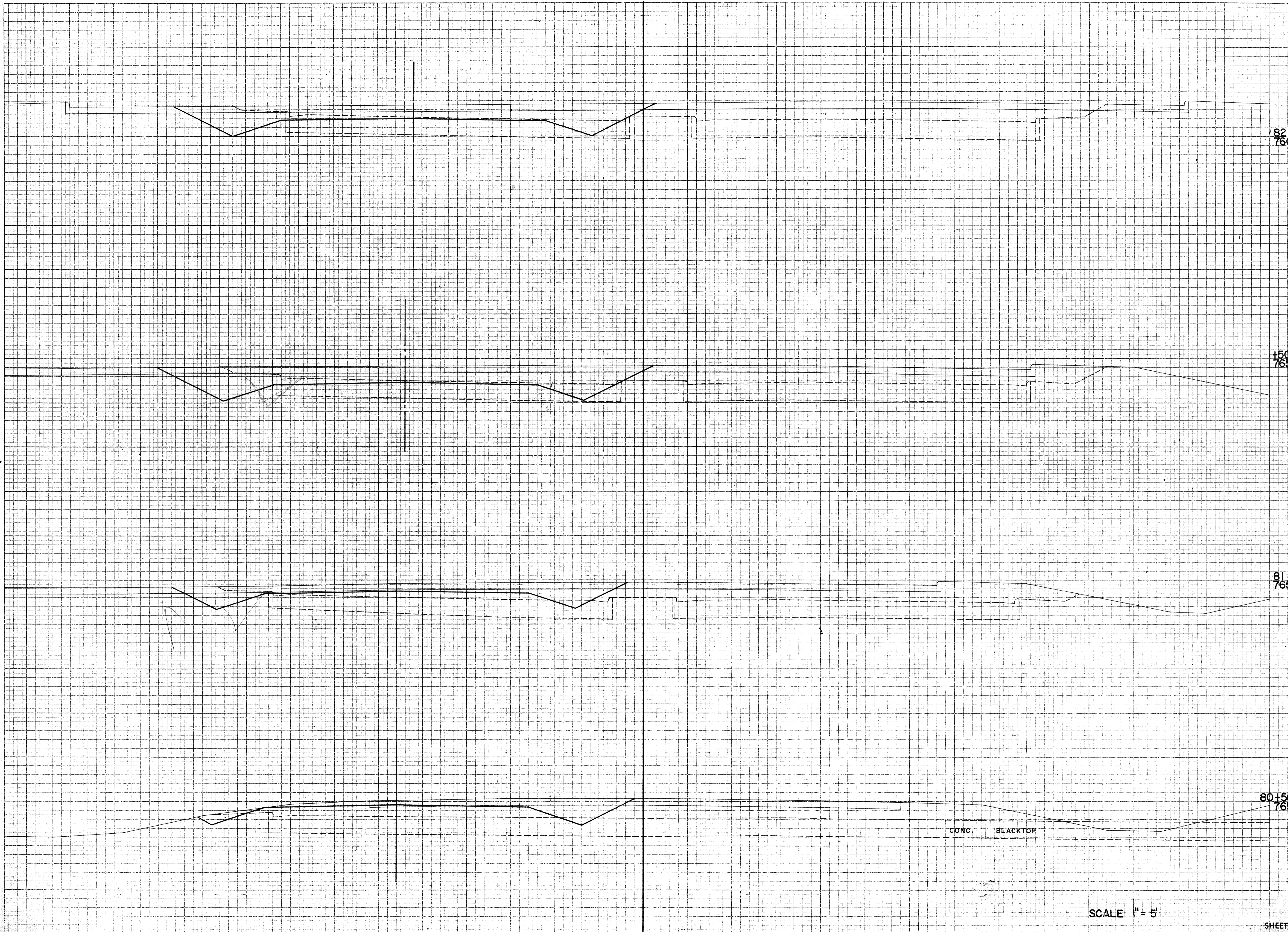
SHEET TOTAL

PLATE 3 CROSS SECTION O.P.R. & R.E.
1/4" = 1' HORIZONTAL AND VERTICAL
CROSS SECTION

B.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	F 03-2 (33)	60	61

FINAL SURVEY	DATE
SURVEYED	BY
AREAS CHECKED	
NOTE BOOK NO.	

ORIGINAL SURVEY	DATE
SURVEYED	BY
AREAS CHECKED	
NOTE BOOK NO.	



STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
82+760			
150+765			
81+765			
80+150+765			
SHEET TOTAL			

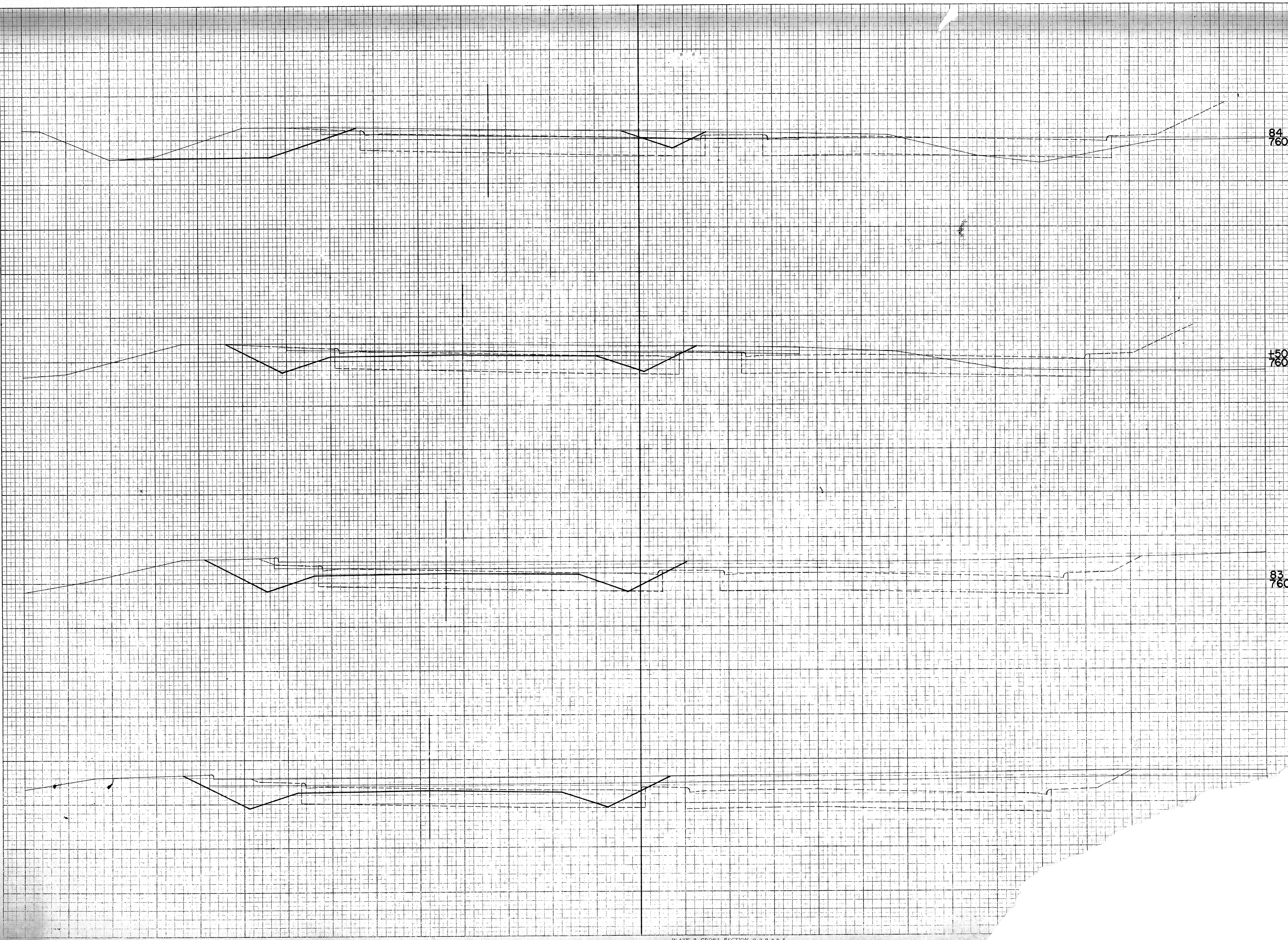
CONC. BLACKTOP

SCALE 1" = 5'

B.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. 4	F 03-2(33)	61	61

FINAL SURVEY NO.	SURVEYED	BY	DATE
	PLOTTED		
	AREAS		
	CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
10752	TEMPLATE	R.L.R.	6-56
	AREAS		
	CHECKED		



STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
84+760			
150+760			
83+760			
82+760			